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Boxford Culvert & Bridge Asset Management

Prepared for: Boxford DPW

7B Spofford Road Boxford, MA 01921

Prepared by: TEC, Inc.

146 Dascomb Road Andover, MA 01810



Executive Summary

The Town of Boxford had retained TEC to conduct a comprehensive culvert and bridge inventory, as well as structural assessment complete with replacement scoping recommendations, and compile this asset management report. TEC began by completing a comprehensive desktop inventory utilizing available online and town-provided resources. Once this initial inventory was completed, TEC performed a town-wide inspection and assessment of the Town-owned structures and searched for additional structures while preforming the field work. In total, an inspection and report was completed on six bridges and 231 culverts. TEC then conducted an in-depth risk assessment, utilizing a two-tier approach. The findings of this resulted in seven Critical structures that TEC recommends be replaced within the next ten years. The Capital Investment impact was divided into two time frames, with two culverts to be replaced within the next five years for an estimated \$700k, and five culverts to be replaced within the next 10 years for \$2.8 million.

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A - Culvert Inspection Raw Data

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Introduction and Purpose

The Town of Boxford is heavily forested, with many tributaries, brooks, streams, and general surface water runoff channels carrying water to one of the several ponds of Boxford. The Town has approximately 540 acres of open water body within its borders, and three watersheds leading to the Ipswich River, Parker River, and Merrimack River respectively.

Throughout the 1960s and 1970s, the population of Boxford more than doubled, and has steadily increased. As Boxford expanded, and public infrastructure became intertwined with the existing surface water tributaries, culverts and bridges became necessary to allow passage of water and travel, without flooding surrounding areas. With much of Boxford's infrastructure being constructed in the 1960s and 1970s to keep up with the demand of the population increase, many structures are nearing their end of serviceable life, and starting to deteriorate.

With this understanding, the Town of Boxford Department of Public Works saw necessary to create a comprehensive inventory of the Town culverts, including a structural assessment of each one. TEC has been retained to perform a town-wide inventory and condition assessment of the Town's structures and provide this asset management report to assist with municipal capitol planning and budgeting for the ongoing culvert and bridge maintenance and replacement projects.

Desktop Inventory Summary

Means and Methods

The first stage of asset management started with locating all known and potential locations of culverts and bridges throughout Boxford. To do this efficiently, a "Desktop Inventory" was created using GIS software to pinpoint each location for field investigation.

This Inventory was made using a variety of resources. Each culvert/bridge location was marked as either "Known" or "Potential" based on the source and/or availability of visual confirmation of the culvert/bridge.

To create an initial list of "Known" culverts, prior Inspection Reports provided by the Town were reviewed and logged. These Inspection Reports were completed by Haley and Ward, Inc. between July and November of 2011. These Inspection Reports included two binders: one with Culvert Inspections and the other with Catch Basin Inspections. The Catch Basin Inspection binder included sketches of drainage systems that included multiple catch basins positioned over a culvert, or sketches that showed nearby culverts. Several of the culverts listed in the Catch Basin binder were not part of the Culvert Inspection binder, and these additional culvert locations were added to the GIS map.

Satellite imagery provided by Google Maps, Google Earth, MassGIS, and Bing were used as visual confirmation for several culvert/bridge locations. Many of these locations were not part of either Inventory Binder mentioned previously. Google Street View also aided in this confirmation



process. Many headwalls, guardrail lengths, street signs, streams, and rivers could be seen only using street view which made visual confirmation possible.

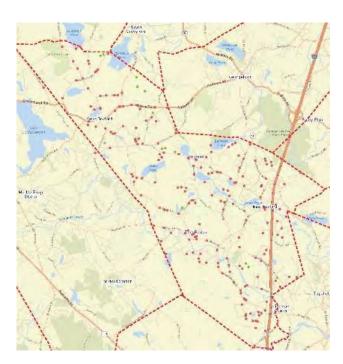
Various other resources, including the ones mentioned above, were used to find potential culvert locations. Historic satellite imagery allowed us to view Boxford during the change of seasons, making multiple streams and wetland areas visible in densely forested or overgrown areas. This imagery combined with topographic and waterway mapping on onlineGIS sites, as well as StreamStats, aided in pinpointing several potential locations that were not visible by other online means.

Online GIS maps available on the MassDOT website (resources made available through the Municipal Small Bridge Program) were used to locate and confirm potential locations of culverts, Town owned bridges, State owned bridges, or other short span structures throughout the Town. In addition to location data, these resources provided us Bridge Numbers which allowed us to search online databases for any existing bridge inspection reports that may be on file. This additional information on the existing condition of the structures assisted us in prioritizing the various culvert/bridge inspections.

Results

The desktop inventory yielded 237 culverts, and 20 bridge locations across Boxford. The culvert and bridge coordinates, approximate addresses, ownership, and source were logged into a comprehensive spreadsheet, and plotted on a GIS map.

The locations were color coded by owner, to aid in determining which locations are Town owned, and therefore to be inspected. Locations denoted as "Private", are denoted as such as the culvert/bridge was potentially located on private property. The locations "State" locations designed as culvert/bridges whose ownership confirmed as the State within the MassDOT GIS Maps / Bridge Inventory. All culverts and bridges were field located, to confirm location and ownership. If the culvert or bridge was determined to be owned by the Town, it was inspected.





Field Inventory Summary

Culverts

Means and Methods

Once the Desktop Inventory was completed, TEC was able to begin a thorough inspection of the **Town's culverts and bridges. Using the locations determined by the desktop inventory, TEC began** at the northern most culvert locations, and worked south. Each location was first confirmed, and then ownership of the culverts was determined based on Town property and parcel lines, prior to the inspection.

In addition to the pre-determined culvert locations, 62 culverts were found by a 'boots on the ground' method, where TEC would scour roadways between the mapped locations to create the most comprehensive inventory possible. TEC would look for specific features such as large topographic depressions on both sides of the roadway, pooling water, traffic safety features, and visible embankment erosion among others indicative features.

Once located and ownership confirmed, each culvert received a thorough inspection of both the inlet and outlet, as well as the roadway, embankment, and headwall/wingwall (if any). Special care was taken to describe the surrounding area, as well as any particular items that may be useful for scoping the potential replacement or rehabilitation of the culvert. The criteria of the inspection is described below



Ratings and Criteria

As mentioned above, culvert inspections consisted of an in-depth visual assessment of the multiple components of the structure at both the inlet and outlet, as well as the structures surroundings. These components consist of the roadway surface, traffic safety features, culvert structure, embankment, and headwall/wingwall/retaining wall structure (if any). All the components mentioned above, were individually rated using a rating system of; Good, Satisfactory, Fair, Poor, or Failing. Additional noteworthy observations were written, when applicable, for the rated components.

Taking into consideration the components, their ratings, and notes, an overall rating for the entire structure was given using the same rating system. Additional noteworthy observations were also written for the entire structure, when applicable. Some culverts were left unrated if observations were hindered due to high water within and/or around the culvert, the culvert being buried by sediment, and/or inaccessible due to severe vegetation overgrowth. A breakdown of the rating criteria of each component can be found below



Culvert Rating Criteria

Good

Dependent of culvert material, consists of:

- No cracks, dents/spalls, or damage
- No to very minor surface rust
- No scaling due to high water or exposed rebar
- No obstructions around the inverts/within culvert
- No shifts in culvert lengths, separation between joints, or settlement
- No scour



'Good' CMP



'Good' RCP

Satisfactory

Consists of one or more of the following, dependent of culvert material:

- No to minor cracks, dents/spalls, minor scaling due to high water, and/or damage
- Minor surface rust and/or exposed rebar
- No to minor obstructions around the inverts/within culvert
- No shifts in culvert lengths or settlement
- No to minor separation between joints and/or scour

Fair

Consists of one or more of the following, dependent of culvert material:

- Minor to moderate cracks, dents/spalls, and/or damage that does not affect the integrity of the culvert
- Moderate surface rust, scaling due to high water and/or exposed rebar
- Minor obstructions around the inverts/within culvert
- Minor to moderate shifts in culvert lengths, settlement or separation between joints
- Minor to moderate scour

Poor

Consists of more than one of the following, dependent of culvert material:

- Moderate cracks, dents/spalls, and/or damage that does affect the integrity of the culvert
- Moderate to severe rust, scaling due to high water and/or exposed rebar
- Moderate obstructions around the inverts/within culvert
- Moderate shifts in culvert lengths, settlement or separation between joints
- Moderate to severe scour

Failing

Consists of more than one of the following:

- Severe cracks, dents/spalls, and/or damage that does affect the integrity of the culvert
- Severe rust/scaling/missing portions of pipe and/or severe exposed rebar
- Severe obstructions around the inverts/within culvert impeding flow
- Severe shifts in culvert lengths, settlement or separation between joints
- Severe scour, leading to structural distress from undermining





Embankment Rating Criteria



Good

Consists of:

- No to very minor erosion
- No vegetation overgrowth
- No tree or root growth affecting the integrity of the structure

Satisfactory

Consists of one or more of the following:

- Minor erosion
- Minor amounts of sediment seeping over and/or through headwall
- Very minor vegetation overgrowth
- Very minor tree and/or root growth that does not affect the integrity of the structure

Fair

Consists of one or more of the following:

- Moderate erosion
- Moderate amounts of sediment seeping over and/or through headwall
- Minor to moderate vegetation overgrowth
- Minor tree and/or root growth affecting the integrity of the structure.



Poor

Consists of more than one of the following:

- Moderate to severe erosion
- Moderate to severe sediment seeping over and/or through headwall
- Moderate vegetation overgrowth
- Moderate tree and/or root growth affecting the integrity of the structure



Failing

Consists of more than one of the following:

- Severe erosion
- Severe sediment seeping over and/or through headwall
- Moderate to severe vegetation overgrowth
- Moderate to severe tree and/or root growth affecting the integrity of the structure



Headwall/Wingwall/Retaining wall Rating Criteria





Good

Dependent of wall material, consists of:

- No to very minor cracks
- No to minor scaling due to water
- No to minor spalling
- No to minor missing mortar/voids between stones/missing stones
- No moss growth
- No signs of rotation

Satisfactory

Consists of one or more of the following, dependent of wall material:

- Sporadic areas of minor cracks/minor spalling
- Minor scaling due to water
- Minor missing mortar/voids between stones
- No to very minor stones missing that does not affect integrity of wall
- Minor moss growth
- No signs of rotation

Fair

Consists of one or more of the following, dependent of wall material:

- Minor to moderate cracks/spalling
- Minor to moderate scaling due to water
- Minor to moderate missing mortar, voids between stones, stones missing that does not affect integrity
- Minor to moderate moss growth
- No to very minor signs of rotation

Poor

Consists of more than one of the following, dependent of wall material:

- Moderate cracks/spalling
- Moderate scaling due to water
- Moderate missing mortar/ voids between stones
- Moderate stones missing/collapsing that does affect the integrity of the wall
- Moderate moss growth
- Minor to moderate signs of rotation





Failing

Consists of more than one of the following, dependent of wall material:

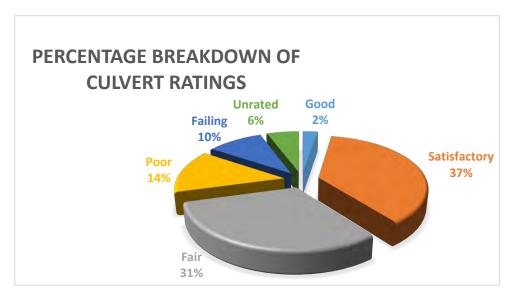
- Severe cracks/spalling
- Severe scaling due to water
- Severe missing mortar/voids between stones
- Severe stones missing/collapsing/collapsed that does affect the integrity of wall and culvert
- Severe moss growth
- Moderate to severe signs of rotation



Summarized Inspection Results

Through the Desktop Inventory a total of 237 culverts were mapped. Once in the field, 30 of these were not locatable/did not exist, but an additional 62 culverts were found and inspected, totaling 269 locations visited. Of the 269 locations, 14 were on private land, 4 were over the town line, 13 were state owned, and 7 were part of closed drainage systems/determined not to be culverts. These types of locations were not inspected, leaving a total of 231 culverts with full inspection reports and ratings.

A graphical breakdown of the overall rating of both the inlet and outlet of all inspected culverts can be seen below. As shown, approximately 70% of the Town's culvert openings are rated 'Fair' or better, with 14% rated as 'Poor', 10% rated as 'failing' and 6% unable to be rated/inspected due to field conditions encountered.



Bridges

TEC inspected the 6 town owned bridges as part of this assessment effort. All six of these town owned structures cross the Fish Brook. Five of these structures have been previously inspected by MassDOT. At these locations, the MassDOT Inspection Reports were used as a baseline to start the assessments. All assessments were performed on foot and using waders, no scaffolding or bridge inspection equipment was used to access the bridges.

TEC analyzed the field observations, the existing plans and existing inspection reports for each of these structures. TEC used this data to generate recommendations for maintenance repairs and traffic safety feature upgrades at each bridge location. These repairs were itemized using MassDOT standard nomenclature and MassDOT standard bid prices. A full breakdown of the findings and recommendations can be found in the Appendix.



High Risk Structures

Out of the 231 culverts and six bridges assessed in the field, the majority of structures ranked 'fair' or better. This general rating is used to describe the condition of the overall structure. Using this rating, TEC was able to make an initial determination of condition, by segregating all 78 culverts with a general rating of 'poor' or worse on at least one side of the culvert. Included in the 78 are the four 'double unrated' culverts which could not be inspected at either end due inaccessibility.

To determine the potential risk of these 78 culverts, the three following criterion was applied to each culvert:

Does the culvert carry a major waterway?

This criteria is to assess the risk of upstream flooding if a failure occurred. A major waterway carries more water, has a larger watershed, and has the potential to flood a large number of properties.

Is the culvert on a major roadway or dead-end?

This criteria is to assess the impact on roadway functionality in the case of a failure. A culvert on a dead end would cut access to the residents who live on that section of road in the event of a failure. Similarly, if a culvert on a major road were to fail, it would cause a severe interruption to traffic flow, and cause congestion and delays throughout the Town. Major roadways used for this criteria include: Barehill Road, Brook View Road, Depot Road, Endicott Road, Georgetown Road, Herrick Road, Ipswich Road, Kelsey Road, Killam Hill Road, Lawrence Road, Lockwood Lane, Main Street, Middleton Road, Pye Brook Lane, Topsfield Road, Washington Street, and Willow Road.

Is the culvert greater than or equal to 18" in diameter?

This criteria is to assess the roadway severity in the event of a failure under loading, as well as the amount of time/resoures needed to apply a temporary fix. It was determined that a smaller culvert (less than an 18" diameter) wouldn't have as severe of an impact on a roadway, nor would it take as much to apply a temporary fix, as an 18" diameter culvert or larger.

For each answered 'yes' given to one of these criteria for a culvert, a point was added to the culvert's critical area value (CAV). A CAV of 3 is viewed as having the highest impact to the Town upon a complete failure of the culvert.

Of the 78 culverts with at least one side having a 'poor' or worse rating, 13 had a CAV of 3, 23 had a CAV of 2, and 42 had a CAV of 1. The complete Critical Area Value breakdown can be found in the Appendix.

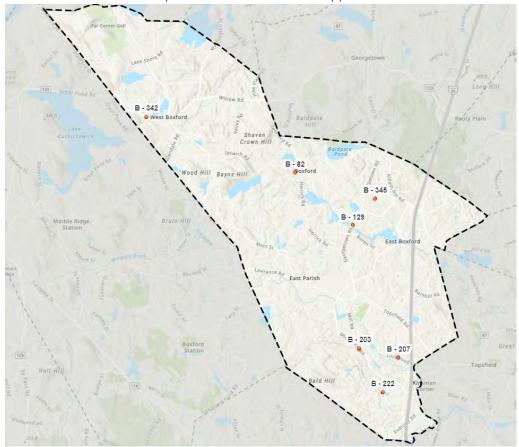


This two-tiered approach allowed TEC to narrow down the list of 'high risk' culverts to 36. Further review amongst the team was performed on these structures which included photo, feature, and location review, as well as follow-up site visits to determine the most critical structures to be replaced.

The additional review resulted in seven out of the 36 marked **as 'high risk'** to be flagged as **being in 'critical condition'**. These seven culverts are in poor or failing condition, and pose a high risk to the **town if a failure occurs**. **It is TEC's recommendation that the following culverts** be looked at closely by the Town:

B - 82
B - 129
B - 203
B - 207
B - 222
B - 342
B - 345
316 Ipswich Road
Road
Road
B - 207
B - 345

Each of these seven critical culverts are described individually on the following pages, and a preliminary plan for replacement or rehabilitation, including approximated remaining service life, recommended rehabilitation or replacement method, and approximated cost.





Address:

316 Ipswich Road

Length:

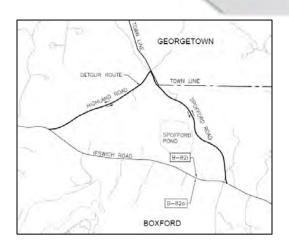
 $\pm 65'$

Materials:

60" RCP, Precast Concrete Headwall and Wingwalls

Waterway:

Pye Brook; downgradient of Spofford Pond



Introduction

Culvert B – 82 carries major water way Pye Brook under major roadway Ipswich Road, through a 60 inch reinforced concrete pipe, resulting in a CAV score of 3. The culvert received a Poor assessment of the inlet and Failing assessment of the outlet, resulting in a high risk of failure. Culvert B-82 is considered a Critical Cuvlert due to the risk of failure, and resultant upstream flooding and severe traffic disruptions if the structure were to fail





Existing Conditions

<u>Inlet (Poor)</u>: Moderate scaling throughout the pipe due to water, and sporadic spalls and missing concrete was observed. The embankment shows moderate erosion around the headwall, with a crack observed in the middle of the headwall and scaling to high water.

Outlet (Failing): Moderate scaling throughout pipe due to water, and sporadic patches of exposed rebar was observed. The embankment was observed to have severe erosion with sediment and tree growth seeping over the headwall. The headwall itself was observed to have severe deterioration with moderate scaling on the bottom 1' of wingwalls.





Additional Notes, Recommendations, and Cost

Overhead wires are present near the culvert's outlet, and a guardrail in fair condition is present near the inlet. Conversations with an abutting neighbor revealed beaver activity observed within and outside of the culvert. The estimated remaining service life for this culvert is 5 to 10 years. TEC recommends a full culvert replacement with a three-sided box culvert. The estimated cost of replacement is \$600,000 to \$750,000, depending on the results of the field survey, hydraulic study, and geotechnical investigations.



Address:

166 Middleton Road

Length:

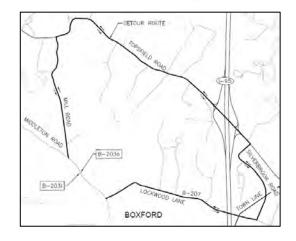
 $\pm 80'$

Materials:

24" CMP, Mortared Stone Headwall

Waterway:

Hydraulic connection between large wetland to Fish Brook



Introduction

Culvert B - 203 provides a hydraulic connection between a large wetland to Fish Brook and can see up to a flow of 38.8 cfs during a 10-year storm event according to StreamStats. Above the 24 inch corrugated metal pipe, major roadway Middleton Road is carried. Considering this information and the Poor and Failing condition of the inlet and outlet respectively, this culvert is high risk, due to the risk of upstream flooding and severe traffic disruptions if the structure were to fail.





Existing Conditions

Inlet (Poor): The pipe inlet was observed to be obstructed by sediment and leaf/branch buildup, with moderate to severe erosion on the embankment and the headwall was observed to be actively overturning. Upstream a beaver structure was noted, as well as an oil sheen on top of water near the inlet.

<u>Outlet (Failing)</u>: Sides of the pipe have rusted away, dropping the bottom into the substrate. Severe rust and patches of pipe were observe to be missing throughout the pipe. The embankment shows moderate erosion, with the headwall actively overturning and the bottom 6" of mortar is missing.





Additional Notes, Recommendations, and Cost

Overhead wires and traffic safety features are present, with the guard rail observed to be in fair condition. The estimated remaining service life for this culvert is 5 to 10 years. TEC recommends a full replacement with a three or four-sided concrete box culvert. The estimated cost of replacement is \$400,000 to \$500,000, with costs depending on the results of a field survey, hydraulic study, and geotechnical investigations



Address:

66 Lockwood Lane

Length:

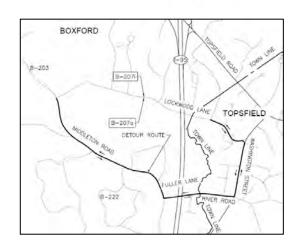
+ 45'

Materials:

32" CMP, Mortared Stone Headwall

Waterway:

Tributary to Fish Brook



Introduction

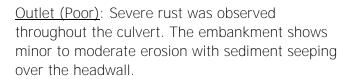
Culvert B – 207 was assessed to have a Failing inlet and an outlet in Poor condition. It is located along major roadway Lockwood Lane, and carries an unnamed tributary to Fish Brook through a 32 inch corrugated metal pipe that can see up to a 51.8 cfs flow during a 10-year storm event according to StreamStats. This earns the culvert a CAV score of 3, and poses a high risk of upstream flooding and severe traffic disruptions if the structure were to fail. Culvert B – 207 is considered critical due to the CAV score, and its assessed condition..





Existing Conditions

Inlet (Failing): The bottom of the pipe has rusted away, and moderate to severe rusting was observed throughout the rest of the pipe. The embankment shows severe erosion, and the headwall is completely separated from culvert, indicating pipe settlement. In addition, the bottom 6" of mortar is missing from the headwall.







Additional Notes, Recommendations, and Cost

No traffic safety features were present at either inlet or outlet, however an aboveground water connection was present indicating water utilities near the inlet. Overhead wires were observed near the outlet. The estimated remaining service life for this culvert is 5 to 10 years. It is TEC's recommendation to fully replace the existing culver with a three or four-sided concrete box culvert. The estimated cost of replacement is \$400,000 to \$500,000 depending on the results of the field survey, hydraulic study, and geotechnical investigations.



Address:

LOT 5 Washington Street

Length:

± 53'

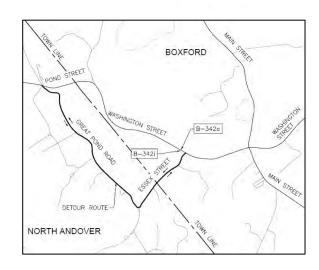
Materials:

24" CMP, Mortared Stone(inlet)/Dry Laid

Stone(outlet)

Waterway:

Hydraulic Connection to the Parker River (upgradient of Sperrys Pond)



Introduction

Culvert B – 342 is a 24-inch corrugated metal pipe providing a hydraulic connection south of Washington Street/Rt 133 to the Parker River (upgradient of Sperrys Pond). The condition of the inlet was assessed to be Poor, and the outlet assessed to be Failing. The culvert is considered to be critical due to the condition of the pipe, and the severe traffic disruption it would cause if it were to fail.





Existing Conditions

Inlet (Poor): The first approximately 2 feet of pipe bottom is rusted away, and severe rust and patches of pipe were observed to be missing throughout the length of pipe. The embankment was observed to have moderate to severe erosion and sediment seeping over the headwall, with loose stones observed at the top of the headwall.

<u>Outlet (Failing)</u>: The first approximately 1 foot of the bottom of pipe is rusted away, with severe rust and patches of pipe observed to be missing throughout the length of pipe. The embankment was observed to have severe erosion with sediment seeping through the headwall. The headwall itself is failing, as the majority of stones are either missing or collapsing.





Additional Notes, Recommendations, and Cost

At both the inlet and outlet, guard rail was observed to be present and in satisfactory to fair condition. Overhead wires were observe above the outlet. The estimated remaining service life for this culvert is 5 to 10 years. It is **TEC's recommendation to complete a full replacement with a three or four**-sided concrete box culvert. The estimated cost of a full replacement is \$450,000 to \$600,000 depending on the results of the field survey, hydraulic study, and geotechnical investigations.



Address:

105 Georgetown Road

Length: **± 55'**

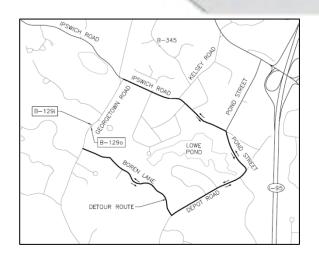
Materials:

54" Three-Sided Mortared Stone Box Culvert,

Mortared Stone and Poured Concrete Headwalls and

Wingwalls Waterway:

Pye Brook; Between Four Mile Pond and Lowe Pond



Introduction

Culvert B - 129 carries Pye Brook, through a large 54-inch box culvert beneath major roadway, Georgetown Road, giving Culvert B - 129 a CAV score of 3, and the high risk of upstream flooding and severe traffic disruptions if the culvert were to fail. The culvert is considered critical due to this, and the Failing assessment of its outlet.





Existing Conditions

Inlet (Fair): The inlet was observed to be missing mortar from the bottom 2' of the structure, and the embankment was observed to have minor to moderate erosion. The headwall and wingwalls had severe scaling and minor scour observed at the bottom of the wingwalls.

Outlet (Failing): The bottom 2'-3' of stones lining the culvert were observed to have collapsed into Pye Brook. The embankment was observed to be moderately eroded, with the headwall and wingwalls showed signs of moderate scour. Additionally, stones from the wingwall were observed to have collapsed at the bottom, and a large settlement crack was observed in the concrete.





Additional Notes, Recommendations, and Cost

Guardrails from satisfactory to poor condition were present near both the inlet and outlet, and overhead wires were present near the inlet. The estimated remaining service life for this culvert is less than 5 years, with immediate maintenance potentially extening the service life. Immediate maintenance would include the repair of the undermined headwall/wingwalls with new stones and mortar. Estimated cost of maintenance repair is \$50,000 to \$100,000.



Address:

21 Wildmeadow Road

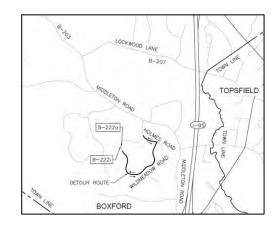
Length: **± 32'**

Materials:

± 33" CMP, Mortared Stone Headwall

Waterway:

Tributary to Fish Brook



Introduction

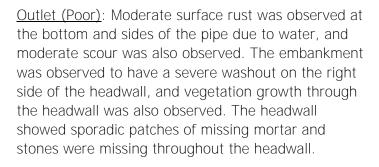
Culvert B – 222 carries a tributary to Fish Brook through an approximately 33-inch corrugated metal pipe that according to StreamStats can see up to a 21.8 cfs flow during a 10-year storm. The condition of the culvert was assessed to be Fair at the inlet and Poor at the outlet, with a large indent approximately 5 feet into the culvert from the inlet causing a significant hydraulic restriction. The culvert is considered critical based on the condition of the pipe, and the risk of flooding upstream if the structure were to fail.





Existing Conditions

Inlet (Fair): A large dent was observed at the top of the pipe at approximately 5' from the mouth of the inlet. This dent significantly restricts the hydraulic capacity, and poses a threat for surficial failure on the roadway due to a potential void. A moderate amount of rust was observed on all sides of the pipe, and severe scour was present. The embankment was observed to have minor to moderate erosion, with vegetation growth growing through the headwall. The headwall was observed to have sporadic patches of missing mortar.







Additional Notes, Recommendations, and Cost

An electrical box was observed near the culvert, with assumed underground wire utilities being present. There were no traffic safety features observed near the culvert at either the inlet or outlet. The estimated remaining service life for this culvert is 5 to 10 years. TEC recommends a full replacement with a three or four-sided concrete box culvert. The estimated cost of replacement is \$350,000 to \$450,000, depending on the results of the field survey, hydraulic study, and geotechnical investigations.



Address:

34 Woodcrest Road

Length:

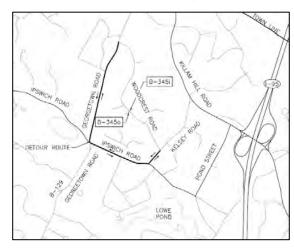
± 65'

Materials:

18" CMP with a Poured Concrete Headwall/ 12"

HDPE Waterway:

Hydraulic connection for a tributary to Penn Brook



Introduction

This culvert carries a hydraulic connection with potential for dual-flow for a tributary to Penn Brook, and is comprised of an 18 inch diameter corrugated metal pipe on the northern side, and a 12 inch diameter HDPE pipe on the southern side. According to StreamStats, this culvert can see up to 8.34 cfs flows during a 10-year storm. The northern side was given a Failing assessment, and the southern side a Poor rating. Due to the condition of the culvert, and the risk of upstream flooding if a failure were to occur, B - 345 is considered a critical culvert.





Existing Conditions

Northern side (Failing): The majority of the pipe was observed to have rusted away with severe rust throughout the remaining pipe also observed. The embankment was noted to have severe erosion, and the headwall is actively overturning. It was observed that a large tree trunk may be what is supporting the headwall.

Southern Side (Poor): The observed pipe is a 12 inch diameter HDPE which either joins the 18 inch CMP on the north mid-span, or was slid through the 18" due to failure. It was noted that the bottom of the pipe is completely missing from the first approximately 12 inches. The embankment was observed to have minor to moderate erosion.



Additional Notes, Recommendations, and Cost

A guardrail and steel cable were in failing condition at both the southern and northern openings, with overhead wires observed above the southern opening. The estimated remaining service life for this culvert is less than 5 years. TEC recommends a full replacement with a three-sided concrete box culvert. The estimated cost of replacement is \$400,000 to \$600,000, depending on the results of the field survey, hydraulic study, and geotechnical investigations.



Recommended Capital Investment Table (Culverts) 0-5 Years

U-3 rears			
	B-129	105 Georgetown Road	\$100,000
	B-345	34 Woodcrest Road	\$600,000
0-5 Years	Total Cost (Estima	ite)	\$700,000
5-10 Years			
	B-82	316 Ipswich Road	\$750,000
	B-203	166 Middleton Road	\$500,000
	B-207	66 Lockwood Lane	\$500,000
	B-342	LOT 5 Washington Street	\$600,000
	B-222	21 Wildmeadow Road	\$450,000
5-10 Years	Total Cost (Estima	ite)	\$2,800,000
Recommended	•	estment Table (Bridges)	

Maintenance and	Repairs
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B-19-001 (899) B-19-002 (8RE)	Middleton Road over Fish Brook Mill Road over Fish Brook	\$158,000 \$0
B-19-003 (2R3)	Lockwood Lane over Fish Brook	\$128,000
B-19-004 (89A)	Fuller Lane over Fish Brook	\$185,000
B-19-005 (89B)	Endicott Road over Fish Brook	\$135,000
B-19-018 (C68)	Brookview Road over Fish Brook	\$211,500
Maintenance and Repair Cost (Estimate)		\$817,500

Conclusion

TEC was able to inventory and assess the culverts and bridges located within the Town of Boxford from known locations, online resources, and an in-person approach. The majority of structures were given a 'Fair' or better assessment, however the structures on the lower end of the scale were analyzed further for risk based on location and characteristics. Seven of these structures were deemed to be in 'critical condition' and pose a significant risk to town infrastructure if failure occurred. An in-depth analysis was conducted on these seven locations, with recommendations to the town including replacement scoping and estimated structure replacement pricing.



Appendix A Culvert Inspection Raw Data

								Hallaton.	Traffic Traffic Traffic		Culvert Culvert Alignment			Obstantian	Culvert Invert	Culvert Effective Substrate W	Water Depth Pipe/	Bankfull	Overall Course the second	Headwall/M	Fingw General Evidence Evi	
Culvert ID	outlet to minimum 3	Date and Time Weather	Inspector Located Cul-	vert Culvert In:	pectable Surface to	type Surface Condition V	Vidth (ft) Total Culvert Length	n (ft) Present Utility Type	Safety Safety Safety Features Feature Feature	General Roadway Observations	Shape to Roadway Culv	ert Alignment to Stream	Water in Culvert Culvert Obstru	ted Location Obstruction T	ype Elevation Culvert Material	(in) Opening Width (in) (ii	n Culvert Culvert Scour Location V in) Condition Observed of Scour ((ft) General Culvert Observations Embankment Type	Culvert (ft) Condition	Retaining Wall Material Condition	g Wall Embankment/Headwall of Beaver Illii Observations Activity Dis	scharge General Comments Rating x y
B - 12o	drop inlets along mainstreet	10/27/2020 12:06 overcast	ajs yes	yes	paved	good	24	35 no	no		round skewed_<45° stres	am_aligned	no_moist_bottom no		perched corrugated_metal_pipe	36 36 0	0 good none	minor surficial rust in bottom half 13 of pipe slope_flatter_than_2:1_,he sporratic rock/sediment within	5.5 poor	concrete_poured_ satisfactory	severe washout behind left no no	severe bank erosion downstream. bank cuts up to 6ft satisfactory -71.0753 42.7264
B - 17i		10/27/2020 12:50 overcast	ajs yes	yes	paved	good	24	41 no	no		round roadway_aligned skew		no_dry_bottom yes_<25%	within_culvert sediment	at_stream_grade corrugated_metal_pipe	12 12 0	0 good none	4 pipe headwall_retaining_wall_sk surface rusting throughout	2.5 good	dry_laid_stone_brick satisfactory	material spilling through he no no	satisfactory -71.0728 42.72273 vegetation growth immediately
B - 17o		10/27/2020 13:05 overcast	ajs yes	yes	paved	good	24	41 yes overhead_wires	no		round roadway_aligned no_o	discernable_stream_channel	no_dry_bottom yes_25_50%	within_culvert sediment	at_stream_grade corrugated_metal_pipe	12 7 12	0 satisfactory none	interior headwall_retaining_wall,sle first 10inches of protruding inlet	4 satisfactory	dry_laid_stone_brick fair	headwall apears to be missi no no	o downstream satisfactory -71.0729 42.72272
B - 18i		10/27/2020 13:26 overcast	ajs yes	yes	paved			yes overhead_wires	no		round skewed_<45° no_c	discernable_stream_channel	no_moist_bottom yes_<25%	upstream branches	at_stream_grade corrugated_metal_pipe	18 12 0	0 fair minor headwall	is deformed and torn. inside pipe appears to be in good condition headwall_retaining_wall,ske	4 poor	dry_laid_stone_brick satisfactory	embankment erosion on bo no no	heavy branch buildup upstream, utility pole adjacent to culvert fair -71.0771 42.72175
B - 18o		10/27/2020 13:37 overcast	ajs yes	no	paved	satisfactory	20	41 no	no	minor transverse cracking not above culvert	round skewed_<45° no_c	discernable_stream_channel	no_moist_bottom yes_50_75%	vegetation_gr upstream h,branches	owt corrugated_metal_pipe	18		headwall_retaining_wall,sle	5 satisfactory	dry_laid_stone_brick	heavy thorns/branches/liml no no	unable to inspect culvert due to heavy thorns/brush -71.077 42.72168
B - 28i		10/27/2020 13:54 overcast	ewm yes	yes	paved	satisfactory	21	41 yes overhead_wires	no	minor edge and transverse cracking	round roadway_aligned no_o	discernable_stream_channel	no_dry_bottom no		inlet_drop corrugated_metal_pipe	12 9 0	0 good none	minor surface rusting at bottom, sediment buildup at pipe opening headwall_retaining_wall	4.5 fair	dry_laid_stone_brick satisfactory	minor material spilling thro no no	satisfactory -71.0832 42.71525 stonewall and stacked branches 5ft
																		minor rusting on bottom, up to 2.5 ft of scour beneath pipe, 2 ft				from outlet, 12in dia tree adjacent to outlet, large birds nest ontop of
B - 28o		10/27/2020 14:08 overcast	ewm yes	yes	paved	satisfactory	21	41 yes overhead_wires	no		round roadway_aligned skew	ved_<45°	no_dry_bottom no		perched corrugated_metal_pipe	12 12 0	0 good moderate culvert	20 of water freefall headwall_retaining_wall,sk minor surface rusting inside pipe,	4.5 fair	dry_laid_stone_brick fair	irregular stones with voids, no no	pipe fair -71.083 42.71521 large metal wire cage at mouth of
B - 21i		10/27/2020 14:29 overcast	ewm yes	yes	paved	good	23	40 yes overhead_wires	no		elliptical skewed_<45° no_c	discernable_stream_channel	flowing no		at_stream_grade corrugated_metal_pipe	46 36 36	2 good none	minor tear at top of pipe, headwall_retaining_wall_sk	3.5 fair	dry_laid_stone_brick fair	severe erosion due to missi no no	inlet fair -71.0701 42.71907
B - 21o		10/27/2020 14:45 overcast	ewm yes	yes	paved	good	21	40 no	no		elliptical skewed_<45° skew	ved_>45°	flowing no		at_stream_grade corrugated_metal_pipe	46 36 36	culvert,he 6 good minor adwall	minor surace rusting inside pipe, 2 minor sediment buildup at outlet headwall_retaining_wall minor surface rust, sporadic rocks	2.5 satisfactory	dry_laid_stone_brick fair	moderate material spilling t no no	heavy vegetation downstream satisfactory -71.07 42.71907
B - 39i		10/27/2020 15:04 overcast	ewm yes	yes	paved	poor	25	62	yes Guardrail satisfact	ory	round roadway_aligned stres	am_aligned	no_dry_bottom no		at_stream_grade corrugated_metal_pipe	18 18 0	0 satisfactory minor culvert	20 in pipe headwall_retaining_wall,sk	6 good	mortared_stone_brick satisfactory	all mortar missing for botto no no	satisfactory -71.0671 42.71087
B - 39o		10/27/2020 15:25 overcast	ewm yes	yes	paved	poor	25	62	yes guardrail fair	fatigue cracking and patching	round roadway_aligned stres	am_aligned	no_moist_bottom yes_<25%	within_culvert sediment	at_stream_grade corrugated_metal_pipe	19 13 18	0 fair minor culvert	slight deformation at left side of culvert outlet, moderate interior 20 surface rust headwall_retaining_wall,sk	7.5 satisfactory	mortared_stone_brick fair	mortar missing from botton no no	general tree growth adjacent to embankment satisfactory -71.067 42.71076
0 - 21i		10/27/2020 15:47 overcast	ewm ves	Long Co.	paved	fair	25	76	yes guardrail fair		round skewed <45° no	directorable etream channel	no dry bottom no		at stream grade corrugated metal pipe	18 18 0	0 good minor culvert	minor surficial interior rusting, sporadic rocks within culvert soan headwall retaining wall.sk	7 enod	mortared stone brick satisfactory	mortar separation from sto no no	moderate vegetation and tree growth in embankment, large tree down upstream satisfactory -71.0686 42.71223
B - 310	culvert bends mid span	10/27/2020 15:59 overcast	ewm yes	yes	paved	fair	25	75	yes guardrail fair			discernable_stream_channel	no_dry_bottom no		at_stream_grade corrugated_metal_pipe	18 18 0	0 good none	minor surficial interior rust, headwall_retaining_wall_sk	9 good	mortared_stone_brick satisfactory	large vertical settlement crano no	large fallen tree downstream satisfactory -71.0687 42.71222 potential illicit discharge in midspan
B - 37o	at manhole in West	10/27/2020 16:19 partly_clou	dy ewm yes	yes	paved	good	25	130 yes overhead_wires	yes guardrail good		round skewed_>45°		flowing no		at_stream_grade hdpe	36 30 14	8 good none	36 inch concrete box header, other	1 poor	concrete_pre_cast_ fair	concrete box header, erosic no	drainage manhole satisfactory -71.0638 42.71118 severe undercutting of upstream banks, 6 in rcp observed adacent to
B - 37i B - 36		10/27/2020 16:35 partly_clou 10/27/2020 16:48 partly_clou	dy ewm culvert_was	yes s_not_located_does_no	paved	good	25	130 yes water_line	no		round skewed_>45° no_c	discernable_stream_channel	flowing no		perched hdpe	36 36 29	7 good moderate culvert	slope_flatter_than_2:1_	2 fair		bank armoring with large st no yes	es culvert, satisfactory -71.0636 42.71143 -71.0638 42.71163
B - 27 B - 26i	twin outlets for two	10/28/2020 11:52 rain 10/28/2020 12:02 rain	wnb ewm yes	ves	paved	good	23	70 yes overhead_wires,drainage_system	em no		round skewed <45° stres	am_aligned	no_moist_bottom no		at_stream_grade hdpe	16 16 0	0 good none	6 Bends midspan. slope_steeper_than_2:1_	5.5 satisfactory		Area of minor erosion on er no no	71.0637 42.71621 Minor vegetation around streambed 71.0607 42.71643
	Extending out of catch																					Minor vegetation on the stream.
	basin Part of closed drainage	10/28/2020 12:19 rain	ewm yes	yes	paved	good	23	70 yes drainage_system	yes bollard fair	Exposed Rebar on concrete bollards	round roadway_aligned skew	ved_<45°	stagnant no	within_culvert,up	at_stream_grade reinforced_concrete_pipe	18 16 18	6 satisfactory minor culvert	2 Minor scour on sides of culvert. slope_steeper_than_2:1_ Scour on right side of culvert. Soil is starting to erode from drop	5 satisfactory		Minor vegetation on embar no no	Discharges to wetland area. satisfactory -71.0606 42.71635 General tree growth in
B - 25i	system	10/28/2020 12:44 rain	ewm yes	yes	paved	good	30	65 yes overhead_wires,drainage_system	em no		round roadway_aligned strea	am_aligned	stagnant yes_<25%	stream leaves, branch	es inlet_drop reinforced_concrete_pipe	12 9 9	1 satisfactory none culvert	9.5 inlet. slope_steeper_than_2:1_,h	7 satisfactory	dry_laid_stone_brick satisfactory	Headwall has voids betweer no no	embankment. fair -71.0571 42.71649 abutter requested dead tree limb
																						removal adjacent to culvert. Limbs present in streambed. Minor
B - 25o B - 24i	Closed drainage system.	10/28/2020 13:07 rain 10/28/2020 13:32 rain	ewm yes ewm yes	yes yes	paved paved	good good	30 23	65 yes drainage_system 66 yes overhead_wires	no no		round roadway_aligned skew round roadway_aligned no_c	ved_<45° discernable_stream_channel	no_moist_bottom yes_<25% no_dry_bottom yes_25_50%	within_culvert leaves within_culvert leaves	at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	12 8 12 12 10 12	0 fair none 0 satisfactory minor culvert	14 Scaling to high water. headwall_retaining_wall,ski headwall_retaining_wall,ski last section of rcp appears to		dry_laid_stone_brick satisfactory mortared_stone_brick satisfactory		erosion on stream bank. fair -71.057 42.71647 satisfactory -71.0548 42.71645
																		have separated, forming a sinkhole/washout approx 8ft				General tree growth. 2 foot
B - 24o		10/28/2020 13:48 rain	ewm yes	yes	paved	good	23	66 no	yes guardrail fair		round roadway_aligned no_c	discernable_stream_channel	no_dry_bottom yes_25_50%	within_culvert sediment	at_stream_grade reinforced_concrete_pipe	12 7 12	0 good none	upgradient from outlet. headwall_retaining_wall,sk Minor Surface rust at bottom of	6 fair	mortared_stone_brick failing	2 ft diameter Sinkhole 8 fee no no	o diameter sinkhole. poor -71.0548 42.7166 Wetland flags adjacent to inlet.
B - 23i		10/28/2020 14:23 rain	ewm yes	yes	paved	good	26	76 yes overhead_wires	yes guardrail failing		round skewed_>45° stres	am_aligned	no_dry_bottom no		at_stream_grade corrugated_metal_pipe	12 12 5	0 fair minor culvert	the pipe. Dent at top right side 8 14 feet inside the culvert. headwall_retaining_wall,ske	7.5 fair	mortared_stone_brick poor	Large settlement crack, larg no no	Fallen tree limbs near inlet. 2 foot diameter hole near end of guardrail. fair -71.0528 42.71717
B - 23o		10/28/2020 14:45 rain	ewm yes	ves	paved	good	23	76 yes drainage system	no	Closed drainage system outleting approximate 100 feet away from outlet.	ely round skewed >45° no o	discernable stream channel	no dry bottom yes <25%	within culvert sediment	at stream grade corrugated metal pipe	12 11 8	0 satisfactory minor culvert	Minor surface rust. headwall retaining wall, sk	7 satisfactory	mortared stone brick fair	Embankment has minor ero no no	Sporatic stones around headwall and culvert outlet. Nearby wetland flags. satisfactory -71.053 42.71718
		10/28/2020 15:10 rain	ewm ves				45							_		12 12 0	0 satisfactory none	Interior pipe separation 3 feet in. headwall retaining wall-sign		mortared stone brick fair		Wetland Flag 6A adjacent to
B - 32i		10/28/2020 15:10 rain	ewm yes	yes	paved	good	25	72 yes overhead_wires	no		round skewed_<45° no_c	discernable_stream_channel	no_dry_bottom no		at_stream_grade reinforced_concrete_pipe	12 12 0	0 satisfactory none	Interior pipe separation 3 feet in. headwall_retaining_wall,sii	5 satisfactory	mortared_stone_brick fair	Embankment has minor erosion on right s	outlet from nearby catch basin
																						present in wingwall. Depression appoximately 2 feet from pipe outlet. Small tree 5 feet from outlet
B - 32o		10/28/2020 15:33 rain	ewm yes	yes	paved	good	25	72 yes gas_line,drainage_system	yes bollard failing		round skewed_<45° no_c	discernable_stream_channel	no_dry_bottom no		perched reinforced_concrete_pipe	12 12 0	0 satisfactory none	Aggregate on bottom of pipe is showing through. headwall_retaining_wall,sk	6.5 satisfactory	mortared_stone_brick satisfactory	Embankment has minor ero no no	in outfall area. Nearby wetland flag B4 satisfactory -71.0542 42.71234
B - 33i		10/28/2020 16:01 rain	ewm yes	yes	paved	good	27	S4 no	no		round roadway_aligned no_o	discernable_stream_channel	no_dry_bottom no		at_stream_grade reinforced_concrete_pipe	15 15 0	0 satisfactory none	Minor aggregate showing through bottom of pipe. headwall_retaining_wall,ske	5.5 fair	mortared_stone_brick satisfactory	Embankment has minor ero no no	satisfactory -71.0535 42.71211
B - 33o		10/28/2020 16:16 rain	ewm yes	yes	paved	good	27	54 yes overhead_wires	yes guardrail good		round roadway_aligned no_c	discernable_stream_channel	no_dry_bottom no		perched reinforced_concrete_pipe	15 15 0	0 satisfactory none	The second pipe length is shifted to the left. Scaling to high water. headwall_retaining_wall.sle	6 fair	mortared_stone_brick satisfactory		Werland flagging nearby. Fallen tree limbs in drainage outfall. satisfactory -71.0535 42.71203
B - 30i B - 30o		10/28/2020 16:37 rain 10/28/2020 16:47 rain	ewm yes	yes	paved paved	fair	27	40 no 40 yes overhead wires	no		round roadway_aligned no_c	discernable_stream_channel		within culvert sediment	at_stream_grade corrugated_metal_pipe at stream grade corrugated metal pipe	12 12 0 12 9 11	0 satisfactory none 0 satisfactory none	Minor surface rust in pipe. headwall_retaining_wall,sk Minor surface rust throughout pipe. headwall_retaining_wall,sk		dry_laid_stone_brick satisfactory dry_laid_stone_brick satisfactory		Tree limbs down in inlet area. satisfactory -71.0521 42.71233 Moderate present at outfall. satisfactory -71.0519 42.71219
B - 40i		10/29/2020 11:48 rain	ewm yes	yes	paved	fair	22	90 yes drainage_system	no		round skewed_>45° no_c				at_stream_grade reinforced_concrete_pipe	12 12 0	0 good none	headwall_retaining_wall,sk		mortared_stone_brick good	Minor erosion around head no no	Forest debris around inlet. good -71.0565 42.71062 Wetland marker above outlet pipe.
																		Scaling on top of exposed pipe end, likely from closed drainage				12 in. RCP pipe from closed drainage system above culvert outlet pipe. Fallen branches close to
B - 40o		10/29/2020 12:01 overcast	ewm yes	yes	paved	fair	22	90 yes drainage_system,other	no		round skewed_>45° no_c	discernable_stream_channel	no_dry_bottom no		perched reinforced_concrete_pipe	12 12 0	0 satisfactory none	system pipe. headwall_retaining_wall,ski Inlet of culvert is inaccessible	6 satisfactory	mortared_stone_brick satisfactory	Embankment has minor scono no	o outlet. satisfactory -71.0562 42.71062
										Moderate to severe cracking along center of roadway. Large dips and depressions in roadw								and non-visible due to vegetation overgrowth and steep				Severe oil stains within the outlet area. Large amounts of trash within
B - 480 B - 301i	Outfalls into Parker Rive At end of Glendale Road	11/20/2020 14:29 overcast r, . 10/29/2020 12:46 overcast	ewm yes	no yes	paved paved	poor satisfactory	35	23 yes Overhead wires. 70 no	yes Guard Rail fair	with patches along prior dips. Dip in road from settlement at pipes location.		discernable_stream_channel	no_dry_bottom no		at_stream_grade pvc	9 9 0	0 satisfactory minor culvert	embankment. Headwall/retaining wall, gr Discoloration on bottom of pipe from water. slope_flatter_than_2:1_	ater than 2:1 failing 2 satisfactory	dry_laid_stone_brick	Severe erosion along embaryes ye: Minor erosion leading to pi no no	s outlet area71.0662 42.70451
B 201-		10/29/2020 13:01 overcast					25	70 yes overhead_wires	yes guardrail fair	Dip in roadway due to settlement above pipe.			no_dry_bottom no				0 fair moderate headwall	2 in. long crack going into pipe. 9 Discoloration at bottom of pipe. headwall_retaining_wall,sle	3.5 satisfactory	dry_laid_stone_brick fair	Embankment has minor sco no no	fair -71.0612 42.70384
B - 303i		10/29/2020 13:01 overcast 10/29/2020 13:35 overcast	ewm yes	yes	paved	good	22	35 no	yes guardraii tair	DIP IN roadway due to settlement above pipe.	_	ved_<45°	no_dry_bottom no		at_stream_grade hdpe	18 18 0	0 fair minor culvert	Head wall stones starting to 3 deform pipe at top and sides. headwall_retaining_wall,sle		dry_laid_stone_brick satisfactory		Streambed is aligned with stone
B - 2020		10/29/2020 13:49 overcast	awa war	LOWE .	paved	good	22	35 yes overhead wires	20		round skewed_<45° stres	am aliened	no dry bottom no		at stream grade, being	18 18 0	0 satisfactory minor culvert	6 Minor scour underneath culvert. headwall_retaining_wall,sk	2 ratisfactors	dry laid stone brick satisfactory	Embankment has minor ereno no	Culver and headwall replaced within five years according to abuttine homeowner. satisfactory -71.0619 42.70076
B - 60o		10/29/2020 14:20 overcast		no	paveu	good	22	35 yes Overhead_wires	110		Tourio Skewed_443 Sues	arr_angrieu	no_dry_bottom no		at_stream_grade hdpe	18 18 0	o satisfactory militor convert	6 Willion scool diluterileatri culvert. Headwaii_retailing_waii_sii	3 Satisfactory	dry_late_stone_onck satisfactory	Embankment has minor exhib	Part of closed drainage system. Cage around outlet71.0619 42.70076
																		Old headwall stone is 2 feet inwards into the culvert. Precast				Precast headwall appears to be placed recently. Large amounts of
B - 302o		10/29/2020 14:31 overcast	004073 ···			man-f	21	22 year growth and role	20		other skewed <45° no	directorable	no day batter		at stream grade . doc laid	20 24 20	0 fair none	head wall and structure over culvert. Miscellaneous small	4 fair	concrete pre cast fair	Embankment is very steep no no	vegetation close to outlet. 24 inch diameter dead tree next to
B - 302o B - 302i		10/29/2020 14:31 overcast 10/29/2020 14:55 overcast	ewm yes	yes yes	paved paved	good	21	33 yes overhead_wires 33 no	no		open_bottcskewed_<45° no_c			within_culvert other	at_stream_grade dry_laid_stone at_stream_grade dry_laid_stone	20 24 20 24 12 24	0 fair none 0 fair none	stones throughout bed of culvert. headwall_retaining_wall,sle Two large stones with in culvert restricting flow. slope_steeper_than_2:1_,h		dry_laid_stone_brick poor	Embankment is very steep no no Embankment has major ercno no	poor -71.0634 42.69861
B - 304i		10/29/2020 15:34 rain	ewm yes	yes	paved	good	24	45 no	no		round roadway_aligned no_o	discernable_stream_channel	no_dry_bottom no		at_stream_grade hdpe	12 12 0	0 good none	Appears to be new, still a sticker on the inside. slope_steeper_than_2:1_	3.5 fair		Embankment as minor scor no	Difficult to access due to thorn overgrowth. satisfactory -71.0634 42.69501
																						12 inch CMP pipe located below culvert, through headwall. Severe
																						vegetation overgrowth above culvert and surrounding culvert. Not easily accessible to to overgrowth of
B - 304o		10/29/2020 15:49 rain	ewm yes	yes	paved	good	24	45 yes gas_line,overhead_wires	yes guardrail good		round roadway_aligned no_o	discernable_stream_channel	no_dry_bottom no		perched hdpe	18 18 0	0 good none	slope_steeper_than_2:1_	3 fair	mortared_stone_brick satisfactory	Culvert resting on top of he no no	thorns. satisfactory -71.0638 42.69503
B - 69i		10/29/2020 16:13 rain	ewm yes	yes	paved		23	85 yes overhead_wires	yes guardrail satisfact	ory	open_bottcskewed_>45° stres	am_aligned	flowing no		at_stream_grade other	30 22 27	2 satisfactory none	Culver is a concrete arch. Minor 5 surface cracks within the culvert. headwall_retaining_wall,sle	4 fair	mortared_stone_brick poor	Moderate erosion behind vno no	Minor vegetation overgrowth. 1 foot wide settlement washout leading towards top of headwall. fair -71.0587 42.69675
																		Minor surface cracking inside of culvert. Scaling at mouth of				Tree limbs covering top of head wall and base of stream. Minor
B - 69o		10/29/2020 16:38 rain	ewm yes	yes	paved	satisfactory	23	85 yes overhead_wires	no		other skewed_>45° stres	am_aligned	flowing no		at_stream_grade other	30 19 28	4 satisfactory none	5 culvert due to water. headwall_retaining_wall,sk	5 satisfactory	mortared_stone_brick fair	Embankment has minor ereno no	
																		Indent in metal 8 feet into the pipe. Moderate surface rust at bottom of pipe. Severe scour on				
		10/20/2020 17 05				and from	21	60 no		Contraction of the Contraction o	alliation						2 mainte	left side of culvert underneath pipe, approximately 12 inches	A	der total many total	Minor erosion on either sid no no	Scour below pipe brings overall rating down to fair. fair -71.0563 42.70194
B - 54i		10/29/2020 17:26 rain	ewm yes	yes	paved	satisfactory	21	50 NO	no	Cracks in roadway along pipe length.	elliptical roadway_aligned stres	an_aigned	nowing no		at_stream_grade corrugated_metal_pipe	55 40 28	3 satisfactory severe culvert	deep. headwall_retaining_wall,sk	Z.5 satisfactory	dry_laid_stone_brick satisfactory	will no erosion on either sid no no	Fallen limbs in streambed. Small
														within culvert.do			culvert.he	Minor surface rust throughout culvert. 11 inch deep severe				spot of foam near outlet possibly illicit discharge. Scour on right side
B - 54o		10/29/2020 17:46 rain	ewm yes	yes	paved	satisfactory	21	60 no	no	Cracks in roadway along pipe length.	elliptical roadway_aligned stres	am_aligned	flowing yes_<25%	within_culvert,do wnstream sediment	at_stream_grade corrugated_metal_pipe	55 31 58		S scour on the right side of culvert. headwall_retaining_wall,sk	3 fair	dry_laid_stone_brick satisfactory	Embankment has moderati no no	of culvert is cause for rating. 71.0565 42.7021
																		10 inches of scour within the culvert on the right. A foot and a half of metal is missing on the left				
B - 55i		10/29/2020 18:09 rain	ewm yes	yes	paved	good	25	35 yes overhead_wires	no	Edge of roadway is dangerously close to headwall only 2 feet before drop off.	round roadway_aligned stres	am_aligned	flowing yes_25_50%	upstream other	at_stream_grade corrugated_metal_pipe	12 9 12	3.5 failing severe culvert	side of culvert. Moderate surface 5 rust of entire pipe headwall_retaining_wall,ske	41 poor	dry_laid_stone_brick failing	Embankment show severe no no	Minor vegetation growth around opening. failing -71.0574 42.702
																		First foot and a half of culvert only has the bottom existing the				
																		top is gone. from one and a half to 5 feet into the pipe the right side is missing. Halfway into the				
										Edge of roadway has large drop off an outlet								culvert the pipe is shifted in, in one section. Severe rust				Due to missing metal within pipe severe scour is present on both
B - 55o		10/29/2020 18:28 rain	ewm yes	yes	paved	good	25	35 no	no	potentially dangerous.	round roadway_aligned strea	am_aligned	flowing no		perched corrugated_metal_pipe	12 13 6	1 failing severe culvert	3 throughout pipe. headwall_retaining_wall,sk	5 fair	dry_laid_stone_brick poor	Headwall stones appear to no no	o sides. Minor vegetation overgrowth. falling -71.0573 42.70204

B - 52i	10/29/2020 18:50 rain	ewm yes	yes	paved good	25	45 yes overhead_wires	по	round roadway_aligned stream_aligned	flowing no	at_stream_grade hdpe	15 15 13	4.5 satisfactory minor culvert	Two side-by-side pipes. Scour 6 located between both pipes.	headwall_retaining_wall,slo	4.5 poor dry_laid_stone_brick fair	Embankment has moderatino	Minor vegetation growth around inlet. fair -71.0586 42.7026
B - 52o	10/29/2020 19:03 rain	ewm yes	yes	paved good	25	45 no	yes bollard failing	round roadway_aligned stream_aligned	flowing no	perched hdpe	152 15 8	1 satisfactory none	101 Two side-by-side HDPE pipes.	headwall_retaining_wall,slo	6 fair dry_laid_stone_brick fair	Embankment has minor to no	no Moderate erosion and streambed. satisfactory -71.0586 42.70274 Stream shows minor erosion on
B - 47i	10/30/2020 12:18 snow	ewm yes	yes	paved fair	21	87 no	no Cracks on roadway along edge of head wall.	round skewed_>45° stream_aligned	no_dry_bottom yes_<25%	within_culvert leaves at_stream_grade corrugated_metal_pipe within_culvert,do	20 20 13	0 satisfactory none	7 Bottom opening of cover is bent. Close to 50% sediment buildup	headwall_retaining_wall,slo	2.5 fair mortared_stone_brick satisfa	tory Embankment shows minor no	no right side. satisfactory -71.0523 42.70475 Electrical box approximately 8 feet
B -47o	10/30/2020 12:41 snow	ewm yes	yes	paved fair	21	87 yes other	no General cracks along roadway.	round skewed_>45° no_discernable_stream_chann	el no_dry_bottom yes_25_50	wnstream leaves,sediment at_stream_grade corrugated_metal_pipe	20 16 19	0 satisfactory none	in middle of pipe.	headwall_retaining_wall,slo	3 fair mortared_stone_brick satisf	tory Embankment shows minor no	no from headwall. satisfactory -71.0519 42.70499
B - 305i	10/30/2020 13:02 rain	ewm yes	yes	paved fair	21	36 yes other	no	round roadway_aligned no_discernable_stream_chann	el no_dry_bottom yes_25_50	upstream,within_ culvert sediment,leaves inlet_drop corrugated_metal_pipe	12 8 12	0 none		headwall_retaining_wall,slo	3 satisfactory dry_laid_stone_brick fair	Embankment shows minor no	no fair -71.0498 42.70394
													Large amount of sediment downstream and within culvert,				
B - 305o	10/30/2020 13:16 snow	ewm yes	yes	paved fair	21	36	no	round skewed_<45° no_discernable_stream_chann	el no_dry_bottom yes_75_10	within_culvert,do % wnstream leaves,sediment buried_clogged_col corrugated_metal_pipe	12 6 12	0 failing none	won't be able to handle strong rainfall.	headwall_retaining_wall,slo	3.5 fair dry_laid_stone_brick fair	Embankment shows moderno	Large tree stump approximately 5 no feet away from outlet. poor -71.0497 42.70381
							Cracks located along roadway and along ler	math.					Moderate scaling on bottom of pipe from water. Third length of				Moderate vegetation growth covering outlet. General tree
B - 49o	10/30/2020 13:44 snow	ewm yes	yes	paved fair	25	72 yes drainage_system	no of pipe.	round skewed_<45° stream_aligned	no_moist_bottom no	perched reinforced_concrete_pipe	24 24 0	0 fair severe headwall	10 pipe of shift left about an inch. Minor scaling along bottom of	headwall_retaining_wall,slo	4.5 fair mortared_stone_brick failing	Embankment has moderatino y	yes growth. poor -71.0457 42.70427
													pipe due to water. On the right side of pipe there's a small chip				
							Cracks located along roadway and along len	th		vegetation_growt			of concrete missing. Small roadway reflectors are at base of				
B - 49i	10/30/2020 14:00 snow	ewm yes	yes	paved fair	25	72 yes drainage_system	no of pipe.	round skewed_<45° no_discernable_stream_chann	el no_dry_bottom yes_<25%	upstream h at_stream_grade reinforced_concrete_pipe	24 24 36	0 fair none		headwall_retaining_wall,slo	2 satisfactory mortared_stone_brick fair	Sporadic stones missing frono	yes fair -71.0456 42.70419
													opening. Minor scaling on bottom of culvert from water.				
													Minor rust spots from rebar within culvert.				Fallen limbs within stream and
B - 57i	10/30/2020 14:19 snow	ewm yes	yes	paved fair	22	80 yes water_line	yes guardrail satisfactory Minor cracks along roadway.	round roadway_aligned skewed_>45°	flowing no	at_stream_grade reinforced_concrete_pipe	60 60 22	2.5 satisfactory moderate culvert		headwall_retaining_wall,slo	3.5 fair dry_laid_stone_brick fair	Moderate erosion leading t no	no opening in culvert. satisfactory -71.046 42.7
													minor scaling at bottom of culvert due to water. Second				
													length of pipe in word is slightly shifted out. Small section of rebar				
													expose the top left corner. Graffiti president long entire				Streambed has severe erosian
													length of pipe. Nearby 12 inch RCP pipe, closed drainage				along headwall on edge of culvert. Fallen branches throughout no streambed. fair -71.0461 42.70002
B - 570 B - 580	10/30/2020 14:55 snow 10/30/2020 15:22 snow	ewm yes ewm culvert was not	yes	paved fair	22	80 yes drainage_system	yes guardrail satisfactory Minor cracks long roadway.	round roadway_aligned skewed_>45°	flowing no	perched reinforced_concrete_pipe	60 60 21	2.5 fair none	9 system.	headwall_retaining_wall,slo	5 satisfactory dry_laid_stone_brick fair	Embankment has moderati no	no streambed. fair -71.0461 42.70002 Pipe is part of closed drainage system, no culvert present71.0462 42.69978
B = 360	10/30/2020 13.22 SIIOW	ewiii cuwert_was_not_	ocated_d no										Minor scaling on your bottom of				system, no curvert present. 771.0402 42.05576
													pipe due to water. Concrete stamp on pipe: August 20, 1981.				
B - 59i	10/30/2020 15:40 snow	ewm yes	yes	paved satis	actory 24	85 yes drainage_system	no	round skewed_<45° skewed_<45°	flowing no	at_stream_grade reinforced_concrete_pipe	36 36 12	1 satisfactory minor headwall	4 Minor scaling at bottom of pipe	headwall_retaining_wall,slo	3 fair mortared_stone_brick satisf	tory Moderate erosion on eithe no	no Minor erosion of streambed. satisfactory -71.0455 42.69896 Close drainage system 12 inch RCP
													due to water. Slight cracking starting to form within the				pipe exiting out same out fall. Follow limbs and branches within
B - 59o	10/30/2020 15:53 snow	ewm yes	yes	paved satis	actory 24	85 yes drainage_system	no	round skewed_<45° stream_aligned	flowing no	at_stream_grade	36 36 17	2 satisfactory minor culvert	8 concrete.	headwall_retaining_wall,slo	3.5 satisfactory mortared_stone_brick satisfactory	tory Embankment is made up of no	no stream bed. satisfactory -71.0457 42.69914
													Minor scour with in first and second length connection. Minor				
B - 56i	10/30/2020 16:12 snow	ewm yes	yes	paved satisi	actory 30	90 yes other	yes guardrail satisfactory	round roadway_aligned stream_aligned	flowing no	at_stream_grade reinforced_concrete_pipe	60 60 26	3 fair minor headwall	scaling of pipe due to water. 7 Ponding towards center of pipe.	headwall retaining wall,slo	15 satisfactory dry_laid_stone_brick fair	Embankment has moderatino r	Fallen limbs and branches within no stream bed and a base of culvert. fair -71.0439 42.70063
									-				3 inch diameter chunk taken out				
													of side of culvert pipe approximately 5 feet in.				
													Approximately 6 inch wide, 1 foot long piece of concrete missing				
										leaves, branches, ot			from end of culvert at the bottom of the pipe. Majority of				Fallen trees and branches within
B - 56o	10/30/2020 16:36 snow	ewm yes	yes	paved satis	actory 30	90 no	yes guardrail satisfactory	round roadway_aligned stream_aligned	flowing yes_<25%	downstream her at_stream_grade reinforced_concrete_pipe	60 57 48	5.5 fair minor headwall		headwall_retaining_wall,slo	20 fair dry_laid_stone_brick fair	Embankment has a very steno	no stream bed. fair -71.0443 42.7
													Minor scaling throughout pipe. Scaling present were pipe links				
	40 700 70000 47 00	ewm yes								upstream,within_vegetation_growt	24 23 19	2.5 fair none	meet. Scaling on outside of culvert. Vegetation restricting		***	tory Moderate erosion on both no r	Vegetation overgrowth present throughout stream. satisfactory -71.0448 42.69796
B - 64i	10/30/2020 17:03 snow	ewm yes	yes	paved satis	actory 24	45 yes overhead_wires	no	round skewed_<45° stream_aligned	stagnant yes_<25%	culvert h,sediment at_stream_grade reinforced_concrete_pipe	24 23 19	2.5 fair none	3.5 flow at culvert opening.	headwall_retaining_wall,slo	5.5 fair mortared_stone_brick satisfa	tory Moderate erosion on both no r	no throughout stream. satisfactory -71.0448 42.69796 Large amounts of vegetation
B - 640	10/30/2020 17:23 snow	ewm ves				45 no	no Minor cracking along edge of roadway.	round skewed <45° stream aligned		within_culvert leaves, sediment at stream_grade reinforced_concrete_pipe	24 15 24	9 fair none	Moderate scaling throughout 4.5 pipe due to water.	headwall retaining wall,slo	8 satisfactory mortared stone brick satisf-	tory Embankment shows minor no	overgrowth throughout headwall, embankment and stream. fair -71.0449 42.69818
B - 040	10/30/2020 17.23 SHOW	ewiii yes	yes	paved saus	actory 24	45 110	mo will of clacking along eage of roadway.	Tourid Skewed_G45 Stream_angred	stagnant yes_12370	within_tuvert leaves, settinent at_stream_grade reminited_concrete_pipe	24 15 24	5 fall florie	Moderate surface rust within pipe. Pipe is partially bent at	neadwan_retaining_wan,sio	a satisfactory mortaled_stolle_orick satisf	tory embankment snows million no	embanisment and stream.
B - 63i	10/30/2020 17:42 snow	ewm yes	yes	paved satis	actory 22	43 no	no	round skewed_<45° stream_aligned	no_dry_bottom yes_50_7	within_culvert sediment at_stream_grade corrugated_metal_pipe	12 5 9	0 fair none	4 inlet. Moderate surface rust within	slope_steeper_than_2:1_	3.5 fair	Embankment has minor ero no	no Fallen trees in the stream. fair -71.042 42.69849
B - 63o	10/30/2020 18:06 snow	ewm yes	yes	paved satis	actory 22	43 yes overhead_wires	no	round skewed_<45° stream_aligned	no dry bottom no	at stream grade corrugated metal pipe	12 12 0	0 satisfactory minor culvert	4 pipe.	headwall retaining wall,slo	4 satisfactory dry_laid_stone_brick fair	Minor erosion on embankn no	no Minor vegetation and tree growth. satisfactory -71.0423 42.6983
									,								
							Roadway appears to have a large crack dow the median. Guard rails appear to be in okay						Minor scaling at bottom of culvert due to water, within both				Two side-by-side RCP pipes make up culvert. Minor vegetation growth
B - 43i	10/30/2020 18:23 snow	ewm yes	yes	paved fair	22	30 yes drainage_system	Roadway appears to have a large crack dow the median. Guard rails appear to be in okay yes guardrail satisfactor, order with minor cracks throughout the woo		flowing yes_<25%	up:tream leaves at_stream_grade reinforced_concrete_pipe	182 18 9	1 satisfactory none	Minor scaling at bottom of culvert due to water, within both 4 pipes.	headwall_retaining_wall,slo	5 satisfactory mortared_stone_brick fair	Headwall has mortar missir no r	up culvert. Minor vegetation growth no around stream. fair -71.0404 42.7067 Fallen tree limbs and branches
B - 43i	10/30/2020 18:23 snow	ewm yes	yes	paved fair	22	30 yes drainage_system	the median. Guard ralls appear to be in okay yes guardrall satisfactory order with minor cracks throughout the woo	d. round roadway_aligned stream_aligned			182 18 9	1 satisfactory none	culvert due to water, within both	headwall_retaining_wall,slo	5 satisfactory mortared_stone_brick fair	Headwall has mortar missir no r	nu p culvert. Minor vegetation growth na round stream. Faile -71.0404 42.7067 Fallen tree limbs and branches throughout stream and at culvert outlet. 18 inch RPO outlet piper from
			yes	paved fair	22		the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	 round roadway_aligned stream_aligned 	flowing yes_<25%	upstream leaves at stream_grade reinforced_concrete_ripe		culvert,he	culvert due to water, within both 4 pipes.				no gruivert. Minor vegetation growth no around stream, and brancher Falter tree limbs, and brancher Falter tree limbs, and brancher Lorder 1.8 inch RP outlet publish codes 1.8 inch RP outlet pipe from closed driange system approximately 10 feet away from
8 - 43i 8 - 43o	10/30/2020 18:23 snow 10/30/2020 18:46 snow	ewm yes	yes yes	paved fair	22	30 yes drainage_system 30 yes drainage_system	the median. Guard rails appear to be in okay yes guardrail satisfactor, order with minor cracks throughout the woo Crack salon center of roadway. Cracks along	d. round roadway_aligned stream_aligned			182 18 9 182 18 8	1 satisfactory none culvert,he 1.5 satisfactory minor advail	culvert due to water, within both		S satisfactory mortared_stone_brick fair 7.5 satisfactory mortared_stone_brick satisfactory		no producet. Minor vegetation growth no yor cluster. Minor vegetation growth Fallen tree limbs and branches throughout stream and it cultivent outlet. 18 inch NPC outlet pipe from outlet. 18 inch NPC outlet pipe from outlet display system outlet pipe from outle
			yes yes	paved fair	22		the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	 round roadway_aligned stream_aligned 	flowing yes_<25%	upstream leaves at stream_grade reinforced_concrete_ripe		culvert,he	culvert due to water, within both 4 pipes.				no producet. Minor vegetation growth no yor culvert. Minor vegetation growth fair 71,0404 42,7067 failen tree limbs and branches throughout stream and at culvert outlet. 18 inch IVC outlet pipe from closed draining system of closed fraining system on Closed raining system Octor charge system, outlet is closed and fully submerged and impossible to see through, 71,0433 42,70244
	10/30/2020 18:46 snow 11/2/2020 13:20 clear	ewm yes	yes yes no	paved fair	22	30 yes drainage_system	the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	 round roadway_aligned stream_aligned round roadway_aligned stream_aligned 	flowing yes_<25% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	182 18 8	culvert.he 1.5 satisfactory minor adwall	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, 811,	headwall_retaining_wall.slo	7.5 satisfactory mortared_stone_brick satisf	tory Embankment has moderatino r	po pulvert. Minor vegetation growth program or products and branches far products and branches far products and branches far products and branches far products and branches for throughout stream and at culvert outset. It should drainage system approximately 10 feet away from approximately 10 feet away from approximately 10 feet away from a satisfactory 71.0493 42.70692 (Cose drainage system, outlet is closed and fill by submerged and impossible to see through. 71.083 42.70724 (Southeast Cose approximately feet away from
	10/30/2020 18:46 snow	ewm yes	yes yes no yes	paved fair paved fair paved satisf	22 22 Sattory 22		the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	 round roadway_aligned stream_aligned 	flowing yes_<25% flowing no	upstream leaves at stream_grade reinforced_concrete_ripe		culvert,he	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, 811,				po pulvert. Minor vegetation growth sound stream and a school fair vegetation for throughout stream and a school fair vegetation for throughout stream and at culvert outset. 38 how hit Counted prings profession of counted drainage system on popularity (3 feet away from the counted outset of the counted outset ou
	10/30/2020 18:46 snow 11/2/2020 13:20 clear	ewm yes	yes no yes	paved fair paved fair paved satisf	22 22 actory 22	30 yes drainage_system	the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	 round roadway_aligned stream_aligned round roadway_aligned stream_aligned 	flowing yes_<25%	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	182 18 8	culvert.he 1.5 satisfactory minor adwall	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, 811,	headwall_retaining_wall.slo	7.5 satisfactory mortared_stone_brick satisf	tory Embankment has moderatino r	no prolivert. Minor vegetation growth some discrete fair virtuelle
	10/30/2020 18:46 snow 11/2/2020 13:20 clear 11/2/2020 13:26 clear	ewm yes wnb yes ewm yes	yes yes no yes	paved fair paved fair paved satisf	22 22 actory 22	30 yes drainage_system	the median. Guard rails appear to be in clay yes guardrail satisfactor, order with minor cracks throughout the woo Ozak salon center of readway. Cracks along length of pipe. Guard rail would shows mino	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	182 18 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortared_stone_brick satisf	tory Embankment has moderatino r	po pulvert. Minor vegetation growth sound stream and a state of the property o
	10/30/2020 18:46 snow 11/2/2020 13:20 clear	ewm yes	yes no yes ves cocated does no	paved fair paved fair paved satisf		30 yes drainage_system	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	 round roadway_aligned stream_aligned round roadway_aligned stream_aligned 	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	182 18 8	culvert.he 1.5 satisfactory minor adwall	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortared_stone_brick satisf	tory Embankment has moderatino r	to prolivert. Minor vegetation growth sound stream and such the sound stream and stream the sound stream and stream throughout stream and st culvert outset. If short IPC outset pipe from closed drainage system approximately 10 feet away from approximately 10 feet away from 100 closed stream and 100 closed s
8 - 430 8 - 306 8 - 441 8 - 440	10/80/2020 18-46 snow 11/2/2020 13-20 clear 11/2/2020 13-26 clear 11/2/2020 13-37 clear	ewm yes who yes ewm yes	yes no yes yes yes ocated_does_no			30 yes drainage_system 61 no	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe	182 18 8 24 24 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortaned_stone_brick satisfactory	tory Embankment has moderate no r General vegetation and tre no r	no producert. Minor vegetation growth sort and stream and branches fair failen tree limits and branches fair failen tree limits and branches sorties. It also the stream and branches context. It also the CP outlet per from coloud drainage system, outlet is closed drainage system, outlet is closed failing system, outlet is controlled failing series outlet drainings systems located near outlet of closeff closed arisings systems located near outlet of closeff closed arisings systems located near outlet of closeff closers arising systems located near outlet of closers arising systems arising
8 - 430 8 - 306 8 - 441 8 - 440	10/80/2020 18-46 snow 11/2/2020 13-20 clear 11/2/2020 13-26 clear 11/2/2020 13-37 clear	ewm yes who yes ewm yes	yes no yes yes yes tocated_does_no			30 yes drainage_system 61 no	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe	182 18 8 24 24 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortaned_stone_brick satisfactory	tory Embankment has moderate no r General vegetation and tre no r	no prolivert. Minor vegetation growth sound stream of the same of
8 - 430 8 - 306 8 - 446 8 - 440 8 - 42	10/80/2020 18-46 snow 11/2/2020 13-20 clear 11/2/2020 13-26 clear 11/2/2020 13-37 clear 11/2/2020 13-38 clear 11/2/2020 13-58 clear	ewm yes wnb yes ewm yes ewm yes ewm culvert_was_not_	no			30 yes drainage_system 61 no	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe	182 18 8 24 24 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortaned_stone_brick satisfactory	tory Embankment has moderate no r General vegetation and tre no r	po pulvert. Minor vegetation growth sound stream and successful for thoughout stream and at culvert outset. It should be thoughout stream and at culvert outset. It should be the country of the country
8 - 430 8 - 306 8 - 441 8 - 440	10/30/2020 18:46 snow 11/2/2020 13:20 clear 11/2/2020 13:26 clear 11/2/2020 13:37 clear 11/2/2020 13:58 clear	ewm yes ewm yes ewm yes ewm yes ewm culvert_was_not_	no			30 yes drainage_system 61 no	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe	182 18 8 24 24 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortaned_stone_brick satisfactory	tory Embankment has moderate no r General vegetation and tre no r	pro pulvert. Minor vegetation growth sound stream and such services of the ser
8 - 430 8 - 306 8 - 446 8 - 440 8 - 42	10/80/2020 18-46 snow 11/2/2020 13-20 clear 11/2/2020 13-26 clear 11/2/2020 13-37 clear 11/2/2020 13-38 clear 11/2/2020 13-58 clear	ewm yes wnb yes ewm yes ewm yes ewm culvert_was_not_	no			30 yes drainage_system 61 no	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the sec. Crack salon center of roadway. Cracks a longer of roadway. Cracks a longer of pope center of roadway. Cracks a longer of roadway. Cra	I. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_c45* no_discernable_stream_chann round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45* round skewed_c45*	flowing yes_425% flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe	182 18 8 24 24 8	cubert.he 1.5 satisfactory minor adwall 0.5 satisfactory minor cubert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wrapped around the edge of	headwall_retaining_wall.slo	7.5 satisfactory mortaned_stone_brick satisfactory	tory Embankment has moderate no r General vegetation and tre no r	no poulvert. Minor vegetation growth sourced stress and starselve fair processing stress and starselve fair processing stress and starselve fair throughout stress and starselve fair throughout stress and starselve fair stress processing system of the starselve fair stress processing stress and stress fair stress processing stress and stress stress stress stress fair stress processing stress around pipe culetts. The stress around pipe suites fair stress processing stress stress fair stress processing stress stress stress fair stress processing stress stress stress fair stress stress stress fair stress stress stress fair stress stress stress fair stress st
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B - 43o B - 306i B - 44i B - 44i B - 44c B - 42 B - 307i B - 35 B - 308i Located on Geograptowy/Bosford Bioc. B - 34i B - 34i B - 34i	10/30/2020 18-46 snow 11/2/2020 13-20 clear 11/2/2020 13-20 clear 11/2/2020 13-37 clear 11/2/2020 13-38 clear 11/2/2020 13-38 clear 11/2/2020 14-23 clear 11/2/2020 14-27 clear 11/2/2020 14-27 clear 11/2/2020 15-36 clear	ewm yes	no nocated_does_no yes yes yes	paved satisfaction paved satisfa	actory 26 actory 26 actory 26 actory 24 actory 24	30 yes drainage_system 61 no 61 yes overhead_wires,drainage 60 yes overhead_wires 60 no 40 yes overhead_wires 40 no 70 yes drainage_system	the median. Guard rails appear to be in olay yes guardrail satisfactory order with minor cracks throughout the work throughout the way. Crack salon center of roadway. Cracks along length of pipe. Guard rail would shows mino length of pipe. Guard rail would shows mino guardrail satisfactory cracking but appears to be OE. No Bollands and steel cable are coming spart we exposed reharm and almost every bolland steel cable are coming spart we exposed reharm and almost every bolland steel cable are coming spart we exposed reharm and almost every bolland steel cable are coming spart we exposed reharm on servicing as intended. Patch in middle of roadway along pipe fength. Spray paint marks along roadway identify writere pipe length is. Bibliands along grandway have exposed reharmous pipe fength. Minor cracking seen a long roadway. Wood guard rail seen on both sides appear to be in decent wood on the ground and seen along the set of the pipe length is satisfactory decent condition. Minor cracking seen a long roadway. Wood guard rail seen on both sides appear to be in decent condition.	t. round roadway_aligned stream_aligned round roadway_aligned stream_aligned round skewed_45° no_discernable_stream_chann round skewed_45° no_discernable_stream_chann round skewed_45° stream_aligned h round roadway_aligned skewed_45° round roadway_aligned stream_aligned n round roadway_aligned stream_aligned	flowing no flowing no el flowing no flowing no flowing no flowing no	upstream leaves at_stream_grade reinforced_concrete_pipe at_stream_grade hdpe at_stream_grade hdpe perched hdpe at_stream_grade hdpe at_stream_grade corrupted_metal_pipe upstream_within_sediment_leaves_o color: ther at_stream_grade corrupted_metal_pipe within_culvert sediment_other at_stream_grade corrupted_metal_pipe inlet_drap reinforced_concrete_pipe	182 18 8 24 24 8 24 24 8 36 36 16 36 36 15 22 18 22 22 19	1.5 satisfactory minor culvert 0.5 satisfactory minor culvert 0.5 satisfactory none 2 satisfactory none 2 satisfactory none 2 failing none 2 failing severe culvert 2 satisfactory minor culvert	culvert due to water, within both 4 pipes. 7 Minor scaling at bottom of pipes. Pipe is labeled: "6-26-15, B11, AK" Still has blue saran wrap wraped around the edge of pipe. Straw wrap still seen on edge of 4 pipe, not obstructing outlet. On top right side of culvert, starting from the beginning and going 3 inches in there is a stilly starting from the beginning and going 3 inches in there is a stilly starting from the beginning and going 3 inches in there is a stilly starting from the beginning and join on the left side of the pipe there's close to 5 feet of metal missing along russ present. The sides of the culvert have been rotted away cousing the pipes to be slight y cushed. Signal of the pipe there's close to 5 feet of metal missing along and water is running along streambed cushing major scour. Severe rust seen on 5 pipe. Minor scaling at bottom of pipe due to water. Minor moss growth seen as inite. Moderate scaling seen along	headwall_retaining_wall_slo slope_flatter_than_2:1_ slope_flatter_than_2:1_ slope_steeper_than_2:1_ tlope_steeper_than_2:1_ headwall_retaining_wall_slo headwall_retaining_wall_slo	7.5 satisfactory montared_stone_brick satisfactory 2.5 satisfactory 2.5 satisfactory 3.5 satisfactory 4.5 poor dry_laid_stone_brick failing 6 poor dry_laid_stone_brick failing	Embankment has moderate no General vegetation and tre no feneral vegetation and tre no final feneral vegetation and tre no final feneral vegetation and tre no final feneral vegetation and tre no fenbankment is comprised no fenbankment is comprised no fenbankment shows moderate no fenbankment shows moderate no fenbankment has milior enno	to publicit. Minor vegetation growth sound stream and a stachest other. It is a statistical to the statistic
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B - 309o B - 16 B - 19 B - 22	11/2/2010 17-53 clear ewm yes yes 11/2/2010 18-54 clear with cultert was not located for with cultert was not located for no	paved good 25	Boodway pavement appears to be less than a 52 yes overhead_wires no few years old. round roadway_aligned no_discernable_stream_channel stagnant	within CulverLup Stram, forwarder se_75_50% am sediment, Jeaves submerged corrupated_metal_pipe 12 7 10 3.5 fair none	Culvert is ponding at outlet. Upland area outside of outlet is close to 6 inches higher causing ponding. Culvert is submerged ponding. Culvert is submerged impection within culvert. headwall_retaining_wall_alo 5 fair mortared_stone_brick satisfactory Embankment has moderatino no	Calvert may not be capable of handling a heavy aim storm event. 12 in chi. VAPC colorest located approximately 15 feet away from outlet. Blefty protecting vioused, if executated slightly it will function much better. executed slightly it will function much better. fact 71.0526 42.71937 factored on private property. Licated on private property. Licated on private property. Variable 12.71792 No culvert present.
B - 53	11/2/2000 18:54 clear wnb culvert_was_not_located_does_no 11/2/2000 19:04 clear wnb culvert_was_not_located_does_no				Pipe appears to have an apphalytur mis covering throughout the pipe. Mis is starring to crack and his green	-1.03s 427037 -1.03s 427037
B - 310i	11/2/2020 19:13 clear ewm yes yes	paved satisfactory 20	31 no no round roadwy_aligned no_discernable_stream_channel no_dry_bottom	uptream,within_ s_25% culvert leaves,ediment at_stream_grade corrupated_metal_pipe 12 11 9 0 satisfactory none sediment_leaves, b within culvert.do ranches_vegetatio	from possible signe. Minor surface rust at bottom of pipe. Needwall_retaining_wall_slo 2 satisfactory mortared_stone_brick satisfactory Crack from top of head wal no no Tar/spalse mix throughout pipe. Mix is cracking and mixing some spaces. Moderate surface rust	p satisfactory -71.0298 42.7 Thorn bush located alone headwall
B - 310o	11/2/2020 19:31 clear ewm yes yes	paved satisfactory 20	31 yes overhead_wires no round roadway_aligned no_discernable_stream_channel no_dry_bottom		throughout pipe. headwall_retaining_wall.slo 2.5 fair mortared_stone_brick satisfactory Moderate vegetation grow no no Minor surface rust at bottom of	o and outlet. fair -71.0297 42.69985
B - 62i B - 62o	11/2/2020 19:52 clear ewm yes yes 11/2/2020 20:08 clear ewm yes yes	paved satisfactory 20 paved satisfactory 20	45 no yes guardrall satisfactory elliptical roadway_aligned no_discernable_stream_channel no_dry_bottom Gauerd rail shows moderate surface rust but 45 yes overhead_wires yes guardrail satisfactor, appears to be in good working order. elliptical roadway_aligned stream_aligned no_dry_bottom		pipe. headwall_retaining_wall_sto 4 satisfactory dry_laid_stone_brick satisfactory Embankment has minor we no no Minor surface rust at bottom of 5 pipe. headwall_retaining_wall_sto 5 satisfactory dry_laid_stone_brick satisfactory Headwall shows sporadic v no	satisfactory -71.0346 42.69855 General tree grove located near culvert. satisfactory -71.0344 42.69834
B - 71 B - 70	11/2/2020 20:34		Cracking present all throughout the road and		Sporadic patches of concrete	Located on private driveway71.0324 42.69637 Located on private driveway71.0308 42.69655
B - 87i	11/3/2020 13:15 clear ewm yes yes	paved fair 22	along the pipe length. No large port holes or cracks present that would potentially harm a car. round roadway_aligned no_discernable_stream_channel no_moist_bottom	within_culvertup s_<25% stream leaves,other at_stream_grade reinforced_concrete_pipe 36 35 17 0 satisfactory none s_<25% stream leaves,other at_stream_grade reinforced_concrete_pipe 36 35 17 0 satisfactory none	seen through joe. Minor revision seen wer first and second length of jips meet. headwall_retaining_wall_slo 2 satisfactory concrete_poured_ fair Minor vegetation growth. (no no Approximately 1 foot lints the curver there is 2 inch diameter piece of concrete inside, 84.	o satisfactory -71.0341 42.69215
B - 87o	11/B/2020 13:36 clear ewm yes yes	paved fair 22	Cracking seen all along roadway and along pipe length. Guard rail appears to have minor rot. 50 no yes guardrail fair starting at posts, but it still in working order. round roadway_aligned no_discernable_stream_channel no_moist_bottom Crackins seen throughout roadway and minor	at_stream_grade reinforced_concrete_pipe 36 36 0 0 satisfactory none	outet opening there is 1 inch of rebar showing through. Minor mosh growth and scaling at bottom of pipe. headwell_retaining_wall_slo 3.5 fair concrete_powred_ satisfactory Embankment is made up of no no Cracks seen within culvert.	o General tree growth. satisfactory -71.0341 42.6921
B - 80i	11/3/2020 13:59 clear ewm yes yes	paved fair 22	Clacking sees more gipe reight, No major crashing year minor crashing sees along pice reight, No major crashing sees along crashing sees along crashing. To und roadway_aligned no_discernable_stream_channel no_dry_bottom	o at_stream_grade reinforced_concrete_pipe 24 24 0 0 fair none	nations scaling at obtain or pipe. Second length of pipe has shift away. headwall_retaining_wall_slo 2 satisfactory concrete_poured_ satisfactory Embankment shows model no no	General tree growth seen within p inlet. fair -71.0326 42.69307
B-80o	11/3/2020 14:26 clear ewm yes yes	paved fair 22	Roadway shows cracking all along and along pipe length. Fire Pulptant scene next to culvert area. Guard all shows minor most powth on 55 yes drainage_system_water_line yes guardral fair to por frail and minor contigue aposts. other roadway_aligned no_discernable_stream_channel no_moist_bottom	within culvert.do leaves.sediment, v es 225% wantresim egetation.growth at itream grade reinforced concrete pipe 24 24 42 0 poor none	First length of pipe at outlet as separated caving a large amount of general than 2.1 2.5 fair Large amounts of general than 0.1 and 0.5 fairs and 0.5 fairs are separated to 0.5 fairs are separat	First pipe length of oalset has sparared causing large amounts of sediment to seep through. Outlet is a fillered mad section, with an initial width of 2 inches and an end of 46 or liches.
						Concrete flared end section approximately 30 feet away from
			General cracking seen along roadway with no		Minor cracking and scaling seen	headwall, likely from close drainage system. Headwall and culvert are difficult to get to do to thorn and
В - 77о	11/3/2020 14:58 clear ewm yes yes	paved fair 22	46 yes water_line_other yes guardrail fair major potholes. round skewed_745° no_discernable_stream_channel no_dry_bottom	o at_stream_grade reinforced_concrete_pipe 24 24 0 0 fair none	within culvert: headwall_retaining_wall,slo 4 fair concrete_poured_ satisfactory Embankment has moderatu no no	o general overgrowth. fair -71.0335 42.69423
B - 77i	11/3/2020 15:13 clear ewm yes yes	paved fair 22	Broadway shows moderate cracking with no 46 yes water_line,other no major potholes. round skewed_345° no_discernable_stream_channel no_dry_bottom	ss_c25% within_culvert branches at_stream_grade reinforced_concrete_pipe 24 22 12 0 fair none	Moderate thom bush overgrowth hindered observations and direct measurements. Minor scaling at bottom of cluvier. Minor moss growth at beginning of inlet. Second length of pipe shifted about 1 inch down. headwall_retaining_wall 4 satisfactory concrete_poured_ satisfactory Embankment shows minor no yet	4 inch HDPE pipe protrouting from ground mera bedwall, potential illicit discharge. Large cut down tree at base of headwall. Stump and roots left in place and are impeding water flow into inlet. Tires seen at sinlet. 250 pipe 10 p
						Fallen trees throughout stream bed. Large tree cracked on the middle looks like it's about to fall
B - 740	11/3/2020 15:48 clear ewm yes yes	paved 24	Minor cracking a long roadway. Roadway dips 45 no no entering both sides of inlet. round roadway_aligned stream_aligned no_dry_bottom	within_culvert.do ss_50_75% wmstream sediment at_stream_grade_reinforced_concrete_pipe 24 12 24 0 fair none	Filed with large amounts of 4 sediment. headwall_retaining_wall 3 fair concrete_pre_cost_ satisfactory Minor erosian along abater no no	potential danger to roadway, culvert, and headwall. fair -71.0314 42.69496 Concrete pad made culvert and
			Mimor cracking along roadway, Dipc in roadway		Culvert sitting on top of concrete pad. Culvert pile is on unlingsetable due to concrete	associated headwall unexpectable. Corrugated metal pipe seen under concrete pipe as well, origin is
B - 74i B - 73	11/3/2020 16:08 clear ewm yes no 11/3/2020 16:25 clear wnb culvert_was_not_located_d no	paved satisfactory 24	45 yes overhead_wires,drainage_system no entering inlet. round skewed_c45° no_discernable_stream_channel	reinforced_concrete_pipe 24	pad. slope_flatter_than_2-1_ satisfactory Embankment is essentially no no	p unknown71.0316 42.69486 Potential culvert was located on private property71.0258 42.69509
B - 76i	11/3/2020 16:35 clear ewm yes no	paved satisfactory 24	Roadway show minor cracking but looks to be guardrail satisfactory well-keys. Roadway shows minor cracking but appears to			Headwall of culvert was locatable but the rest of the culvert and the headwall was completely buried71.0235 42.69441
B - 76o	11/3/2020 16:39 clear wnb culvert_was_not_located_d no	paved satisfactory 24	be in good working order. God rail shows spinificant surface rus but appears to be in yes overhead wires yes guardrail satisfactor; good structural order.			Culvert outlet was unlocatable potentially buried under soil71.0235 42.6942
					Minor scaling opening and bottom of pipe. Minor moss growth on outside and initide of	Fallen tree limbs and branches near
B - 85i	11/3/2020 17:02 clear ewm yes yes	paved satisfactory 30	140 yes overhead_wires no Minor cracking and patches along roadway: round skewed_z45' skewed_c45' no_dry_bottom	o at_stream_grade reinforced_concrete_pipe 15 15 0 0 satisfactory none	3 pipe. headwall_retaining_wall_slo 2.5 poor mortared_stone_brick fair Moss growth seen over heino no	p inlet. fair -71.0153 42.69265 12 inch corrugated metal closed drainage system outlet located next to culvert outlet. Corrugated metal outlet bas guard rail post impailing
B - 85o	11/3/2020 17:21 clear ewm yes yes	paved satisfactory 30	Minor surface rust on guard ralls. Minor 140 yes overhead_wires yes guardrall satisfactory cracking along roadway. round skewed_>45° no_discernable_stream_channel stagnant	ss_50_75% within_culvert sediment at_stream_grade reinforced_concrete_pipe 15 7.5 15 0.5 fair none	Moderate scaling at bottom and sides of pine. Beadwall_retaining_wall_side 5.5 poor mortared_stone_brick poor Embankment show severe no no Potential puncture at top of pipe.	middle of pipe, blocking it completely. fair -71.0152 42.69228
8 - 2110	11/3/2020 17:51 clear ewm yes yes	paved satisfactory 30	Guard rail shows minor surface rust. Roadway yes overhead wires, drainage, system yes guardrail satisfacton has minor cracking, round skewed_485° no discensable_stream_channel no_moist_bottom	o perched corrugated_metal_pipe 12 12 0 0 fair none	recensian join.cute et a top or jupie apprentant part of the programment of the programme	General tree growth located on embankment and wetland area. poor -71.0152 42.69221
B - 311i	11/3/2020 18:03 clear ewm culvert was not located d no	paved satisfactory 30	yes drainage system no Minor cracking along the roadway with patches, round	corrugated metal pipe 12	Activate transference paper. Angle_neeper_time_e-r	Pipe is likely part of closed drainage system, pin is on top of catch basin71.015 42.69249
			,,		Moderate scaling at bottom of pipe. Entrance of pipe has moderate scaling and exposed	
B - 86i	11/3/2020 18:17 clear ewm yes yes	paved satisfactory 26	80 no no Roadway shows minor cracking and patches. round skewed_>45' no_discernable_stream_channel no_dry_bottom	o at_stream_grade reinforced_concrete_pipe 12 12 0 0 fair minor culvert	rebar. headwall_retaining_wall.slo 5.5 fair mortared_stone_brick fair Embankment has moderat no no Second length of pipe is shifted	p fair -71.0146 42.69224 Pipe is comprised of ductile iron
				within_culvert.up	down and left approximately 1 inch. Pipe is comprised of Ductile iron jpipe and reinforced concrete	and a reinforced concrete pipe, appears to be connected somewhere in the middle. Fallen
B - 860	11/3/2020 18:31 clear ewm yes yes	paved satisfactory 26	80 yes overhead_wires no Roadway.shows.minor.cracking.in.patches. round skewed_>45° no_discernable_stream_channel no_dry_bottom		pipe connected together. slope_flatter_than_2:1_ 6.5 fair Embankment has minor err no no Moderate scaling on sides of	o utility pole located at pipes outlet. satisfactory -71.0149 42.69209
B - 82i	11/3/2020 19:00 clear ewm yes yes	paved satisfactory 24	Roadway is cracking slightly with some patches. Guard fall shows moderate nut and partial 65 no yes guardrall fair retting a posts. round roadway_aligned no_discernable_stream_channel flowing	leaves, vegetation leaves, vegetation growth, branches at_stream_grade reinforced_concrete_pipe 60 32 48 15 fair none	pipe and presumably the bottom as well, Sognatin missing divots in concrete. Metal grates placed in from Collecter preventing denon from failing un. headwall_retaining_wall_ulo 1.5 poor concrete_pre_cast_ poor Moderate erosion around syes no Moderate straige of sides and	Falsen branches and trees leading into water body. Portion of a unity in the body. Portion of a unity per common of a unity per common of a butting neighbor mention beaver activity with and outside of
B - 82o	11/3/2020 19:26 clear ewm yes yes	paved satisfactory 24	65 yes overhead wires no Minor cracking in patches along roadway, round roadway_aligned stream_aligned flowing	within culvert do leaves, sediment, b 12,55,50% wnstream ranches at stream grade reinforced_concrete_pipe 60 45 60 12 fair none	Monocrete stuning or since and presumably bettom of pipe. Sporadic places of rebar showing 2 through Through	culvert. Potential beaverdam located just outside of culvert
B - 81 B - 95	11/3/2020 19:45 clear wmb culvert_was_not_located_d no					Pye Brook crossing located at 8 - 82. No culvert found. Close drainage system located at point71.0195 42.69306 No culvert was found71.0126 42.69339
B - 79o	11/3/2020 20:01 clear wmb culvert_was_not_located_d no 11/4/2020 13:13 clear other yes yes	paved fair 21	no no Cracking seen along roadway. round skewed 345° no discernable stream channel no dry bottom	o at_stream_grade_reinforced_concrete_pipe 12 12 0 0.good_none	slope_flatter_than_2:1_ S_satisfactory General vegetation growth. no no	Minor vegetation growth. 18 inch tree approximately 3 feet away
B - 79i	11/4/2020 13:48 clear other culvert was not located d no	paved fair 21	no no Minor cracking seen along roadway.		n	Culvert inlet was not found potentially buried under sediment or and leaves71.01 42.69305
B - 100i	11/4/2020 13:57 clear other yes yes	paved 21	85 no no Minor cracking along length of razdway, round skewed_45° no_discernable_stream_channel no_dry_bottom Guard rall appears to be struck by a vehicle/broken multiple sections. Cracking	at_stream_grade reinforced_concrete_pipe 18 18 0 0 good none	headwall_retaining_wall,slo \$ fair dry_laid_stone_brick satisfactory Embankment has spots of no no	satisfactory -71.0069 42.68723 Adjacent catch basin outlet directly
B - 100o	11/4/202014:31 clear other yes yes	paved satisfactory 21	85 yes drainage_system yes guardrall failing along sections of roadway. round skewed_c45' skewed_z45' no_moist_bottom	s_225% within_culvert sediment at_stream_grade reinforced_concrete_pipe 18 15 16 0 good none	9 slope_flatter_than_2-1_ 10 satisfactory General vegetation growth no no Minor scaling at waterline.	o next to culvert outlet. satisfactory -71.007 42.6872
B - 101o	11/4/2020 15:00 clear other yes yes	paved satisfactory 19.5	Roadway has 2 foot tall montared stone walls, acting as traffic safety, Minor cracking seen long 40 yes other,drainage_system yes other satisfactor, roadway. Tound roadway, aligned stream_aligned no_moist_bottom	branche vegetatil on growth leaves, culvert.he as _25% downstream sediment at stream grade reinforced_concrete_pipe 24 24 0 0 satisfactory minor advail	Minor delamination/palling at outlet. Cultwis comprised of three side-by-side 24 inch diameter RDF opins. Span scross 23 all pipes is 89" headwall_retaining_wall 2 satisfactory mortared_stone_brick satisfactory isolated moderate voids/lurno nc	Close drainage outlet located near culvert71.0087 42.88687
B - 101i	11/4/2020 15:37 clear other yes yes	paved satisfactory 19.5	Roadway shows minor cracking, 2 foot tall 40 yes drainage system, other yes other satisfacton montar and stone wall acting staffic safety. round roadway aligned no discernable stream_channel no_moist_bottom	o at_stream_grade_reinforced_concrete_pipe 24 24 0 0.good_none	Culvert comprised of 3 - 24 inch RCP pipes with a span of approximately 9 feet. headwall_retaining_wall 2.5 satisfactory mortared_stone_brick_fair Moderate voids between sino no	
B - 97i	11/4/2020 16:07 clear other yes yes	paved satisfactory 22.5	Minor cracking along roadway, Cracking seen 40.5 yes overhead_wires no along pipe length. round roadway_aligned no_discernable_stream_channel no_dry_bottom		slope_steeper_than_2:1_ 2.5 fair One and a half feet off of e no no	Culvert inlet is located within a

																					Small downed trees just above	
B - 97o	11/4/2020 16:38 clear	other yes	yes	paved	satisfactory	22.5	40.5 yes ov	werhead_wires	no	Minor cracking seen along roadway. Minor cracking seen along pipe length.	round roadway_aligned stream_aligned	no_dry_bottor	m no	at_stream_grad	de hdpe	12 12 0	0 good none	4 Channel lined with rocks. headwall_reta Puncture located at segment	ing_wall,slo 3 satisfactory dry_laid_stone_b	orick fair Rock lined embankment sk no	streambed. General vegetation no growth. good	-71.0045 42.68755
B-98o	11/4/2020 16:55 clear	other yes	yes	paved	satisfactory	24	38 no		no	Minor cracking/dips seen on roadway.	round roadway_aligned no_discernable_stream_ch	nannel no_dry_bottor	m no	at_stream_grad	de corrugated_metal_pipe	16 16 0	0 fair none	joint and edge of roadway. slope_steeper	han_2:1_ 3.5 satisfactory	General vegetation growth no	Culvert inlet has an adjacent	-71.0027 42.68738
																					driveway culvert approximately 2 feet away. Adjacent parallel roadway shoulder culvert located	
B - 98i	11/4/2020 17:10 clear	other yes	yes	paved	satisfactory	24	38 yes ov	verhead_wires	no	Cracking and dip seen a along roadway.	round roadway_aligned skewed_>45°	no_dry_botton	m no	at_stream_grad	de corrugated_metal_pipe	16 16 0	0 good none	Small puncture located near 5 outlet side. headwall_reta	ing_wall_slo 2 satisfactory dry_laid_stone_b	orick satisfactory Stone placed overhanging (no	approximately 15 feet away from	-71.0026 42.68749
B - 110o	11/4/2020 17:37 clear	other yes	yes	paved	satisfactory	25	45 no		no	General cracking along roadway.	round roadway_aligned stream_aligned	no_dry_bottor	m no	at_stream_grad	de smooth_metal_pipe	15 15 0	headwall,c 0 good minor ulvert	Two pieces of steel rebar 7 partially blocking pipe outlet. headwall_reta	ing_wall,slo 4 satisfactory mortared_stone	_brick satisfactory Minor undermining of ston no	Rebar likely acting as a beaver no fence. good	-70.9977 42.68513
																		Sediment buildup halfway within the culvert. Slight bend in middle			Large beaver fence on outside of pipe preventing direct	
B - 110i	11/4/2020 18:00 clear	other yes	yes	paved	satisfactory	25	45 yes ov	verhead_wires	no	Minor cracking seen along roadway.	round roadway_aligned skewed_<45*	no_dry_bottor	m no	at_stream_grad	de smooth_metal_pipe	18 18 0	0 good none	7 of pipe. Minor rusting at joints. headwall_reta	ing_wall,slo 2.5 satisfactory mortared_stone	_brick satisfactory Minor voids between stone yes	no measurements. satisfactory	-70.9975 42.68516
																		Inlet has bottom of pipe missing approximately 3' inward. Signs of				
																	culvert,he	potential beaver fence at one point. Pipe is elevated			Potential beaver den located	
B - 115i	11/4/2020 18:24 clear	other yes	yes	paved	good	25	50 yes ov	verhead_wires	no	General cracking along roadway.	round roadway_aligned stream_aligned	flowing	no	perched	hdpe	12 12 0	0 satisfactory moderate adwall	20 approximately 1 foot in the air. other,slope_st Pipe is submerged and cannot be	eper_than_2 4 fair dry_laid_stone_b	orick satisfactory Embankment is reinforced yes	no directly next to culvert inlet. fair	-70.9953 42.68369
																		inspected inside. Bottom half of pipe entrance appears to be missing. Scour appears to be				
B - 115o	11/4/2020 18:51 clear	other yes	yes	paved	satisfactory	25	50 yes w	water_line	no	General cracking along the roadway.	round roadway_aligned stream_aligned	stagnant	no	submerged	hdpe	12 12 12	culvert, ot 24 fair severe her	around the culvert pipe and 10 along ditch. slope_steeper	han_2:1_ 4.5 poor	Embankment lined with mi yes		-70.9956 42.68359
R - 114i	11/4/2020 19:13 clear	other yes	Long .	named	ratirfactory	24	55 no		yes guardrail good	Guard rail has minor surface rust. Visible patch along roadway and along pipe length.	round skewed_<45° no_discernable_stream_ch	nannel no_dry_bottor	m no	perched	hdos	15 15 0	0 good minor culvert	Scour is located at base of inlet. slope_steeper	han 2:1 4 satisfactory	General vegetation growth yes	Evidence of a former beaver fence. Black blankets laid over former no beaver fence area. satisfactory	-70.9945 42.6838
B - 114o	11/4/2020 19:29 clear	other yes	yes	paved	satisfactory	24	55 yes ov	verhead_wires	no second	General cracking and patches long roadway.	round skewed_<45° skewed_<45°	no_dry_botton		at_stream_grad	de hdpe	15 15 0	0 good none	Cascading slope at end of 8 culvert. slope flatter 1			Potential evidence of former	-70.9947 42.68377
										Minor patches along roadway. Surface rust								Moderate rusting at water level. Tar lining has worn away at water level. Minor section lost near			Streambed lined with	
B - 96i	11/4/2020 19:48 clear	other yes	yes	paved	satisfactory	25	45 yes ov	verhead_wires	yes guardrail satisfact		round roadway_aligned skewed_<45°	no_moist_bott	tom no	at_stream_grad	de corrugated_metal_pipe	18 18 8	0 fair none	6 pipe inlet. slope_steeper	han_2:1_ 7 fair	Potential headwall may ha no	no miscellaneous stones. fair	-70.993 42.68913
B - 96o	11/4/2020 20:08 clear	other yes	Long C	paved	satisfactory	25	45 yes ov	conthand wirer	yes guardrail satisfact	Patches along roadway. Minor surface rust on	round roadway aligned no discernable stream ch	annel no moist hott	tom use <25%	within culvert leaves, sediment at stream grad	de comunited metal nine	18 18 12	0 fair none	Ruston present at waterline. Tar lining has worn away at waterline. Minor rusting at joint. headwall reta	ing wall,slo 6.5 satisfactory mortared stone	brick poor Major spalling in headwall ino	18 inch fallen tree approximately 5 feet away from outlet. Initial no streambed line with stone. fair	-70.9928 42.68914
B - 78 B - 84	11/4/2020 20:33 clear 11/5/2020 13:17 clear	wnb culvert_was	s_not_located_does_no s_not_located_d no	pares	жизиссоту		45 (65	remad_wites	yes guardian saturate	ton 1 gour o' ren.	Tours Toursy_signed To_duceThabse_stream_cr	miner no_nour_occ	1011 YES_123N	wom_cover seaso, seament at_attent_grav	se congues_mean_pipe	10 10 11	O IIII IIII		mg_wampio 0.3 automotory moreure_score	and poor major spaning in readwarms	Culvert does not exist.	-70.9933 42.69379 -70.9909 42.69256
																		Moderate surface rust throughout pipe. Pipe lined with an asphalt/tar mix.			Minor vegetation growth at inlet. 12 inch RCP privately owned culvert	
																		Approximately 12 inches of right side of pipe is missing. Moderate			approximately 30 feet away. Culvert located on Boxford-	
B - 65i	11/5/2020 13:33 clear	ewm yes	yes	paved	satisfactory	22	45 yes w	vater_line	no	Minor cracks and patches and roadway.	round skewed_<45° no_discernable_stream_ch	nannel no_dry_bottor	m yes_<25%	within_culvert sediment,leaves at_stream_grad	de corrugated_metal_pipe	18 12 17	0 poor minor culvert	to severe surface rust throughout pipe. headwall_reta	ing_wall,slo 6 satisfactory mortared_stone	_brick fair Minor vegetation growth. A no	Georgetown line, most likely with in no Boxford. fair General tree growth. Headwall	-70.9876 42.69799
																		Bottom and sides of pipe missing from outlet to more than			appears to be in decent condition but pipe is in major disrepair.	
																		halfway. Top pipe pipe is lined with tar/asphalt mix. Severe			Culvert located next to Boxford Georgetown line, most likely with in	
B - 650	11/5/2020 13:53 clear	ewm yes	yes	paved	satisfactory	22	45 yes ov	verhead_wires,water_line	no	Minor cracking and patches in roadway.	round skewed_<45° no_discernable_stream_ch	nannel no_dry_botton	m yes_25_50%	within_culvert sediment,leaves at_stream_grad	de corrugated_metal_pipe	18 11 20	0 failing moderate headwall	surface rust. headwall_reta On left side of pipe there are four strips of rebar showing	ing_wall,slo 6 fair mortared_stone	brick fair Embankment shows model no	no Boxford. poor	-70.9877 42.69806
B - 61i	11/5/2020 14:22 clear	ewm yes	yes	paved	good	22	60 yes dr	rainage_system	no		round roadway_aligned no_discernable_stream_ch	nannel no_dry_botton	m no	at_stream_grad	de reinforced_concrete_pipe	12 12 0	0 satisfactory none	through. Minor cracking forming on right side of pipe. slope_steeper	han_2:1_ 4.5 satisfactory	Moderate tree growth. Mir no	Pipe opening is a flared end no section. satisfactory	-70.9947 42.69848
B - 610	11/5/2020 14:42 clear	ewm ves	wes	paved	satisfactory	22	60 no		20		round roadway_aligned no_discernable_stream_ch	sannel no dry hotton	m no	at stream grad	de reinforced_concrete_pipe	12 12 0	0 satisfactory none	Second length of pipe has shifted approximately 2 inches outward from inlet. slope_steeper.	han 2:1 5.5 fair	Embankment shows moderno	Pipe is comprised of a flared end no section. satisfactory	-70.9948 42.69857
	,-,	,	,	,		-							-					Left side opening has a sizable			,	
														within culvert-up				crack. Second length of pipe is shifted up and to the right. Minor moss growth. Opening of pipe			Streambed is lined with rocks, and	
B - 68i	11/5/2020 15:12 clear	ewm yes	yes	paved	good	22	60 yes ot	ther,water_line,gas_line	no		round roadway_aligned skewed_<45°	no_dry_bottor	m yes_<25%		de reinforced_concrete_pipe	16 16 27	0 fair none	4 has a flared end section. slope_flatter_t	an_2:1_ 3 satisfactory	Minor vegetation growth a no	no likely man-made. fair	-70.9983 42.6971
																		Scour located underneath ending of pipe. Roots growing over entrance. Inside the second			End of pipe is made of a flared end	
B - 68o	11/5/2020 15:25 clear	ewm yes	yes	paved	good	22	60 yes ga	as_line,water_line,other	no		round roadway_aligned no_discernable_stream_ch	nannel no_dry_botton	m no	perched	reinforced_concrete_pipe	16 16 0	0 satisfactory moderate culvert	length of pipe is shifted up and to the left. slope_steeper	han_2:1_ 4 fair	Embankment is comprised no	section. Steep drop off directly after	-70.9984 42.69711
																		Top section of culvert is cracked and slightly shifted at opening.				
																		That second length of pipe there is approximately a 2 inch opening				
B - 312i	11/5/2020 15:54 clear	ewm yes	yes	paved	satisfactory	20	35 yes ot	ther,water_line	no	Minor cracking along roadway.	round skewed_<45° no_discernable_stream_ch	nannel no_dry_botton	m yes_25_50%	other,leaves,sedi upstream ment at_stream_grad	de reinforced_concrete_pipe	12 12 0	3 fair none	and it just shifted slightly to the right. headwall_reta	ing_wall,slo 3.5 poor dry_laid_stone_b	orick failing Embankment shows moderno		-70.9995 42.6964
B = 312o B = 313o	11/5/2020 16:13 clear 11/5/2020 16:26 clear	ewm yes ewm yes	yes yes	paved paved	satisfactory satisfactory	20 20	35 yes ga 40 yes ot	as_line ther,water_line	no no	Minor cracking along roadway. Minor cracking along roadway.	round skewed_<45° no_discernable_stream_ch round skewed_<45° no_discernable_stream_ch	nannel no_dry_botton	m yes_<25% m yes 25 50%	within_culvert sediment at_stream_grad within_culvert sediment at_stream_grad	de reinforced_concrete_pipe de reinforced_concrete_pipe	12 9.5 11 12 8 12	0 satisfactory none 0 satisfactory minor headwall	Minor scaling along bottom and sides. headwall_reta headwall_reta	ing_wall,slo 4 fair dry_laid_stone_b ing_wall,slo 3.5 fair dry_laid_stone_b		General tree growth directly no outside of culvert pipe. fair no fair	-70.9995 42.69647 -70.9994 42.69652
																		Second length of pipe is slightly				
B - 313i	11/5/2020 16:46 clear	ewm yes	yes	paved	satisfactory	20	40 yes ot	ther,water_line	no	Minor cracking along roadway.	round skewed_<45° no_discernable_stream_ch			at_stream_grad within_culvert,up	de reinforced_concrete_pipe	12 12 0	0 fair none	shifted apart. Small piece of concrete missing from pipe top. slope_steeper Second length of pipe is slightly	han_2:1_ 3.5 fair other	fair Embankment is covered wi no		-70.9994 42.69658
B - 314i B - 314o	11/5/2020 16:58 clear 11/5/2020 17:08 clear	ewm yes ewm yes	yes yes	paved paved	satisfactory satisfactory	20 20	37 yes ot 37 yes wa	ther,water_line vater_line,other	no no	Minor cracking along roadway. Minor cracking along roadway.	round skewed_<45° no_discernable_stream_ch round skewed_<45° no_discernable_stream_ch	nannel no_dry_botton nannel no_dry_botton	m yes_50_75% m yes_25_50%		de reinforced_concrete_pipe de reinforced_concrete_pipe	12 6.5 11.5 12 8 12	0 satisfactory none 0 satisfactory none	shifted away. slope_steeper headwall_reta			no satisfactory no General tree growth. fair	-70.9993 42.69644 -70.9994 42.6966
																		Suck length of pipe is slightly shifted out, with a miscellaneous				
B - 315i	11/5/2020 17:33 clear	ewm yes	yes	paved	satisfactory	22	30 yes ot	ther,water_line	no	Minor cracking along roadway.	round roadway_aligned no_discernable_stream_ch	nannel no_dry_botton	m no	at_stream_grad	de reinforced_concrete_pipe	12 12 0	0 satisfactory none	stone in the middle. slope_flatter_t Headwall has collapsed in on	an_2:1_ 1.5 satisfactory	Minor vegetation growth. no	no satisfactory	-71.0019 42.6945
																		culvert completely obstructing front end of outlet. When culvert			Moderate vegetation growth and	
B - 215o	11/5/2020 17:42 clear	marm. Land	Long C	named	ratirfactory	22	30 yes ot	ther,water line		Minor cracking along roadway.	round roadway_aligned no_discernable_stream_ch	sannel no dry hotton	m completely obstru	other,vegetation_ downstream,with growth,sediment,l ctein_culvert eaves buried_clogged	d_col reinforced_concrete_pipe	12 4 12	0 failing none	was found it was completely covered with sediment and vegetation. headwall_reta	ing_wall,slo 3 fair dry_laid_stone_b	srick failing Embankment rhour minor no	thorn bushes directly after outlet. Culvert likely in OK condition but no headwall drastically needs repairs. failing	-71.002 42.69455
5-3130		cum yes	,cs	pares	жизиссоту	**								within culvert,do sediment,other,le			-	Obstructions are restricting full			Inside of culvert looks satisfactory but headwall and outfall area are	
B - 316o	11/5/2020 18:04 clear	ewm yes	yes	paved	satisfactory	22	30 yes wa	vater_line,other	no	Minor cracking along roadway way.	round skewed_<45° no_discernable_stream_ch	nannel no_dry_botton	m completely_obstru	ctewnstream aves at_stream_grad	de reinforced_concrete_pipe	12 4 12	0 fair none	inspection of the culvert. headwall_reta	ing_wall,slo 2.5 satisfactory dry_laid_stone_b	orick failing Headwall stones are active no	no failing. failing Culvert pipe and headwall are completely buried by rocks and	-71.002 42.69459
																					landscaping. Could not inspect headwall and culvert properly due	
B - 316i	11/5/2020 18:23 clear	ewm yes	no	paved	satisfactory	22	30 yes ot	ther,water_line	no	Minor cracking along roadway.	round		completely_obstru	cteupstream other,sediment	reinforced_concrete_pipe	12 0		slope_flatter_t Inlet opening has exposed rebar and several chunks of concrete	an_2:1_ fair	Embankment made up of la no	no to obstruction.	-71.002 42.69445
B - 317i	11/5/2020 18:32 clear	ewm yes	yes	paved	satisfactory	22	30 yes w	vater_line,other	no	Minor cracking along roadway.	round roadway_aligned no_discernable_stream_ch	nannel no_dry_botton	m yes_75_100%	upstream,within_ culvert sediment at_stream_grad	de reinforced_concrete_pipe	12 5.5 12	0 poor none	missing, with rebar falling into the culvert. slope_flatter_t	an_2:1_ 1.5 satisfactory	Minor erosion leading to in no	Inlet opening was almost 100% no covered upon arrival. poor	-71.0019 42.69452
																		Culvert has large pieces of concrete missing and exposed				
														within_culvert,do				rebar. Inside of culvert is completely obstructed by			Upon arrival headwall stones completely covered and obstructed	
B - 317o	11/5/2020 18:50 clear	ewm yes	yes	paved	satisfactory	22	30 yes ot	ther,water_line	no	Minor cracking along roadway Roadway shows minor cracking. Bollard likely	round roadway_aligned no_discernable_stream_ch	nannel no_dry_botton	m completely_obstru	ctewnstream sediment,other at_stream_grad	de reinforced_concrete_pipe	12 0 12	0 failing none	sediment, preventing inspection. headwall_reta Middle length of pipe has moderate rock buildup. Minor	ing_wall,slo 2.5 satisfactory dry_laid_stone_b	orick failing Headwall stones are active no	no outlet. failing	-71.0019 42.69454
										part of concrete and steel cable guard rail. Ballard has exposed rebar and is actively								surface rust at bottom of pipe. Wetland flag mark next to culvert			Large overgrowth of thorn bushes	
B - 72o	11/5/2020 19:34 clear	ewm yes	yes	paved	satisfactory	18	28 yes dr	rainage_system,water_line,gas_lin	e yes bollard failing	overturning.	round roadway_aligned stream_aligned	no_moist_bott	tom yes_<25%	within_culvert other perched	corrugated_metal_pipe	24 24 0	0 satisfactory none	6 outlet. headwall_reta	ing_wall,slo 3 fair dry_laid_stone_b	orick fair Moderate erosion of emba no	no at outlet opening and embankment. fair Embankment headwall were	-71.0073 42.69512
																					observable upon inspection. Culvert pipe was unexpectable due to large	
B - 72i	11/5/2020 19:48 clear	ewm ves		paved	satisfactory	18	20 de	rainage system,gas line,water lin		Minor cracking along roadway.	round roadway aligned stream aligned			cteupstream other	corrugated metal pipe	24		Culvert opening is blocked by large stone preventing 4 inspection. headwall reta	ing wall,slo 3 poor dry laid stone b	orick poor Stone covering inlet appearno	stone fallen from headwall. Stream lined with general vegetation no growth and thorns. failing	-71.0073 42.69509
6-721	11/5/2020 15.46 Ceal	ewiii yes	110	paveu	satisfactory	10	20 yes ui	ramage_system,gas_mie,water_mi	ie,mo	willor cracking along roadway.	Tourid Touriway_angried Stream_angried		completely_oustru	ccapstream other	corrugateu_metar_pipe	24		Minor moss growth. Minor scaling a bottom of pipe. Covert	ing_waii,sio s pool uiy_aau_stone_t	stok pool score covering met appearno	no growth and chorns. raming	71.0073 42.05305
																		opening appeared to be purposefully obstructed with			6 inch concrete area drain located	
																		rocks. 1 inch diameter blue pipe coming out of and through culvert. Culvert length			approximately 2 feet away from culvert. Culvert appeared to be purposely covered along with 6 inch	
B - 102i	11/6/2020 13:01 clear	ewm yes	yes	paved	fair	23	50 yes ov	verhead_wires	no	Roadway shows moderate cracking in minor potholes.	round roadway_aligned skewed_<45°	no_moist_bott	tom no	at_stream_grad	de reinforced_concrete_pipe	15 15 0	culvert,he 0 satisfactory moderate adwall	measurement is an 2.5 approximation. headwall_reta	ing_wall,slo 4 satisfactory mortared_stone	brick fair Minor vegetation growth a no	area drain. 1 inch blue pipe appears	-71.0478 42.68694
																					Culvert and headwall were uninspectable due to large pile of	
										Moderate cracking and potholes along								1 inch diameter blue pipe sticking out of where culvert may			debris likely placed by owner of property. 1 inch diameter pipe was	
B - 102o	11/6/2020 13:42 clear	ewm yes	no	paved	tair	23	50 no		по	roadway.	round			legyes, wegetation	reinforced_concrete_pipe	15		be.	poor	Embankment shows moder no	yes visible outside of debris pile. Downstream approximately 80 feet away there's a private culvert	-71.0479 42.68683
B - 106o	11/6/2020 13:55 clear	ewm yes	yes	paved	satisfactory	19	55 yes ov	verhead_wires,gas_line	no	Cracking along pipe length.	round roadway_aligned skewed_<45°	stagnant	yes_<25%	upstream _growth at_stream_grad	de hdpe	15 15 10	2 good none	8 headwall_reta	ing_wall,slo 4.5 fair dry_laid_stone_b	orick satisfactory Embankment shows moder no	no crossing a driveway. satisfactory	-71.0473 42.68585
																		Small 2 inch tear on bottom left side of culvert inlet. Rubber O-			Streambed appears to be partially filled in likely by owner. Private culvert approximately 50 feet down	
B - 106i	11/6/2020 14:17 clear	ewm yes	yes	paved	satisfactory	19	55 yes ov	verhead_wires,gas_line,drainage_s	sysno	Cracking along pipe length. Manhole on side of roadway connecting culvert pipes.	round roadway_aligned stream_aligned	stagnant	no	at_stream_grad	de hdpe	15 15 9	1 satisfactory none	ring at the base of pipe has fallen 3 off and is laying on base. headwall_reta	ing_wall,slo 4 satisfactory dry_laid_stone_b	orick satisfactory Minor erosion along embar no	stream appears to be filled in by no owner. satisfactory	-71.0474 42.68602
B - 111i	11/6/2020 14:45 clear	ewm yes	Wer	payed	satisfactory	21	35 yes ov		no		round roadway_aligned skewed_>45°	flowing	ves <25%	upstream debris_trash,other at_stream_grad		36 35 10	culvert,he 0.5 satisfactory minor adwall	Minor scaling at bottom of pipe. Slight cracking on outside edge of 8 pipe. headwall_reta			Streambed shows minor erosion. Streambed has multiple fallen tree	-71.0468 42.68393
	,-,-020 27-73 UESI	yes	70.5	paveu			25,63 00		-	Minor cracking along roadway. Guard rail		ownik	,	, ocoro_sum,outer at_stream_grad	ppe	. 33 10	auwan	Minor scaling at bottom of pipe.	Uly said_stone_t	SHOWN HOUSE HO	. Idli	72.00373
B - 111o	11/6/2020 15:02 clear	ewm yes	yes	paved	satisfactory	21	35 no		yes guardrail fair	appears to have several bent posts. Guard rail shape appears to be relatively intact.	round roadway_aligned stream_aligned	flowing	no	perched	reinforced_concrete_pipe	36 36 9	0.5 satisfactory none	Where pipe Lanks meet the 9 mortar is starting to come loose. headwall_reta	ing_wall,slo 3 satisfactory dry_laid_stone_b	orick fair Right side of head wall is st no	no Fallen limbs within Stream bed. fair	-71.0466 42.68391

B - 360o	11/6/2020 15:22 clear e	wm yes yes	paved satisfactory	21	29 no	no	Minor cracking along roadway. Small crack along entire length of pipe.	round skewed_<45° no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert leaves,sediment at_stream_grade ductile_iron_pipe	8 7.5 4	0 satisfactory none	Minor surface rust on pipe. slope_flatter_than_2:1_ 1 satisfactory	Minor erosion on embankn no no	satisfactory -71.0463 42.68439
B - 360i	11/6/2020 15:39 clear e	wm yes yes	paved satisfactory	21	29 yes overhead_wires	no	Minor cracking on roadway. Crack seen along pipe length.	round skewed_<45° no_disc	scernable_stream_channel no_dry_botto	om yes_25_50%	within_culvert sediment,leaves at_stream_grade ductile_iron_pipe	8 6 6.5	0 good none	slope_flatter_than_2:1_ 1 satisfactory	Embankment is comprised no no	good -71.0463 42.68445 Headwall and pipe looked to be
B - 107o	11/6/2020 15:52 clear e	wm ves ves	paved satisfactory	23	58 yes overhead wires	no	Minor cracking along roadway.	round roadway aligned stream	n aligned no moist bo	ttom yes <25%	sediment,vegetati within culvert on growth perched corrugated metal pipe	24 21 18	0 satisfactory none	Minor surface rust on bottom of 5 pipe. headwall retaining wall, slo 3.5 fair dry laid stone brick satisfactory	Embankment has minor ereno no	relatively new, potentially less than 10 years old. satisfactory -71.0452 42.68537
		,	, , , , , , , , , , , , , , , , , , , ,							,				Minor surface rust on bottom of pipe. Inlet has a flared end		End of culvert is a flared end
B - 107i	11/6/2020 16:08 clear e	wm yes yes	paved satisfactory	23	58 yes overhead_wires	no	Minor cracking along roadway way.	round skewed_<45° no_disc	scernable_stream_channel stagnant	yes_<25%	upstream leaves,other at_stream_grade corrugated_metal_pipe	24 24 13	2 satisfactory none	section. headwall_retaining_wall_slo 3 fair dry_laid_stone_brick fair Culvert made up of 4 corrugated metal pipes each 18 inches in	Embankment has minor ereno no	section. satisfactory -71.0453 42.68548 Outlet control structure approximately 40 feet away is
							Madagas analysis along length of conducti						and made for	diameter. Span of culvert is approximately 10 feet. Moderate		Damed up likely due to beavers. Culvert is made up of 4 - 18 inch Cally a line and a lin
B - 108i	11/6/2020 16:51 clear e	wm yes yes	paved fair	14	22 no	no	Moderate cracking along length of roadway with small potholes.	round roadway_aligned stream	n_aligned no_moist_bo	ttom no	at_stream_grade corrugated_metal_pipe	18 18 11	culvert,he 0 fair minor adwall	surface rust. Moderate moss 15 growth. headwall_retaining_wall_slo 2.5 poor dry_laid_stone_brick fair	Embankment has moderatives no	CMP pipes approximately 10 feet in span. fair -71.0424 42.68536
														Culvert made up of 4 corrugated metal pipes each 18 inches in		
														diameter. Span of culvert is approximately 10 feet. Facing the		Culvert meet up of four -18 inch
														pond, the two culvert pipes on the left have rusted out bottoms		CMP pipes with a span of approximately 10 feet. Nearby
B - 108o	11/6/2020 17:07 clear e	wm yes yes	paved fair	14	22 no	no	Moderate cracking along roadway with small sized potholes.	round roadway_aligned stream	n_aligned no_moist_bo	ttom yes_<25%	within_culvert branches perched corrugated_metal_pipe	18 18 15	0 failing minor culvert	and are failing. Other two have 8 moderate surface rust. headwall_retaining_wall_slo 2 fair dry_laid_stone_brick failing	Embankment has a compo: no no	wetland flag at edge of culvert. General tree growth within stream. failing -71.0425 42.68522
B - 113	11/6/2020 17:39 clear w	mb yes no												Culvert is made up of 2 - 36 inch		Culvert located on private property71.036 42.68377
														corrugated metal pipe, approximate span is 15 feet.		
														Facing down stream the left pipe currently has water flowing		Top of left pipe is partially crushed
														through it while the right is completely dry. Minor surface		inward. Right pipe has steel mesh beaver fence in front of it. Large
B - 128i	11/6/2020 17:53 clear e	wm yes yes	paved good	22	36 no	no		round roadway_aligned stream	n_aligned flowing	yes_<25%	upstream other,leaves at_stream_grade corrugated_metal_pipe	36 31 28	4 fair none	rust throughout pipe. More in 15 general notes. headwall_retaining_wall_slo 42.5 fair dry_laid_stone_brick fair	Embankment shows minor yes no	metal grate fallen in front of left pipe. Fallen tree within stream. fair -71.0432 42.67892
														Culvert is comprised of 2 - 36 inch CMP pipes. Span between		
														pipes is 12.5 feet. Facing down stream left pipe has water		
														flowing through it, while right is dry. Minor surface rust. Dry pipe		
B - 128o		wm yes yes	paved good	22	36 yes overhead_wires	yes guardrail satisfa	actory Minor dents in guard rail.	round roadway_aligned stream	n_aligned flowing	no	at_stream_grade corrugated_metal_pipe	36 36 18	3 fair minor culvert	has an indent on top 14 approximately 5 feet in. headwall_retaining_wall_slo 4 fair dry_laid_stone_brick poor	Minor erosion present on e no no	General tree growth and limbs down within stream. fair -71.0434 42.67881 No culvert was found. -71.0312 42.67348
B - 145	11/6/2020 18:37 clear e	wm	paved											One of the headwall stones		No culvert was found71.0312 42.67348 Pipe appears to be relatively new
B - 361i	11/6/2020 18:43 clear e	wm yes yes	paved good	22	37 no	no		round skewed >45° skewed	ed <45° flowing	no	perched hdoe	15 15 12	3 poor minor culvert	punctured the top of pipe leading	Moderate erosion of emba no no	but still has large puncture a top of
																Pipe appears to be new but
																headwall stone has punctured the top. Nearby wetland flags. Fallen
	11/6/2020 18:59 clear w		paved good									15 15 5	******	Headwall wall stone has punctured the top of culvert,	Headwall has minors void tho no	sign located approximately 10 feet away from culvert, "fire department waterhole". poor -71.0302 42.67327
B = 3610	11/6/2020 18:59 Clear W	mb yes yes	paved good	22	37 yes overhead_wires,gas_line	no		round skewed_>45° stream	n_aligned flowing	no	perched hdpe	15 15 5	0.5 poor minor culvert	11 leaving cracks along the sides. headwall_retailning_wall_slo 3 satisfactory dry_laid_stone_brick fair On of the pipe is lined with an asphalt/tar mix. Minor surface	Headwaii nas minors void tho no	waterhole". poor -71.0302 42.67327
B - 141i	11/6/2020 19:25 clear e	wm yes yes	paved good	21	41 no	no		round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om no	at_stream_grade corrugated_metal_pipe	15 15 10	0 satisfactory none	rust throughout pipe. slope_flatter_than_2:1_ 2 fair	Minor erosion along embarno no	satisfactory -71.0335 42.67421 General tree growth and fallen
B - 141o	11/6/2020 19:41 clear e	wm yes yes	paved good	21	41 yes overhead_wires,gas_line	no		round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert sediment at_stream_grade corrugated_metal_pipe	15 11 13	0 fair none		Embankment shows gener; no no	limbs at outlet opening. Nearby wetland flags. fair -71.0335 42.67414
B - 147i	11/6/2020 19:54 clear e	wm yes yes	paved good	21	35 no	no		round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert leaves,sediment at_stream_grade corrugated_metal_pipe	12 11 9	0 satisfactory none	Minor surface rust at bottom of pipe. slope_steeper_than_2:1_ 3 fair	Moderate erosion of emba no no	Various branches growing in front of inlet area. satisfactory -71.0297 42.67332
B - 1470	11/6/2020 20:11 clear e	wm ves ves	paved good	21	35 ves overhead wires.gas line			round roadway aligned no disc	scernable stream channel no dry botto	·	perched corrugated metal pipe	12 12 0	0 satisfactory minor culvert	Minor surface rust at bottom of pipe. slope_steeper_than_2:1_ 3 poor	Moderate to severe erosio no no	Thick vegetation overgrowth at outlet. Wetland flags located nearby. fair -71.0297 42.6733
5-24/0	11/0/2020 20:22 Cress	wiii yes yes	parea good		33 yes Oterman_wice,gas_inc			Tourist Tourist Injuries	actinization control incomplete	AII 110	percined corrugated_metal_pipe	11 11 0	o autoricory minor correct	hber ander transfer a hone	Productive to service erosionio no	Culvert located at Pye Brook. Large beaver great in front of headwall
											leaves,sediment,o			Beaver grate placed in front of inlet. Moderate surface rust		and culvert. Headwall covered with forest debris with beaver naw marks
B - 93i	11/9/2020 13:03 clear e	wm yes yes	paved good	24	40 no	no		round skewed_<45° no_disc	scernable_stream_channel stagnant	yes_25_50%	upstream ther at_stream_grade corrugated_metal_pipe	36 36 20	2.5 fair minor culvert	Moderate surface rust	Embankment shows moderyes no	in branches. poor -71.0164 42.68994
														throughout culvert. Approximately 3 feet in there is		Beaver debris located next to outlet. At end of inspection, a man
B - 93o	11/9/2020 13:39 clear e	wm yes yes	paved good	24	40 yes overhead_wires	evidence_{guardrail	Former guard rail with only one metal post left. Post is actively overturning without any remaining guard rail sections.	round roadway_aligned stream	n aligned stagnant	yes <25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	36 36 24	4 fair minor culvert	an indent in word. At outlet opening looks like pipe is starting to deform. headwall_retaining_wall_sto 3.5 fair dry_laid_stone_brick fair	Embankment shows minor yes no	came and raked up the inlet of the culvert causing greater water flow within the culvert. fair -71.0163 42.68993
B - 103 B - 122	11/9/2020 14:16 clear w	mb culvert_was_not_located_d no mb culvert_was_not_located_d no	•		· · · · · · · · · · · · · · · · · · ·			, <u> </u>						V	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	Culvert does not exist71.0161 42.68682 Culvert did not exist71.0201 42.68163
														Second length of pipe in is		Wetland flag A1 located next to
B - 120o	11/9/2020 14:54 clear e	wm yes yes	paved satisfactory	22	80 no	no	Roadway shows minor cracking.	round skewed_<45° skewed	d_<45° no_dry_botto	om yes_<25%	within_culvert leaves,sediment at_stream_grade corrugated_metal_pipe	18 14 16	0 satisfactory none	slightly bent inward in bottom 6 left corner. Minor surface rust. headwall_retaining_wall_slo 4.5 satisfactory mortared_stone_brick fair	Minor erosion along embarno no	culvert. Multiple wetland flags seen along stream. satisfactory -71.0225 42.68192
														Second length of pipe on the bottom right side is slightly bent		Large tree is popping out stones from headwall. What one flag G-4
B - 120i	11/9/2020 15:19 clear e	wm yes yes	paved satisfactory	22	80 yes overhead_wires	no	Minor cracking along roadway.	round skewed_<45° no_disc	scernable_stream_channel no_dry_botto	om no	at_stream_grade corrugated_metal_pipe	18 18 0	0 satisfactory minor culvert	upward. headwall_retaining_wall,slo 2.5 satisfactory mortared_stone_brick fair	Embankment shows minor no no	located directly next to inlet. fair -71.0226 42.68207 Culvert was located on private
B - 121 B - 125	11/9/2020 15:49 clear e 11/9/2020 16:04 clear w	wm yes no mb culvert_was_not_located_d no														property71.0244 42.6818 Culvert did not exist71.0207 42.68032
																Wetland flag located nearby. General vegetation growth around
B - 132i	11/9/2020 16:14 clear e	wm yes yes	paved fair	22	60 yes gas_line	no	Moderate cracking in patches along roadway. Recent roadway patch aligned with gas line.	round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om no	at_stream_grade corrugated_metal_pipe	12 12 0	0 satisfactory moderate culvert	Minor surface rust at bottom of pipe. headwall_retaining_wall_slo 3 satisfactory mortared_stone_brick satisfactory	Embankment shows minor no no	inlet. Inlet significantly covered by leaf pile. satisfactory -71.0168 42.67689
							Moderate cracking in patches along roadway. Recent roadway patch aligned with gas line.							Minor surface routes throughout		Thorn bushes located around
B - 132o	11/9/2020 16:27 clear e	wm yes yes	paved fair	22	60 yes gas_line,overhead_wires,other	no	Ground-based electrical box located nearby. Minor cracking and patches seen along long	round roadway_aligned stream	n_aligned no_dry_botto	om yes_<25%	within_culvert sediment at_stream_grade corrugated_metal_pipe	12 10 9.5	0 satisfactory minor culvert	10 pipe. slope_flatter_than_2:1hea 3.5 fair mortared_stone_brick satisfactory	Embankment shows minor no no	culvert outlet. satisfactory -71.0167 42.67708
B - 131i	11/9/2020 16:39 clear e	wm yes yes	paved fair	24	64 yes gas_line	no		elliptical skewed_>45° skewed	ed_<45° no_moist_bo	ttom yes_<25%	within_culvert sediment at_stream_grade corrugated_metal_pipe	30 16 25	0 satisfactory minor culvert	Surface rust at bottom and sides 8 of pipe. slope_flatter_than_2:1_,hea 2.5 satisfactory mortared_stone_brick fair	Embankment shows minor no no	Wetland flag 1A5 located on top of headwall. Minor vegetation growth. satisfactory -71.0173 42.67696
B = 131a	11/9/2020 17:02 clear e	wm ves ves	paved fair	24	64 yes overhead wires,gas line	no	Minor cracking and patches seen along long roadway. Small sized potholes located near center of roadway.	elliptical skewed >45° stream	n aliened no dry bette	om yes <25%	within culvert leaves, sediment at stream grade corrugated metal pipe	30 17 22	0 satisfactory minor culvert	Minor surface rust throughout 7 pipe. headwall retaining wall, slo 2.5 satisfactory dry laid stone brick satisfactory	General vegetation growth no no	satisfactory -71.017 42.67703
B = 1510	11/9/2020 17:02 Clear e	wiii yes yes	paved fair	24	64 yes Overnead_wires,gas_line	110	center of roadway.	eliputai skewed_245 strealii	n_angree no_ory_bott	nii yes_<25%	within_curvert reaves, sediment at stream grade corrugated inetal_pipe	30 1/ 22	o satisfactory fillion Convert	/ pipe. Minor surface rust throughout pipe. Outlet side of pipe is	General vegetation growth no	Saustactory -71.01/ 42.07/05
B - 318i	11/9/2020 17:27 clear e	wm yes yes	paved satisfactory	28	80 yes overhead_wires	no	Minor cracking seen along roadway.	round skewed_<45° no disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert sediment,leaves inlet_drop corrugated_metal_pipe	12 11 10	0 satisfactory none	comprised of a 12 inch HDPE plan handwall retaining well sto. 2 good doc laid stone book castisfactors.	Top of headwall has a half ino no	satisfactory -71.0156 42.67525
		,	•				•		= -=					Head wall stone has settled causing top of culvert to crack		•
														along pipe width. Pipe is comprised of an inlet made of CMP and an outlet made of		Head wall stone directly above culvert pierced top of pipe causing a
B - 318o	11/9/2020 17:48 clear e	wm yes yes	paved satisfactory	28	80 yes overhead_wires,gas_line	no	Minor cracking seen along roadway.	round skewed_<45° no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert sediment,leaves at_stream_grade hdpe	12 11 9	0 poor none	Clivir and an outlet made or HDPE. headwall_retaining_wall_slo 3 satisfactory dry_laid_stone_brick fair	Headwall has voids betweeno no	curvert pierced top or pipe causing a crack along pipe width. fair -71.0152 42.67527
B - 135o	11/9/2020 18:19 clear e	wm yes yes	paved satisfactory	25	85 yes gas_line,overhead_wires	no	Minor cracking seen along roadway.	round skewed_>45° no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 11 8	0 satisfactory minor culvert	headwall_retaining_wall_slo 1.5 satisfactory dry_laid_stone_brick satisfactory	Voids between stones with no no	General tree growth. Large amount of leaves directly after outlet. satisfactory -71.0156 42.67602
														Approximately 1 foot into the		
														pipe there is a 4 inch section that is separated from the rest of the pipe. Small dents on top length of		Culvert inlet was buried with leaves
B - 135i	11/9/2020 18:40 clear e	wm yes yes	paved satisfactory	24	85 yes overhead_wires,gas_line	no	Minor cracking along roadway.	round skewed_>45° no_disc	scernable_stream_channel no_dry_botto	om yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 9 11	0 poor none	pipe. headwall_retaining_wall_slo 2 satisfactory dry_laid_stone_brick poor	General vegetation growth no no	and thorn bushes upon arrival. poor -71.0154 42.67592
B - 136o	11/9/2020 19:03 clear e	wm yes yes	paved fair	26	80 yes overhead_wires	no	Moderate cracking along center of roadway. Small side pothole in center of roadway.	round skewed_>45° no_disc	scernable_stream_channel no_dry_botto	om yes_25_50%	within_culvert sediment at_stream_grade corrugated_metal_pipe	12 8.5 11	0 satisfactory minor culvert		Embankment shows minor no no	Minor voids between stones, with sediment seeping through. satisfactory -71.0186 42.67595
														Culvert is buried under large piles of leaves and sediment,		Culvert is buried under large piles of leaves and sediment, headwall
B - 136i	11/9/2020 19:23 clear e	wm yes yes	paved fair	26	80 yes gas_line,overhead_wires	no	Moderate cracking along center of roadway. Small side pothole in center of roadway.	round skewed_>45° no_disc	scernable_stream_channel	completely_obstr	upstream,within_leaves,sediment,b ructeculvert ranches corrugated_metal_pipe	12		headwall was found but culvert is not observable. headwall_retaining_wall_slope_flatter_th:fair dry_laid_stone_brick	Embankment shows minor no no	was found but culvert is not observable. failing -71.0185 42.67571
B - 139	11/9/2020 19:38 clear w	mb culvert_was_not_located_d no														Culvert was on private property/does not exist71.022 42.67425 Inlet of culvert is the out fall of
B - 149i	11/9/2020 19:48 clear e	wm yes yes	paved satisfactory	22	40 no	no	Minor cracking along roadway.	round skewed_<45° no_disc	scernable_stream_channel no_dry_botto	on no	at_stream_grade corrugated_metal_pipe	30 30 0	0 satisfactory minor culvert	Minor surface rust along time. headwall_retaining_wall_slo 1.5 poor dry_laid_stone_brick poor	Moderate erosion along enyes no	Kimball's pond. fair -71.024 42.67213
														War for the state of the state		Large Beaverdam located approximately 10 feet away from
B - 149o	11/9/2020 20:08 clear e	wm yes yes	paved satisfactory	22	40 yes overhead_wires	no	Minor cracking along roadway.	round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om no	perched corrugated_metal_pipe	30 30 0	0 satisfactory none	Minor surface rust throughout pipe. headwall_retaining_wall_slo 3 poor dry_laid_stone_brick fair Minor scraling at hottom and	Moderate to severe erosioryes no	outlet. Large 5 foot concrete slab located directly after Beaverdam. fair -71.024 42.67198
B - 112i	11/10/2020 13:17 clear e	wm yes yes	paved satisfactory	22	75 no	no	Minor cracking along road.	round roadway aligned stream	n aligned flowing	yes_<25%	within_culvert,up other,sediment,le stream aves at_stream_grade reinforced_concrete_pipe	30 30 17	2.5 satisfactory minor culvert	Minor scaling at bottom and sides of pipe. Minor moss inside sides of pipe. Minor moss inside 8 and outside of pipe. headwall_retaining_wall_slo 4 satisfactory dry_laid_stone_brick satisfactory	Minor erosion along embarno no	satisfactory -71.0108 42.68374
=		, yes		•		-	00			,		== #	- Luncis	Minor scaling on bottom and sides of pipe. Second and third		
														length of pipe have separated with sediment seeping through.		
B - 112o	11/10/2020 13:34 clear e	wm yes yes	paved satisfactory	22	75 no	no	Minor erosion along roadway.	round roadway_aligned stream	n_aligned flowing	yes_50_75%	sediment,leaves,o within_culvert ther at_stream_grade reinforced_concrete_pipe	30 14 29	culvert,he 2 poor minor adwall	Small crack present on outside of	Embankment has minor errno no	Outlet leads into Four Mile Pond. fair -71.0107 42.68397
B - 127o	11/10/2020 14:04 clear e	wm yes yes	paved satisfactory	22	46 yes other, gas	no	Minor cracking along roadway way. Electric box seen just down roadway.	round roadway slice-d ***	scernable stream channel no dry botto	nm upe 60 750	within culvert sediment at stream grade reinforced concrete pipe	12 7 12	0 satisfactory none	Amount of sediment increases throughout pipe length. headwall retaining wall, slo 3.5 satisfactory dry laid stone brick satisfactory	Possible stone missing on r no no	Amount of sediment within pipe increases with pipe length. satisfactory -71.0071 42.67885
B = 12/0	11/10/2020 14:04 Cear e	····· yes yes	portol satisfactory	44	-o yes outer, gas	110	scen just down roadway.	rouse rouseay_aligned no_disc	accommunication in an analysis of the control of th	лн yes_5U_/5%	at_stream_grade reinforced_concrete_pipe	12 / 12	Satisfactory mone	тоорумм рорь тепдит. певичин_техниник_wam,au 3.5 satisfactory ory_lasto_stone_onck satisfactory	- ozame stutie ilissilig on r no no	increases with pipe length. satisfactory -71.0071 42.67885 Headwall and culvert are buried
																under sediment, rocks, leaves, and vegetation growth. Location and
				_			Minor cracking seen along roadway. Electric bo				sediment,vegetati within_culvert,up on_growth,leaves,					length of culvert is approximate. Headwall and culvert are not totally
B - 127i	11/10/2020 14:20 clear e	wm yes no	paved satisfactory	22	46 yes other, gas	no	seen down roadway.	round roadway_aligned no_disc	scernable_stream_channel	completely_obstr	ructestream other reinforced_concrete_pipe	12		headwall_retaining_wall_slope_flatter_th:satisfactory dry_laid_stone_brick	Top of head wall is visible t no no	visible leaving them uninspectable. failing -71.0069 42.67894 24 inch diameter tree
							Minor cracking along roadway. Electric boxes							Second length of pipe has shifted backwards approximately 4		24 inch diameter tree approximately 3 feet away from the inlet. General vegetation growth
B - 133i	11/10/2020 14:37 clear e	wm yes yes	paved satisfactory	24	45 yes other	no		round roadway_aligned no_disc	scernable_stream_channel no_dry_botto	om no	inlet_drop reinforced_concrete_pipe	18 18 0	0 fair none		Embankment shows minor no no	

Minor cracking within 1st foot of pipe. Moss growth on outside of

															pipe. Moss growth on outside of pipe.			
									Minor cracking along roadway. Electric boxes seen down both sides of street. Guard rail					culvert,he				Wetland flag seen directly outside of outlet. General tree growth and
B - 133o	11/10/2020 14:51 clear	ewm yes	yes	paved	atisfactory	24	45 yes other	yes guardrail fair	shows minor signs of beginning to rot at top. roun	d roadway_aligned no_discernable_stream_channel	l no_dry_bottom no	perched reinforced_concrete_pipe	18 18 0	0 satisfactory moderate adwall	headwall_retaining_wall,slo Moderate scaling throughout	5 fair dry_laid_stone_brick fair	Embankment shows minor no no	fallen limbs at outlet. fair -71.0026 42.67692
															pipe. Second length of pipe shifted back approximately 6			
B - 134i	11/10/2020 15:12 clear	ewm yes	yes	paved	satisfactory	24	45 yes other	no	Minor cracking along roadway. roun	d roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_50%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	18 11 17	0 fair minor culvert	inches. headwall_retaining_wall,slo	3.5 satisfactory dry_laid_stone_brick fair	Embankment shows minor no no	fair -71.0006 42.67642
															Moderate scaling throughout pipe. Second length of pipe is			
															separated from outlet and			
									Minor cracking along roadway. Nearby electric						approximately 3 inches. Large pile of sediment has come			Large pile of sediment where pipes have separated within culvert.
B - 134o	11/10/2020 15:22 clear	ewm yes	yes	paved	atisfactory	24	45 yes other	no	boxes. roun	d roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_50%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	18 9 17	0 poor none	through 3 inch separation. headwall_retaining_wall,slo	4 fair dry_laid_stone_brick poor	Embankment has minor ereno no	Outlet outfalls into detention pond. poor -71.0008 42.67627
															Second length of pipe has separated approximately 3			
									Minor cracking along roadway. Electrical box			leaves,other,sedi			inches. Large build up of sediment in the middle of pipe			Large buildup of sediment within
B - 123i	11/10/2020 15:45 clear	ewm yes	yes	paved	satisfactory	20	90 yes other,water_line	no	sent down roadway. Hydrant located across street. roun	d roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_<25%		21 16 18	0 fair none	likely from separation of pipes. headwall_retaining_wall,slo	4 satisfactory dry_laid_stone_brick satisfactory	Embankment shows minor no no	pipe, possibly from pipe separation. fair -71.0012 42.68115
															Second length of pipe separated			
									Minor cracking along roadway. Electrical box						approximately 2 inches. Small amount of sediment seeping			Pipe changes direction at halfway
B - 123o	11/10/2020 16:00 clear	ewm yes	ves	paved	atisfactory	20	90 yes water_line,other	no	sent down roadway. Hydrant located on same side of street. roun	d skewed_<45° no_discernable_stream_channel	l no dry bottom no	perched reinforced_concrete_pipe	21 21 0	0 satisfactory minor culvert	through. Minor moss growth on outside of pipe. slope_flatter_than_2:1_	2 satisfactory	Minor erosion and general no no	point approximately just before roadway. satisfactory -71.0015 42.68095
B - 124i	11/10/2020 16:16 clear	ewm ves	ves	naved	satisfactory	21	75 yes other	no	Minor cracking along roadway. Electrical box	d skewed <45° no discernable stream channel		sediment,leaves,b	18 11 17	0 satisfactory none	headwall retaining wall,slo	4.5 satisfactory dry laid stone brick satisfactory	Along embankment there i no no	satisfactory -70.9988 42.68079
		,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,			,				,					,		, , , , , , , , , , , , , , , , , , , ,		Inside of outlet there are two large
															to the second se			stones partially impeding flow. If
B - 124o	11/10/2020 16:30 clear	ewm yes	yes	paved	satisfactory	21	75 yes other	no	Minor cracking along roadway. Electrical box sent down roadway. roun	d skewed_<45° no_discernable_stream_channel	no_dry_bottom yes_75_100%	within_culvert other at_stream_grade reinforced_concrete_pipe	18 6 15	0 satisfactory none	Large stones at outlet opening, impeding flow. headwall_retaining_wall,slo	5.5 fair dry_laid_stone_brick fair	Embankment has minor ve no no	stones are removed, rating of culvert would go to satisfactory. fair -70.9991 42.68063
															Stones lining culvert			
															approximately 2 to 3 feet from bottom have collapsed in to			Moderate erosion of streambed.
									Moderate surface rust along guard rail. Minor cracking and patches along roadway. Several					culvert,he	natural streambed. Top of culvert is comprised of a concrete slab,			Essex County bound located approximately 8 feet away from
B - 129o	11/10/2020 17:03 clear	ewm yes	yes	paved	atisfactory	25	55 no	yes guardrail poor	post of guard rail are actively overturning. open	_bottrskewed_<45° stream_aligned	stagnant yes_25_50%	within_culvert other at_stream_grade mortared_stone	54 48 54	7 failing moderate adwall	7 while walls are mortar and stone. headwall_retaining_wall,slo	6.5 poor mortared_stone_brick failing	Embankment shows moderno no	outlet along streambed. failing -70.9981 42.67872
															Bottom 2 feet of stones are missing mortar. Top of culvert			
B - 129i	11/10/2020 17:28 clear				anti-factor.	25	55 yes overhead wires	avandanii assisti	Minor bends throughout guard rail. Minor	hattalanad eff?	0i	inlet drop mortared stone		5 fair minor wingwall	and wing walls are comprised of 12 cast in place concrete. headwall retaining wall, slo	7.5 fair concrete poured fair	Embankment has minor to no no	Fallen trees and limbs within stream bed. fair -70.9981 42.67894
B - 1291	11/10/2020 17:28 clear	ewm	yes	paved	atistactory	25	55 yes overnead_wires	yes guardraii satista	actory cracking and patches along roadway. open	_bott:skewed_<45° stream_aligned	nowing no	inlet_drop mortared_stone	54 46 54	5 tair minor wingwaii	12 Cast in place concrete. neadwall_retaining_wall,slo	7.5 fair concrete_poured_ fair	Embankment has minor to no no	Inlet of pipe was found but part of
																		closed drainage system. Drainage system likely outlets behind
B - 137	11/10/2020 18:06 clear	wnb yes	no												Minor scaling a bottom pipe.			abutting property70.9963 42.67583
									Guard rail appears to be dried wood and slightly					headwall,c	Top of inlet opening has small amount of rebar showing			
B - 148i	11/10/2020 18:16 clear	ewm yes	yes	paved	good	21	40 yes other	yes guardrail satisfa	actory cracking. Electric boxes seen along roadway. roun	d roadway_aligned stream_aligned	no_dry_bottom no	at_stream_grade reinforced_concrete_pipe	42 42 0	0 good minor ulvert	7 through. headwall_retaining_wall,slo	4 satisfactory mortared_stone_brick satisfactory	Sporadic separation of mor no no	satisfactory -70.9907 42.67301
																		Culvert outlets into small pond.
									Guard rail appears to be dried wood and slightly						Mineralianishin			Starting at each side of wing walls in going around pond there is a small
B - 148o	11/10/2020 18:30 clear	ewm yes	yes	paved	good	21	40 yes other,water_line	yes guardrail satisfa	cracking. Electric boxes seen along roadway. Fire actory hydrant seen along roadway. roun	d roadway_aligned no_discernable_stream_channel	l no_dry_bottom no	at_stream_grade	42 42 0	headwall,c 0 satisfactory moderate ulvert	Minor scaling within pipe. Minor moss growth. headwall_retaining_wall_slo	3.5 satisfactory mortared_stone_brick satisfactory	Small sized sinkhole on left no no	wire fence. Minor vegetation growth at outlet. satisfactory -70.9905 42.67306
									Guard rail shows minor cracking. Roadways						Pipe inlet is a flared end section. On left side rebar is showing			
B - 152i	11/10/2020 18:54 clear	ajs yes	yes	paved	fair	21	80 no	yes guardrail satisfa	actor, shows minor to moderate cracking. roun	d skewed_>45° stream_aligned	no_dry_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	12 12 24	0 satisfactory none	5 through. slope_steeper_than_2:1_ Pipe outlet is a flared end	4 fair	Minor erosion along embar no no	Pipe inlet is a flared end section. satisfactory -70.9892 42.66949
															section. Moderate scaling throughout pipe. On top and left			General tree growth at outlet
	**************					24	80 no		Guard rail shows minor cracking. Roadways	d skewed >45° no discernable stream channel		within culvert sediment,leaves at stream grade reinforced concrete pipe		0 fair none	side of culvert rebar is showing	4.60		opening. Fallen tree limb at outlet
B - 152o	11/10/2020 19:06 clear	ewm yes	yes	paved	ar	21	8U NO	yes guardraii satista	· ·	a skewed_>45 no_discernable_stream_channel	no_ary_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	12 12 22	U tair none	through. slope_steeper_than_2:1_	4 fair	Minor erosion along embar no no	opening. fair -70.9894 42.66936
									Guard rail missing wooden rail with multiple concrete posts falling over. Roadway shows						Minor scaling forming at bottom of pipe. Two small chips taken			Moderate vegetation overgrowth
B - 143i	11/10/2020 19:29 clear	ewm yes	yes	paved	satisfactory	23	42 yes overhead_wires	yes guardrail failing	g minor cracking and patches. roun	d skewed_<45° skewed_>45°	no_dry_bottom yes_<25%	within_culvert sediment at_stream_grade reinforced_concrete_pipe	30 24 25	0 satisfactory none	9 out of the top of inlet. headwall_retaining_wall,slo	5.5 poor mortared_stone_brick satisfactory	Moderate to severe erosion o no	at inlet opening. satisfactory -71.0011 42.67326
									Guard rail missing wooden rails with multiple concrete posts falling over. Roadway shows			sediment, branche downstream, with s, vegetation_grow			Minor scaling starting to form. Black tar/asphalt mix ring along			
B - 143o	11/10/2020 19:41 clear	ewm yes	yes	paved	satisfactory	23	42 no	yes guardrail failing		d skewed_<45° skewed_>45°	no_dry_bottom yes_<25%	in_culvert th at_stream_grade reinforced_concrete_pipe	30 25 23	0 satisfactory none	6 outside of culvert. headwall_retaining_wall,slo	4.5 poor mortared_stone_brick fair	Embankment has moderati no no	Vegetation overgrowth at outlet. fair -71.0013 42.67343
	11/10/2020 20:08 clear	ewm ves					34 no			d roadway aligned no discernable stream channel				0 fair none	Moderate surface rust	Author to the contract		Small tree located approximately 3 feet away from inlet. satisfactory -70.9932 42.66325
B = 1681	11/10/2020 20:08 Clear	ewm yes	yes	paved	good	21	34 110	no	Roadway appears recently paved. roun	roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 8 11	U tair none	throughout. headwall_retaining_wall_slo	2 satisfactory dry_laid_stone_brick satisfactory	General vegetation growth no no	
												downstream, with sediment, leaves, b			Minor surface rust throughout			General tree growth directly next to headwall. Fallen limbs and branches
B - 168o	11/10/2020 20:19 clear	ewm yes	yes	paved	good	21	34 yes overhead_wires	no	Roadway appears recently paved. roun	d roadway_aligned stream_aligned	no_dry_bottom yes_25_50%	in_culvert ranches at_stream_grade corrugated_metal_pipe	12 7.5 12	0 satisfactory none	4 pipe. headwall_retaining_wall,slo	2 fair dry_laid_stone_brick fair	Embankment has minor to no no	on embankment and streambed. fair -70.9931 42.66318
									Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each						Minor scaling at bottom of pipe.			
B - 155i	11/11/2020 13:14 clear	ewm yes	yes	paved	iatisfactory	21	45 yes other,drainage system	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of	d skewed >45° no discernable stream channel	el no dry bottom yes <25%	within culvert sediment,other at stream grade reinforced concrete pipe	24 23 13	culvert,he 0 satisfactory minor adwall	Small surface cracks at top of	1 dry laid stone brick satisfactory	No embankment present, i no no	General tree growth. satisfactory -71.0133 42.66809
B - 155i	11/11/2020 13:14 clear	ewm yes	yes	paved	satisfactory	21	45 yes other,drainage_system	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of	d skewed_>45° no_discernable_stream_channel	el no_dry_bottom yes_<25%	within_culvert sediment,other at_stream_grade reinforced_concrete_pipe	24 23 13	culvert,he 0 satisfactory minor adwall	Small surface cracks at top of inlet. headwall_retaining_wall	1 dry_laid_stone_brick satisfactory	No embankment present, i no no	General tree growth. satisfactory -71.0133 42.66809
B - 155i	11/11/2020 13:14 clear	ewm yes	yes	paved	satisfactory	21	45 yes other,drainage_system	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of actory roadway part of retaining wall. roun Minor cracks along roadway. Electrical boxes	d skewed_>45° no_discernable_stream_channel	el no_dry_bottom yes_<25%		24 23 13	culvert,he 0 satisfactory minor adwall	Small surface cracks at top of inlet. Minor scaling at bottom of pipe. Obstruction located with in flared	1 dry_laid_stone_brick satisfactory	No embankment present, t no no	
			yes	paved	satisfactory	21			seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of actor, roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of roadway.			sediment,leaves,v egetation growth,			Small surface cracks at top of inlet. headwall_retaining_wall headwall_retaining_wall hinds retaining_to the control of pipe. Obstruction located with in flared end section, but not within pipe. Minor chips and concrete missing			1 inch thick iron rod located approximately 8 feet away from
B-1550 B-1550 B-167	11/11/2020 13:14 clear 11/11/2020 13:36 clear 11/11/2020 13:59 clear	ewm yes ewm yes ewm culvert_was_not_loca	yes yes sted_d no	paved paved	iatisfactory	21	45 yes other,drainage_system 45 yes other,drainage_system		seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of actor, roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of roadway.	d skewed_v45* no_discernable_stream_channel d skewed_v45* no_discernable_stream_channel		sediment_leaves,v		Culvert,be 0 satisfactory minor adwall 0 satisfactory none	Small surface cracks at top of inlet. headwall_retaining_wall Minor scaling at bottom of pipe. Obstruction located with in flared end section, but not within pipe.	1 dry_laid_stone_brick satisfactory 2 dry_laid_stone_brick fair	No embankment present, I no no Top of headwall/retaining vno no	1 inch thick iron rod located
8 - 155o	11/11/2020 13:36 clear	ewm yes	yes yes tted_d no	paved paved	atisfactory	21			seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of actor, roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foor rock wall on each side of roadway.			sediment,leaves,v egetation growth,			Small surface cracks at top of inlet. Minor scaling at bottom of pipe. Obstruction located with in flared end section, but on their pipe. Minor chips and concrete missing within flared end section, when headwall_retaining_wall in the control of			1 inch thick iron rod located approximately 8 feet away from outlet. satisfactory -71.0134 42.6681
8 - 155o	11/11/2020 13:36 clear	ewm yes	yes yes sted_d no	paved paved	antisfactory autisfactory	21 21 22	45 yes other,drainage_system		seen along street. Nearby cotch basins on each side of readway. 2 floot rock wall on each side of readway. Electrical some station, readway part of retaining wall. Minor cracks along readway. Electrical boxes seen along street. Nearby cotch basins on each side of readway. Electrical boxes seen along street. Nearby cotch basins on each side of readway. Four cotch wall on each side of readway part of retaining wall.	d skewed_>45° no_discernable_stream_channel	el no_dry_bottom yes_25_50%	sediment_lewes_v egratation_growth, downstream branches at_stream_grade reinforced_concrete_pipe	24 24 48	0 satisfactory none	Small surface cracks at top of indet. Miles or scalings, bottom of jejec. Observation school with the fued end section, but not within jejec. Miles or description of the fued end section, but not within jejec. Miles or hips and concrete missing within flared end section. headwall_retaining_wall Miles or hipping on outside of			1 inch thick iron rod located approximately 8 feet away from outlet. Sales of the s
8 - 155o	11/11/2020 13:36 clear 11/11/2020 13:59 clear	ewm yes	yes yes sted_d no yes	paved paved	Latisfactory	21 21 22			seen along street. Nearby cotch basins on each side of readway. 2 floot rock wall on each side of readway. Electrical some station, readway part of retaining wall. Minor cracks along readway. Electrical boxes seen along street. Nearby cotch basins on each side of readway. Electrical boxes seen along street. Nearby cotch basins on each side of readway. Four cotch wall on each side of readway part of retaining wall.		el no_dry_bottom yes_25_50%	sediment,leaves,v egetation growth,			Small surface cracks at top of inlet. Minor scaling at bottom of pipe. Obstruction located with in flared of the scaling at bottom of pipe. Minor chips and concrete missing within flared end section. Minor chipping on outside of siete. slope_flatter_than_2:1_		Top of headwall/retaining \(\text{no}\) no	1 inch thick iron rod located approximately a feet easy from processing the control of the contr
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8 - 155o	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear	ewm yes ewm culvert_was_not_loca ewm yes	yes yes sted_d no yes	paved	natisfactory satisfactory	21 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of stations, 2 foot rock wall on each side of roun. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of roadway. 2 foot rock wall on each side of roadway and of retaining wall. General cracking along roadway. roun	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment/leaves.v agratation_growth, downstream branches at_tream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves.v egotation_growth,	24 24 48	0 satisfactory none	Small surface cracks at top of inet. Minor scaling at bottom of pipe. Obstruction becated with in face and continue to the order of th	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining uno no no Minor erosion along embar no no	1 inch thick iron rod located approximately 8 feet away from control does not exist. Culvert does not exist. Culvert fiels is part of an inlet control structure surrounded by stones and a corrugated metal pipe. satisfactory Small tree growing at base of filared
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B - 1556 B - 167 B - 1681 B - 1686	11/11/2020 13:36 clear 11/11/2020 13:39 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear	ewm yes ewm yes ewm yes	yes yes	paved	natisfactory astisfactory astisfactory	21 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of stations, 2 foot rock wall on each side of roun. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of roadway. 2 foot rock wall on each side of roadway and of retaining wall. General cracking along roadway. roun	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment/leaves.v agratation_growth, downstream branches at_tream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves.v egotation_growth,	24 24 48	0 satisfactory none	Small surface cracks at top of inlet. Misor or saling, a bottom of pipe. Obstruction located with in flared end section, but not within pipe. Minor chips and concrete missing within flared end section. Minor chipping on outside of inlet. Jope flatter_than_2:1_ Outlet is a flared end section. On the right side of flared end section there is rebar showing through. Minor chips and flared end section opening. Jope flatter_than_2:1_ Jope flatter_than_2:1_ Jope flatter_than_2:1_	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining uno no no Minor erosion along embar no no	I into thick ron rod located approximately 8 feet away from costlet. Cultert does not exist. Cultert ride it, part of an lote is controlled and a corrugated metal pipe. satisfactory Small tree growing at base of flared end section. Small tree growing at base of flared end section.
B - 1556 B - 167 B - 1681 B - 1686	11/11/2020 13:36 clear 11/11/2020 13:39 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear	ewm yes ewm yes ewm yes	yes yes	paved	autisfactory lautisfactory lautisfactory lautisfactory	21 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of stations, 2 foot rock wall on each side of roun. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of roadway. 2 foot rock wall on each side of roadway and of retaining wall. General cracking along roadway. roun	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment/leaves.v agratation_growth, downstream branches at_tream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves.v egotation_growth,	24 24 48	0 satisfactory none	Small surface cracks at top of inet. Minor scaling at bottom of pipe. Obstruction becated with in face and continue to the order of th	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining uno no no Minor erosion along embar no no	I into thick ron rod located approximately 8 feet away from costlet. Cultert does not exist. Cultert ride it, part of an lote control state around the control strates amounted by stones and a corrugated metal pipe. satisfactory Small tree growing at base of flared end section.
B - 1556 B - 167 B - 1681 B - 1686	11/11/2020 13:36 clear 11/11/2020 13:39 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear	ewm yes ewm yes ewm yes	yes yes	paved	lastisfactory Lastisfactory Lastisfactory Lastisfactory	21 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of stations, 2 foot rock wall on each side of roun. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of roadway. 2 foot rock wall on each side of roadway and of retaining wall. General cracking along roadway. roun	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment/leaves.v agratation_growth, downstream branches at_tream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves.v egotation_growth,	24 24 48	0 satisfactory none	Small surface cracks at top of inlet. Minor casing at bottom of pipe. Obstanction boards with in flared end section, but not within pipe. Minor chippa and concrete missing within flared end section. The seadwall_retaining_wall within flared end section. Minor chippaing on outside of inlet. Jope_flatter_than_2:1_ Outsite in a flared end section. On the right side of flared end section there is even showing through, Minor chips and flared end section there is even showing through, Minor chips and flared end section proming. Jope_flatter_than_2:1_ Li obstructed approximately 50% while the red are less than 20%. 1.6. Latege (hips missing from	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining uno no no Minor erosion along embar no no	1 inch thick iron rod located approximately 8 feet away from outlet. Culvert does not exist. Culvert linet is part of an inlet control structure surrounded by stones and a corrugated metal pipe. satisfactory Small tree growing at base of flared and section. Small tree growing at base of flared and section. Satisfactory 71,0169 42,66357 Culvert Gover not exist.
B - 1556 B - 167 B - 1681 B - 1686	11/11/2020 13:36 clear 11/11/2020 13:39 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear	ewm yes ewm yes ewm yes	yes yes	paved	natisfactory lastisfactory lastisfactory satisfactory	21 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of stations, 2 foot rock wall on each side of roun. Minor cracks along roadway. Electrical boxes seen along street. Nearby catch basins on each side of roadway. 2 foot rock wall on each side of roadway. 2 foot rock wall on each side of roadway and of retaining wall. General cracking along roadway. roun	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment,leaves, v egratation growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment,leaves, v egratation_growth, downstream other at_stream_grade reinforced_concrete_pipe	24 24 48	0 satisfactory none	Small surface cracks at top of inlet. Minor casing a, bottom of pipe. Obstruction located with in flared end section, but not within pipe. Minor chippa and concrete missing within flared end section. Minor chippain do concrete missing within flared end section. Minor chippain go noutside of inlet. Jope_flatter_than_2:1_ Outlet is a flared end section. On the night side of flared end section there is crear showing through. Minor chips and flared end section there is crear showing through. Minor chips and flared end section there is crear showing through. Minor chips and flared end section there is crear showing through Minor chips and flared end section there is crear showing through the control of the control	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining uno no no Minor erosion along embar no no	1 inch thick iron rod located approximately 8 feet away from outlet. Cuhert does not exist. Small tree growing at base of filared end section. Cuhert does not exist. Cuhert does not exist. Cuhert comprised of four identical 8CP pipes. Pipes are numbered one
B - 1556 B - 167 B - 1681 B - 1686	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear	ewm yes ewm yes ewm yes	yes yes	paved	aatisfactory	21 21 22 22 22	45 yes other,drainage_system 60 yes drainage_system,other	yes other satisfa	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of containing wall on each side of roadway. Zo foot rock wall on each side of roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway and the roadway of the roadway and the roadway of the roadway. General cracking along roadway. roun Minor cracking along roadway. Electrical box is actory located just down the street.	d skewed_>45° no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50%	sediment/leaves.v agratation_growth, downstream branches at_tream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves.v egotation_growth,	24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none	Small surface cracks at top finder. Millor cracking, at bottom of joine. Millor cracking, at bottom of joine. Obstruction because with in fland end of section, but not within pipe. Millor chipping on outside of inlet. Josephane of the section. On the night sake of fland end section. On the night sake of fland end section there is rebus showing dender of section opening. Lo districted approximately 50%, while the real se is less than 25%. 1.84. Large chips missing from culvert persons.	2 dry_laid_stone_brick fair 2 satisfactory	Top of headwall/retaining sno no Minor erosion along embarno no Minor erosion along embarno no	1 in thick iron rad located approximately 8 feet away from costlet. Culvert does not exist. Culvert inlet is part of an inlet control structure surrounded by stones and a corregated metal pipe. satisfactory Small tree growing at base of flared end section. Culvert does not exist. Culvert does not exist. Culvert does not exist. Culvert comprised of four identical
B - 1550 B - 167 B - 1661 B - 1660 B - 1664	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes	yes yes sted_d no	paved	aatisfactory	21 22 22 22	45 yes other,drainage_system 60 yes drainage_system.other 60 yes other	yes other satisfi no yes guardrali satisfi	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of containing wall on each side of roadway. Zo foot rock wall on each side of roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway and the roadway of the roadway and the roadway of the roadway. General cracking along roadway. roun Minor cracking along roadway. Electrical box is actory located just down the street.	d skewed_>45" no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25%	sediment, leaves, v egetation growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches, sedimene	24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none	Small surface cracks at top of iniet. Millor or scalings a bottom of piges. Observation because the bedwall _retaining_wall Millor or scalings a bottom of piges. Observation because with the fixed end section, but not within piges. Millor or bipping on outside of iniet. Millor chipping on outside of iniet. Jose _flatter_than_2:1_ Outset is a flarred end section. On the right side of flatter and control of the piges	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory	Top of headwall/retaining sno no Minor erosion along embarno no Minor erosion along embarno no	1 in thick iron rad located approximately 8 feet away from contel. Culvert does not exist. Culvert linet is part of an inlet control structure surrounded by stones and a corrupated metal pipe. satisfactory Small tree growing at base of flured end section. Small tree growing at base of flured end section. Culvert does not exist. Culvert does not exist. Culvert comprised of four directical 8/C pipes. Pipes are numbered one through 6 our thore being closest.
B - 1550 B - 167 B - 1661 B - 1660 B - 1664	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes	yes yes sted_d no	paved	aatisfactory	21 22 22 22	45 yes other,drainage_system 60 yes drainage_system.other 60 yes other	yes other satisfi no yes guardrali satisfi	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of containing wall on each side of roadway. Zo foot rock wall on each side of roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway and the roadway of the roadway and the roadway of the roadway. General cracking along roadway. roun Minor cracking along roadway. Electrical box is actory located just down the street.	d skewed_>45" no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25%	sediment, leaves, v egetation growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches, sedimene	24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none	Small surface cracks at top of inlet. Minor scaling at bottom of pipe. Obstruction boards with in flared end section, but not within pipe. Minor chipping concerted missing within flared end section, but not within pipe. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Joseph flatter_than_2:1_ Outlet is a flared end section. On the right side of flared end section then to rebut showing the section there is other showing the section specime. 1. obstructed approximately 50% while the rest are less than 25%. 1. & Lat are pith pin sizing from culver opening. 2: concretes bottom right 2: invinor chips at 11 cepturing. All: Moderate scaling. 2. medicates amount of rober showing through 3. 1: lach chip in soncrete on left side. 4: exposed	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory	Top of headwall/retaining sno no Minor erosion along embarno no Minor erosion along embarno no	i inith thick iron rad located approximately 8 feet away from contect. Culvert does not exist. Culvert linet is part of an inlet control structure surrounded by stones and a corrupated metal pipe. satisfactory -71.0147 42.66317 Smill tree growing at base of flured and exclusive structure structure structure structure structure structure surrounded by stones and a corrupated metal pipe. satisfactory -71.0169 42.66357 Culvert does not exist71.0169 42.66434 Culvert comprised of four identical RCP pipes. Pipes are numbered one through 6 our whome belong dozest to Main Street. Span is 10 feet. satisfactory -71.0163 42.66443
B - 1550 B - 167 B - 1661 B - 1660 B - 1664	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes	yes yes sted_d no	paved	aatisfactory	21 21 22 22 22	45 yes other,drainage_system 60 yes drainage_system.other 60 yes other	yes other satisfi no yes guardrali satisfi	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of contains, 2 foot rock wall on each side of roadway part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway. Z foot rock wall on each side of roadway and the roadway of the roadway of the roadway. General cracking along roadway. Minor cracking along roadway. Electrical box is actory located just down the street.	d skewed_>45" no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25%	sediment, leaves, v egetation growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe sediment, leaves, v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches, sedimene	24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none	Small surface cracks at top of inlet. Minor cracing, a hottorn of pipe. Obstruction located with in flared end section, but not within pipe. Minor chips and concrete missing within flared end section. Minor chipping on outside of inlet. Idea of the concrete missing within flared end section. Minor chipping on outside of inlet. Jope flatter_than_2:1_ Outlet is a flared end section. Outlet is a flared end section. The night side of flared end section there is ever a flower of the concrete missing through. Minor chips and flared end section there is ever showing through minor chips and flared end section period. 1: obstructed approximately 50% while the rest are less than 25%. 1 &4 Large chips measing from separating from culvert at bettom right. 3: minor chips at 1 (penning, All Moderate scaling. 2: moderate amount of rebar showing through 2: 1 therchip in	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory	Top of headwall/retaining sno no Minor erosion along embarno no Minor erosion along embarno no	1 in thick iron rad located approximately 8 feet away from contel. Culvert does not exist. Culvert linet is part of an inlet control structure surrounded by stones and a corrupated metal pipe. satisfactory Small tree growing at base of flured end section. Small tree growing at base of flured end section. Culvert does not exist. Culvert does not exist. Culvert comprised of four directical 8/C pipes. Pipes are numbered one through 6 our thore being closest.
B - 1556 B - 167 B - 1666 B - 1666 B - 164	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:43 clear 11/11/2020 15:02 clear 11/11/2020 15:09 clear	ewm yes ewm cuhert_wes_root_loca ewm yes ewm yes wmb cuhert_wes_root_loca ewm yes	yes yes sted_d no	paved paved	actisfactory actisfactory actisfactory	21 21 22 22 22	45 yes other,drainage_system 60 yes drainage_system,other 60 yes other 47 yes other	yes other satisfa no yes guardrall satisfa	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of containing wall on each side of roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway. Z foot rock wall on each side of roadway and of roadway. To control of the containing wall. General cracking along roadway. Electrical box is room. Minor cracking along roadway. Electrical box is room. Minor cracking along roadway.	d skewed_>45" no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned	of no_dry_bottom yes_25_50% no_dry_bottom no no_dry_bottom yes_<25% no_dry_bottom yes_<25%	sediment_leaves_v egetation_growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment_leaves_v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches_sedimen cubert Useaves at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none 0 satisfactory none	Small surface cracks at top of iniet. Millor or scalings a bottom of pige. Obstaction boxed with in fasted end section, but not within pige. Millor or chipping on outside of iniet. Minor chipping on outside of iniet. Jope, flatter_than_2:1, Outset is a flared end section. On the right side of flated end section with the pige. Outset is a flared end section. On the right side of flated end section opening through. Millor chips and flared end section opening. 1: obstructed approximately 50% while the rest era ex less than 25%. 1 & Lat_large chips missing from culvert a pening. Alt. Moderate scaling at 10 pening. Alt. Moderate scaling at 10 pening. Alt. Moderate scaling at 10 pening with pige. 3: 1 inch chips in concrete in exist eta. expected with the concrete context flate. Expected in the concrete context scaling at the concrete context at context at the concrete context at t	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory 4.5 satisfactory dry_laid_stone_brick satisfactory	Top of headwall/retaining two no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general v no no	I into thick iron rod located approximately 8 feet away from costlet. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert does not exist. Small tree growing at base of flared end section. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert comprised of four identical RCP pper. Pipers are numbered one through flow which me being closest to Main Street. Span is Officer. Culhert comprised of four identical RCP ppics. Pipers are numbered one through flow with one being closest.
B - 1556 B - 167 B - 1664 B - 164 B - 1631 B - 1631	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear 11/11/2020 15:09 clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes ewm yes ewm yes	yes yes sted_d no	paved paved paved	aatisfactory	21 22 22 22 20 20	45 yes other, drainage_system 60 yes drainage_system, other 60 yes other 47 yes other	yes other satisfied to the satisfied to	seen along street. Nearby cutch basins on each side of roadway. 2 floot rock wall not each aide of catchy cabows part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. 2 floot rock wall on each side of roadway. 2 floot rock wall on each side of roadway. 2 floot rock wall on each side of roadway and roadway. General cracking along roadway. Electrical box is round floot to roadway and roadway. Will be side of roadway along roadway. In the side of roadway is round floot of roadway along roadway. In the side of roadway is round floor roacking along roadway. Electrical box is round floor roacking along roadway. Electrical boxes seen up street.	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_aligned	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25% no_dry_bottom yes_<25% no_dry_bottom yes_<25%	sediment/leaves, v egratation growth, branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves, v egratation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches_sedimen cubert t eaves at_stream_grade reinforced_concrete_pipe within_cubert other at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 0 24 24 11 244 24 0	0 satisfactory none 0 good none 0 satisfactory none 0 satisfactory none 0 fair minor culvert	Small surface cracks at top of inlet. Minor scaling at bottom of pipe. Obstruction boards with in flared end section, but not within pipe. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Joseph Sale of Sa	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory 4.5 satisfactory dry_laid_stone_brick satisfactory 4 satisfactory dry_laid_stone_brick fair	Top of headwall/retaining sno no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general viro no General tree growth on em no no	1 into thick iron rod located approximately 8 feet away from costlet. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert does not exist. Small tree growing at base of flared end section. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert comprised of four identical RCP ppes. Pipes are numbered one through flow when beeing closed to Main Street. Span is 10 feet. Culhert comprised of four identical RCP ppes. Pipes are numbered one through flow when beeing closed to Main Street. Span is 10 feet. Culhert comprised of four identical RCP ppes. Pipes are numbered one through flow when beeing closed to Main Street. Span is 11 feet. Small tree growing at base of flour dentical RCP ppes. Pipes are numbered one through flow with meeb length or search and the pipes. Culhert comprised of four identical RCP ppes. Pipes are numbered one to Main Street. Span is 11 feet. Small tree growing at base of flour dentical RCP ppes. Pipes are numbered one to Main Street. Span is 11 feet. Fig. 10.1056 42.66459
B - 1556 B - 167 B - 1666 B - 1666 B - 164	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:43 clear 11/11/2020 15:02 clear 11/11/2020 15:09 clear	ewm yes ewm cuhert_wes_root_loca ewm yes ewm yes wnb cuhert_wes_root_loca ewm yes	yes yes sted_d no	paved paved	actisfactory actisfactory actisfactory	21 21 22 22 20 20 21	45 yes other,drainage_system 60 yes drainage_system,other 60 yes other 47 yes other	yes other satisfa no yes guardrall satisfa	seen along street. Nearby cutch basins on each side of roadway. Zo foot rock wall on each side of containing wall on each side of roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. Z foot rock wall on each side of roadway. Z foot rock wall on each side of roadway and of roadway. To control of the containing wall. General cracking along roadway. Electrical box is room. Minor cracking along roadway. Electrical box is room. Minor cracking along roadway.	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_aligned	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25% no_dry_bottom yes_<25% no_dry_bottom yes_<25%	sediment_leaves_v egetation_growth, downstream branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment_leaves_v egetation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches_sedimen cubert Useaves at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 0 24 24 48	0 satisfactory none 0 good none 0 satisfactory none 0 satisfactory none	Small surface cracks at top of inlet. Minor casing at hotton of pipe. Obstanction boards with in flared end section, but not within pipe. Minor chipping concerted missing within flared end section, but not within pipe. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Siope_flatter_than_2:1_ Outsite in a flared end section. On the right side of flared end section there is even browing through, Minor chips and flared end section there is even showing through, Minor chips and flared end section persists. 1. chatracted approximately 50% while the res are less than 23%. 1. & Late_graph in missing from culvert opening. 2: concrete separating from culvert at bottom right. 2: minor chips at 10 opening. All flored see Salling. 2. moderate amount of rebar showing through, 2: In chick pin concrete on left side. 4: exposed of culverts from surrounding concrete. Moderate scalling, at the chips of pices. Minor surface not at bottom of headwall_retaining_wall_slo Top of culverts from surrounding concrete. Moderate scalling, at other	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory 4.5 satisfactory dry_laid_stone_brick satisfactory	Top of headwall/retaining two no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general v no no	I into thick iron rod located approximately 8 feet away from costlet. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert does not exist. Small tree growing at base of flared end section. Culhert does not exist. Culhert does not exist. Culhert does not exist. Culhert comprised of four identical RCP pper. Pipers are numbered one through flow which me being closest to Main Street. Span is Officer. Culhert comprised of four identical RCP ppics. Pipers are numbered one through flow with one being closest.
B - 1550 B - 1661 B - 1661 B - 1660 B - 1691 B - 1631 B - 1630 B - 1591	11/11/2020 13:36 Clear 11/11/2020 13:59 Clear 11/11/2020 14:05 Clear 11/11/2020 14:05 Clear 11/11/2020 15:02 Clear 11/11/2020 15:09 Clear 11/11/2020 15:09 Clear 11/11/2020 15:47 Clear 11/11/2020 16:41 Clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes ewm yes ewm yes	yes yes sted_d no	paved paved paved paved paved	actisfactory actisfactory actisfactory	21 22 22 22 20 20	45 yes other, drainage_system 60 yes drainage_system, other 60 yes other 47 yes other 47 yes other	yes other satisfied to the satisfied to	seen along street. Nearby cutch basins on each side of readway. Foot rocks will not each aide of rector, residency part of retaining wall. Minor cracks along readway. Electrical boxes seen along street. Nearby catch basins on each side of readway. 2 floot rock wall on each side of readway. 2 floot rock wall on each side of readway part of retaining wall. General cracking along readway. Plectrical box is considered and part of retaining wall. Minor cracking along readway. Electrical box is roun. Minor cracking along readway. Electrical boxes seen up street.	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_aligned d no_discernable_stream_channel	id no_dry_bottom yes_25_50% id no_dry_bottom no id no_dry_bottom yes_425% id no_dry_bottom yes_425% id no_dry_bottom yes_425% id no_dry_bottom no	sediment,leaves, v egratation growth, branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment,leaves, v egratation_growth, other at_stream_grade reinforced_concrete_pipe upstream_within_branches,sedimen culvent Leaves at_stream_grade reinforced_concrete_pipe within_culvent other at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe within_culvent other at_stream_grade reinforced_concrete_pipe at_stream_grade corrupted_metal_sipe	24 24 48 24 24 0 24 24 11 244 24 0 12 12 0	0 satisfactory none 0 good none 0 satisfactory none 0 fair minor culvert 0 fair moderate culvert 0 satisfactory minor culvert	Small surface cracks at top of inlet. Minor casing at hottorn of pipe. Obstruction boarded with in flared end section, but not within pipe. Minor chipping do concrete missing within flared end section. Minor chipping on outside of inlet. Idope_flatter_than_2:1_ Outlet is a flared end section. The right side of flared end section that the section there is every side of flared end section there is only side of flared end section there is only side of flared end section there is only side of flared end section. 1. obstructed approximately 50% with the rere are less than 20%. 1. obstructed approximately 50% of flared end section of flared end section. 1. obstructed approximately 50% of flared end section flared end section. 2. moderate and section of the	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory 4.5 satisfactory dry_laid_stone_brick satisfactory 4 satisfactory dry_laid_stone_brick fair 2.5 poor dry_laid_stone_brick poor	Top of headwall/retaining vno no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general vno no General tree growth on em no no Moderate erosional along rno no	1 inch thick iron rod located approximately 8 feet away from outet. Cuhert does not exist. Small tree growing at base of flured end section. Small tree growing at base of flured end section. Cuhert does not exist. Cuhert does not exist. Cuhert does not exist. Cuhert does not exist. Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being does to Main Street. Span is 10 feet. Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being does to Main Street. Span is 10 feet. Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through flow with one being does to Main Street. Span is 10 feet. Small tree growth at first. Small tree growth sinder. Satisfactory 71.0163 42.66449 General tree growth halfet. Satisfactory 71.0169 42.66459 General tree growth halfet. Satisfactory 71.0189 42.66569
B - 1556 B - 167 B - 1664 B - 164 B - 1631 B - 1631	11/11/2020 13:36 clear 11/11/2020 13:59 clear 11/11/2020 14:05 clear 11/11/2020 14:05 clear 11/11/2020 15:02 clear 11/11/2020 15:09 clear	ewm yes ewm cuhert_was_not_loca ewm yes ewm yes ewm yes ewm yes ewm yes	yes yes sted_d no	paved paved paved	actisfactory actisfactory actisfactory	21 22 22 22 20 20 21	45 yes other, drainage_system 60 yes drainage_system, other 60 yes other 47 yes other	yes other satisfied to the satisfied to	seen along street. Nearby cutch basins on each side of roadway. 2 floot rock wall not each aide of catchy cabows part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. 2 floot rock wall on each side of roadway. 2 floot rock wall on each side of roadway. 2 floot rock wall on each side of roadway and roadway. General cracking along roadway. Electrical box is round floot to roadway and roadway. Will be side of roadway along roadway. In the side of roadway is round floot of roadway along roadway. In the side of roadway is round floor roacking along roadway. Electrical box is round floor roacking along roadway. Electrical boxes seen up street.	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_aligned d no_discernable_stream_channel	il no_dry_bottom yes_25_50% il no_dry_bottom no il no_dry_bottom yes_<25% no_dry_bottom yes_<25% no_dry_bottom yes_<25%	sediment/leaves, v egratation growth, branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment/leaves, v egratation_growth, downstream other at_stream_grade reinforced_concrete_pipe upstream_within_branches_sedimen cubert t eaves at_stream_grade reinforced_concrete_pipe within_cubert other at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 0 24 24 11 244 24 0	0 satisfactory none 0 good none 0 satisfactory none 0 satisfactory none 0 fair minor culvert	Small surface cracks at top of inlet. Minor cracking, a bottom of pipe. Otherschon located with in flared end section, but not within pipe. Minor chips and concrete missing within flared end section. Minor chipsing on outside of inlet. Outlet is a flared end section. Minor chipping on outside of inlet. Outlet is a flared end section. On the right side of flared end section flore is crear showing through. Minor chips and flared end section there is rebar showing through. Minor chips and flared end section opening. 1: obstructed approximately 50% while the rear are less than 25%. 1 & L. targe chip missing from separating from cuber at bottom right. 3: minor chips at 11 copening. All flowders texture to the showing through 3. 1: inch chip in concrete. Moderate scaling. 2: moderate amount of rebar showing through 3. 1: inch chip in concrete on left side. 4: exposed rebar within pore. All: separation of cuberts from surrounding concrete. Moderate scaling at other standard of pope. Top of outwert opening slightly dented inwards. Moderate surface and surf	2 dry_laid_stone_brick fair 2 satisfactory 2.5 satisfactory 4.5 satisfactory dry_laid_stone_brick satisfactory 4 satisfactory dry_laid_stone_brick fair	Top of headwall/retaining sno no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general viro no General tree growth on em no no	1 inch thick iron rod located approximately 8 feet away from costlet. Culhert does not exist. Culhert does not exist. Culhert service arrows in left control strategy of the control strategy arrows in left control strategy are an enumbered one through flow with one being closest to Main Street. Span is 11 feet. Culhert comprised of four identical 8CP pipes. Pipes are numbered one through flow with one being closest to Main Street. Span is 11 feet. General tree growth at inlet. fair -71.0169 42.66459 General tree growth at inlet. fair -71.0169 42.66459
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Pipes are numbered one through flow with one being closest to Main Street. Span is 10 feet. Fair -71.0169 42.66459 Moderate through growth directly fair -71.0170 42.66469 Culvert leads into private culvert approximately 15 feet away. poor -71.0207 42.66459 Small beaver dam located on the left side of headwall. Fair -71.0166 42.6659
B - 1550 B - 1661 B - 1660 B - 1660 B - 1630 B - 1630 B - 1630 B - 1621 B - 1620 B - 1620 B - 1570 B - 1570	11/11/2020 13-36 clear 11/11/2020 13-39 clear 11/11/2020 14-05 clear 11/11/2020 14-05 clear 11/11/2020 14-05 clear 11/11/2020 15-02 clear 11/11/2020 15-02 clear 11/11/2020 15-02 clear 11/11/2020 16-02 clear 11/11/2020 17-18 clear 11/11/2020 17-18 clear 11/11/2020 18-05 clear 11/11/2020 18-05 clear	ewm yes	yes yes yes yes yes yes yes yes	paved	autisfactory autisfactory autisfactory autisfactory good good good good autisfactory autisfactory autisfactory autisfactory	20 21 21 23 23	45 yes other, drainage_system 60 yes drainage_system, other 60 yes other 47 yes other 47 yes other 30 no 30 yes overhead_wires 33 yes overhead_wires 37 no 37 yes overhead_wires 37 no 37 yes overhead_wires	yes other satisfied of the satisfied of	seen along street. Nearby cutch basins on each side of roadway. Entor ciscle will no each aide of receive part of retaining wall. Minor cracks along roadway. Electrical boxes seen along street. Nearby cutch basins on each side of roadway. 2 floor tox will not each side of roadway. 2 floor tox will on each side of roadway. 2 floor tox will one each side of roadway. 2 floor tox will one each side of roadway. 2 floor tox will one each side of roadway. Pleating wall on each side of the roadway and the street. Minor cracking along roadway. Electrical box is catchy located just down the street. Minor cracking along roadway. Electrical box is round the street. Roadway appears freshly paved and painted. round the street is round the street is round the street. Roadway appears freshly paved and painted. round the street is round the stree	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_stream_channel d roadway_aligned stream_stream_channel d roadway_aligned stream_channel d roadway_aligned stream_channel	d no_dry_bottom yes_25_50% d no_dry_bottom no d no_dry_bottom yes_25% no_dry_bottom yes_25% no_dry_bottom yes_25% flowing yes_25% flowing yes_25% stagnant no no_dry_bottom no no_dry_bottom yes_25%	sediment,leaves, v egratation growth, branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment,leaves, v egratation_growth, other at_stream_grade reinforced_concrete_pipe upstream_within_branches,sedimen cubert Obewes at_stream_grade reinforced_concrete_pipe within_cubert other at_stream_grade corrupated_metal_pipe within_cubert sediment at_stream_grade corrupated_metal_pipe within_cubert sediment_leaves at_stream_grade corrupated_metal_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe within_cubert sediment at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 0 24 24 48 24 24 11 244 24 0 12 12 0 12 9 9 24 20 20 24 21 23 30 30 30 30 30 30 30	O satisfactory none O good none O satisfactory none O satisfactory none O fair minor culvert O fair minor culvert O fair minor culvert 2.5 fair minor culvert 4.5 poor none culvert 13 fair none O satisfactory none	Small surface cracks at top of inlet. Minor casing at bottom of pipe. Obstruction board with in flared end section, but not within pipe. Minor chipping concusted of inlet. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Outset is a flared end section. Minor chipping on outside of inlet. I obstructed approximately 50% while the rest are less than 25%. 1 obstructed approximately 50% while the rest are less than 25%. 1 & 6.1 are of him surface and section opening. I obstructed approximately 50% while the rest are less than 25%. 1 & 6.2 trace point pipe. All traparation of culters from culter to pening. 2: concrete separating from culter to pening. 2: concrete separating from culter to pening. 2: concrete separating from surface and section opening the same and at 10 opening. All: Moderate scaling. 2: moderate amount of reference of culters from surrounding concrete. Moderate is calling at concrete one list. Minor surface rust a bottom of pipe. Top of othert opening slightly dented inward. Moderate scaling to the second of the seco	2 dry_laid_stone_brick fair 2 satisfactory 4.5 satisfactory 4.5 satisfactory 4 satisfactory 4 satisfactory 4 satisfactory 4 satisfactory 6 ry_laid_stone_brick 7 poor 7 poor 6 ry_laid_stone_brick 7 poor 7 poor 6 ry_laid_stone_brick 7 poor 7 poor 7 satisfactory 6 ry_laid_stone_brick 7 poor 7 poor 8 satisfactory 7 poor 8 satisfactory 10 poor 11 poor 12 satisfactory 12 satisfactory 13 poor 14 concrete_poured_ 15 fair 16 fair 17 concrete_poured_ 2.5 poor 2.5 poor	Top of headwalf/retaining uno no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general uno no General tree growth on emno no Moderate erosional along rno no Embarkment shows moder no no Minor erosion on embarke no no Minor erosion along embar yes no Embarkment shows moder no yes Minor tro moderate erosion no Moderate to severe erosion no	1 inch thick iron rol located approximately 8 feet away from outlet. Culvert does not east. Culvert line is part of an inlet control structure variously stores and a corrugated metal pipe. satisfactory 71.0156 42.66351 Small tree growing at base of filtered end section. Culvert does not east. Culvert does not east. Culvert does not east. Culvert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being closest to Main Street. Joan is 10 feet. satisfactory 71.0169 42.66434 Culvert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being closest to Main Street. Joan is 10 feet. satisfactory 71.0169 42.66443 Culvert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being closest to Main Street. Joan is 10 feet. fair 71.0169 42.66459 General vegetation and tree growth directly outside of outlet. fair 71.0169 42.66469 Moderate thorn growth directly next to culvert. fair 71.0207 42.66469 Cutlet leads into private culvert approximately 15 feet away. poor 71.0207 42.66469 Small beaver dam located on the left side of headwail. poor 71.0207 42.66459 Small active dam located on the left side of headwail. poor 71.0207 42.66459 General tree growth throughout stream. Large amount of fallen branches and twigs at outlet. fair 71.0366 42.66751
B - 1550 B - 1661 B - 1660 B - 164 B - 1630 B - 1630 B - 1630 B - 1630 B - 1621 B - 1620 B - 1620 B - 1570 B - 1570	11/11/2020 13-36 clear 11/11/2020 13-39 clear 11/11/2020 14-05 clear 11/11/2020 14-05 clear 11/11/2020 14-05 clear 11/11/2020 15-02 clear 11/11/2020 15-02 clear 11/11/2020 15-02 clear 11/11/2020 16-02 clear 11/11/2020 17-18 clear 11/11/2020 17-18 clear 11/11/2020 18-05 clear 11/11/2020 18-05 clear	ewm yes	yes yes yes yes yes yes yes yes	paved	astisfactory astisfactory astisfactory astisfactory astisfactory good good good good astisfactory astisfactory astisfactory astisfactory astisfactory astisfactory	20 21 21 23 23	45 yes other, drainage_system 60 yes drainage_system, other 60 yes other 47 yes other 47 yes other 30 no 30 yes overhead_wires 33 yes overhead_wires 37 no 37 yes overhead_wires 37 no 37 yes overhead_wires	yes other satisfied of the satisfied of	seen along street. Nearby cutch basins on each side of roadway. Foot rocks will no each side of roadway. Foot rocks will not each side of roadway. Fleetrical boxes seen along street. Nearby cutch basins on each side of roadway. Fleetrical boxes seen along street. Nearby cutch basins on each side of roadway. Fleetrical boxes seen along street. Nearby cutch basins on each side of roadway. Fleetrical box is considered to the street. General cracking along roadway. Fleetrical box is considered to the street. Minor cracking along roadway. Electrical box is considered to the street. Minor cracking along roadway. Electrical box is roadway appears freshly pawed and painted. Tourn Minor cracking along roadway. Fleetrical box is roadway appears freshly pawed and painted. Tourn Minor cracking along roadway. Minor oil stains on roadway. Minor oil stains on roadway. Minor cracking along roadway. Small oil stains along roadway. Minor oil stains on roadway. Small oil stains along roadway. Small oil stains along roadway. Minor oil stains on roadway.	d skewed_>45* no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned no_discernable_stream_channel d roadway_aligned stream_aligned d roadway_aligned stream_stream_channel d roadway_aligned stream_stream_channel d roadway_aligned stream_channel d roadway_aligned stream_channel	in o_dry_bottom yes_25_50% in o_dry_bottom no in o_dry_bottom yes_25% no_dry_bottom yes_25% no_dry_bottom yes_25% flowing yes_25% flowing yes_25% stagnant no	sediment,leaves, v egratation growth, branches at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe sediment,leaves, v egratation_growth, other at_stream_grade reinforced_concrete_pipe upstream_within_branches,sedimen cubert Obewes at_stream_grade reinforced_concrete_pipe within_cubert other at_stream_grade corrupated_metal_pipe within_cubert sediment at_stream_grade corrupated_metal_pipe within_cubert sediment_leaves at_stream_grade corrupated_metal_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe within_cubert sediment at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	24 24 48 24 24 48 24 24 48 24 24 11 244 24 0 12 12 0 12 12 0 24 21 23 30 30 30 30 30 30 30 24 24 20 24 26 18 24 20 18	O satisfactory none O good none O satisfactory none O satisfactory none O fair minor culvert O fair minor culvert O fair minor culvert 2.5 fair minor culvert 4.5 poor none culvert 13 fair none O satisfactory none	Small surface cracks at top of inlet. Minor casing at hotton of pipe. Gottanction board with in flared end section, but not within pipe. Minor chipping concerted missing within flared end section, but not within pipe. Minor chipping on outside of inlet. Minor chipping on outside of inlet. Coutlet in a flared end section. Minor chipping on outside of inlet. Side of listed end section on the right side of flared end section there is ever showing through, Minor chipping and growing through, Minor chipping and growing through the pipe. L. chaltracted approximately 50% while the rea se in less than 2%. 1. 6.4 Large chipping. 1. 6.4 Large chipping and the section there is a set of the standard proximately and the separating from culvert opening. 2: concrete separating from subrate and section of culverts from surrounding concrete. Moderate scaling, at the section of culverts approximately 3 inches deep with a freat best or culvert. Adoption of pipe. Minor surface rust at bottom of pipe. All present and control of pipe. Minor surface rust at bottom of pipe. All present yellows the pipe in searly fillied with water. A outsight throughout pipe. Type is nearly fillied with water. A outsight throughout pipe. Minor surface rust at bottom of pipe. Mi	2 dry_laid_stone_brick fair 2 satisfactory 4.5 satisfactory 4.5 satisfactory 4 satisfactory 4 satisfactory 4 satisfactory 4 satisfactory 6 ry_laid_stone_brick 7 poor 7 poor 6 ry_laid_stone_brick 7 poor 7 poor 6 ry_laid_stone_brick 7 poor 7 poor 7 satisfactory 6 ry_laid_stone_brick 7 poor 7 poor 8 satisfactory 7 poor 8 satisfactory 10 poor 11 poor 12 satisfactory 12 satisfactory 13 poor 14 concrete_poured_ 15 fair 16 fair 17 concrete_poured_ 2.5 poor 2.5 poor	Top of headwalf/retaining uno no Minor erosion along embar no no Minor erosion along embar no no Embarkment has general uno no General tree growth on emno no Moderate erosional along rno no Embarkment shows moder no no Minor erosion on embarke no no Minor erosion along embar yes no Embarkment shows moder no yes Minor tro moderate erosion no Moderate to severe erosion no	1 inch thick iron rol located approximately 8 feet away from outet. Cuhert does not exist. Cuhert does not exist. Cuhert liefs is part of an inlet control structure unrounded by stones and a corrugated metal pipe. satisfactory 71.0156 42.66351 Small tree growing at base of flured end section. Cuhert does not exist. Cuhert does not exist. Cuhert does not exist. Cuhert does not exist. Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being doesn't to Main Street. Span is 10 feet. satisfactory 71.0163 42.66449 Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being doesn't to Main Street. Span is 10 feet. satisfactory 71.0163 42.66449 Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being doesn't to Main Street. Span is 10 feet. fair 71.0190 42.66449 Cuhert comprised of four identical 8CP pipes. Pipes are numbered one through four with one being doesn't fair 71.0191 42.6657 Moderate thorn growth directly next to cuhert. fair 71.0207 42.66469 Moderate thorn growth directly next to cuhert. fair 71.0207 42.66469 Small beaver dam located on the left use of headwall. poor 71.0207 42.66458 Small beaver dam located on the left use of headwall. poor 71.0207 42.66559 Centeral tree growth throughout stream. 71.0366 42.66751 Thorn bush directly next to cuhert. fair 71.0329 42.66571

						Pipe was completely buried	Culvert connection to close
B - 180i	11/12/2020 13:25 clear ewm yes yes	paved satisfactory 21	87 yes other,drainage_system no seen along roadway. Electric box	es round skewed_<45° no_discernable_stream_channel no_dry_bottom yes_25_50%	sediment,leaves, o within_culvert ther at_stream_grade reinforced_concrete_pipe 12 9 12 0 satisfactory	upon arrival. Three large stones were at inlet opening. slope_steeper_than_2:1_ 3 failing dry_laid_stone_brick failing Severe erosion throughout no	drainage system that outlets to
B - 180o	11/12/2020 13:44 clear ewm yes no	paved satisfactory 21	Minor cracking along roadway. Electric box 87 yes drainage_system.other no seen along roadway.	es round skewed_c45° no_discernable_stream_channel no_dry_bottom yes_c25%	vegetation growt downstream,with h,branches,sedim n_culvert entleaves at_stream_grade reinforced_concrete_pipe 12 0	Cultert was unapproachable due to thick view and thom growth over embastment and cutter area. slope, flatter; than 2-1, 3 poor Minor to moderate erosion no	due to a vegetation overgrowth with thorns. Outlet is part of clothes drainage system that connects to no inlet71.0303 42.65691
		,	Moderate cracks and small potholes in			Severe rust throughout pipe. On culvert, he both bottom and sides of pipe it	Culvert connects through a close
B - 319o	11/12/2020 14:27 clear ewm yes yes	paved fair 22	65 yes drainage_system, gas, other no roadway. Electrical boxes seen down roadw Moderate cracks and small potholes in	ay. round roadway_aligned stream_aligned stagnant yes_25_50%	within_culvert sediment,other at_stream_grade corrugated_metal_pipe 9 6 10 0.5 failing upstream, within other, leaves, sedi		no drainage system. falling -71.0243 42.66105 Culvert inlet cannot be found due to being buried by sediment, rocks
B - 319i	11/12/2020 14:44 clear ewm yes no	paved fair 22	65 yes other,drainage_system, gas no roadway. Electrical baxes seen down roadw	ay. round completely_obs			no and leaves. failing -71.0243 42.66116
B - 179i	11/12/202015:17 clear ewm yes yes	paved satisfactory 21	Minor cracks along roadway. Electrical box 80 yes other no seen up street.	round skewed_<45° no_discernable_stream_channel no_dry_bottom no	at_stream_grade reinforced_concrete_pipe 12 12 0 0 satisfactory	separated from second section culvert approximately 1 inch. slope_steeper_than_2:1_ 4.5 satisfactory Embankment is reinforced no Outlet is a fire off section.	General vegetation growth at inlet opening. satisfactory -71.0198 42.65704
0.170-	11/12/2020 15:35 clear ewm ves ves	naved satisfactory 21	Minor cracks along roadway. Electrical box seen up street. Closed drainage system see 80 yes other,drainage system no street.	ts up round skewed_<45° no_discernable_stream_channel no_dry_bottom no	at_stream_grade_reinforced_concrete_pipe 12 12 0 0-satisfactory	General moss growth: Flared end section length shifted 1 inch upwards compared to other are culvert length: siope_steeper_than 21_ 6.5 satisfactory dry_laid_stone_brick fair Minor erosion on embankn no	no General tree growth. satisfactory -71.0198 42.65689
B - 1/30	11/11/2020 15:35 clear ewm yes yes	paved satisfactory 21	su yes orner, orannage_system no street.	round skewed_<45: no_discernable_stream_cnannel no_dry_bottom no	ar_stream_grade reinforceo_concrete_pipe 12 12 U U-satisfactory		no General tree growth. satisfactory -/1.0198 -42.55689
						inlet is a flutred end section. On right side of flutred end section, approximately if siches of	
B - 320i	11/12/2020 15:56 clear ewm yes yes	paved good 21	65 yes other no Electrical box seen up street.	round skewed_>45° no_discernable_stream_channel no_dry_bottom yes_25_50%	leaves,sediment,o within_culvert ther at_stream_grade reinforced_concrete_pipe 12 10 21 0 satisfactory	concrete its missing with exposed rebar. Obstruction is in flared end rebar. Obstruction is in flared end culvert section but not throughout pipe. slope_steeper_than_2.1_ 3 fair Moderat Erosion along emi no	Inlet has severe scour underneath culvert. Inlet is placed within a no basin. fair -71.0191 42.65794
B - 320o	11/12/2020 16:13 clear ewm yes yes	paved good 21	65 yes other no Electrical box seen up street.	round skewed_>45° no_discernable_stream_channel no_dry_bottom yes_<25%	sediment_leaves,0 within_culvert ther at_stream_grade reinforced_concrete_pipe 12 12 23 0 satisfactory	Outlet is a flared end section. slope_steeper_than_2:1_ 4.5 satisfactory Embankment is comprised no	no satisfactory -71.019 42.65779 Culvert was not found on Perley
B - 177	11/12/2020 16:30 clear wnb culvert_was_not_located_d no					Culvert is comprised of 2 - 48	Lane71.0166 42.65737
			Moderate cracks and small potholes with			inch RCP pipes. Pipes are numbered 1 through 2 with 1 being closer to Ferly tame. Span	
B - 184i	11/12/2020 16:46 rain ewm yes yes	paved fair 19	patches within roadway. Electrical boxes se down the street. Closed drainage system lo 53 yes other.gas_line,drainage_system yes guardrali satisfacton next to culvert.		upstream leaves at_stream_grade_reinforced_concrete_pipe 482 48 172 1 satisfactory	of pipes is 10 feet. 2: exposed rebar on right side. 1 & 2:	no Fallen limbs in stream bed. satisfactory -71.0138 42.65511
						Culvert is comprised of 2 - 48 inch 8CP piper. Pipes are	, ·
			Moderate cracks and small potholes with patches within roadway. Electrical boxes: so down the street. Closed drainage system to		branches.leaves.s	numbered i through 2 with 1 being closer to Perfey Jane. Span of joses 10.1 i 28.2 Minor scaling	General vesetation, vine, and tree
B - 184o	11/12/2020 17:01 rain ewm yes yes	paved fair 19	53 yes gas_line,drainage_system,other yes guardrail satisfactor\next to culvert.	round roadway_aligned stream_aligned flowing yes_<25%	downstream ediment at_stream_grade reinforced_concrete_pipe 482 48 272 4 satisfactory	12 and most growth mof of pipe. Exposed refer on intel opening. Exposed refer on intel opening.	no growth within stream. fair -71.0136 42.65509
B - 186i	11/12/2020 17:21 clear ewm yes yes	paved fair 20	53 yes other,gas_line no Minor to moderate cracking along roadway	round roadway_aligned stream_aligned no_dry_bottom no	at_stream_grade reinforced_concrete_pipe 30 30 0 0 satisfactory	Concrete appears to be	General tree and vegetation growth no at inlet opening. satisfactory -71.0135 42.65391
B - 1860	11/12/2020 17:38 clear ewm yes yes	paved fair 20	Minor to moderate cracking along roadway 53 yes gas_line,other yes guardrail satisfacton, Electrical boxes seen up street.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_<25%	downstream branches,leaves perched reinforced_concrete_pipe 30 30 0 0 satisfactory		no satisfactory -71.0133 42.65405
			Moderat cracking along roadway. Sides of			Culver tin let a n flared end section. Obstructions are opportunated within flared end	
B - 321i	11/12/2020 17:54 clear ewm yes yes	paved fair 20	roadway are starting to crumble. Electrical I 70 yes other yes guardrall satisfactor, seen along roadway.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_50_75%	upstream,within_other,leaves,sedi culvert ment at_stream_grade reinforced_concrete_pipe 12 8.5 24 0 satisfactory	section. Obstructions within culvert are less than 25%. slope_steeper_than_2:1_ 5 poor dry_laid_stone_brick failing Embankment shows moderno	poor -71.0104 42.65307
						Outlet is a flared end section. Flared end section length	
						separated from second length approximately 6 inches allowing a significant amount of sediment	
B - 321o	11/12/2020 18:10 clear ewm yes yes	paved fair 20	Moderate cracking along road way. Edge of roadway starting to crumble over. Electrical 70 yes other yes guardrail satisfactory boxes seen in along roadway.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_50_75%	within_culvert,do sediment,leaves,b wnstream ranches at_stream_grade reinforced_concrete_pipe 12 12 12 0 poor	to pile up. Flared end section right side has exposed rebar in a culvert small sized crack: slope_steeper_than_2-1_ 5.5 fair General tree growth. Emba no	no poor -71.0104 42.65309
B - 187i	11/12/2020 18:33 clear ewm yes yes	paved fair 20	Moderate cracking along roadway. Electric 48 yes other yes guardrail satisfactory boxes seen up street.	al round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_<25%	within_culvert leaves,sediment inlet_drop reinforced_concrete_pipe 12 12 11 0 satisfactory	Inlet is a flared end section. slope_steeper_than_2:1_ 4 fair Minor erosion along embar no	no fair -71.0098 42.65356
			Moderate cracking along roadway. Electric boxes seen up street. Fire hydrant located			Outse is a fixered end section. Top of fixer end section has a small chip in concrete, Inside of	
B - 187o	11/12/2020 18:48 clear ewm yes yes	paved fair 20	approximately 25 feet away from culvert. G rail has significant cracks and breaks, is acti 48 yes other,water_line yes guardrail poor overturning.	uard ety round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_75_100%	downstream,with in_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe 12 5.5 14 0 poor	culvert looks satisfactory but is unable to pass water due to	General tree growth at outlet no opening. poor -71.0099 42.65363
B - 188i	11/12/2020 19:28 clear ewm yes yes	paved good 23	51 no no	round skewed_>45* no_discernable_stream_channel no_dry_bottom no	at_stream_grade corrugated_metal_pipe 12 12 0 0 satisfactory	Minor surface rust throughout rate culvert pipe. headwall_retaining_wall_slo 4 fair dry_laid_stone_brick poor Minor erosion and General no	General tree growth and fallen no limbs at and around culvert. fair -71.004 42.65335
B - 1880	11/12/2020 19:41 clear ewm yes yes	paved good 23	S1 yes overhead_wires no	round skewed_>45° no_discernable_stream_channel no_dry_bottom yes_<25%	downstream,with sediment,other,le in_culvert aves at_stream_grade corrugated_metal_pipe 12 9.5 8 0 satisfactory	culvert.he Minor surface rust throughout adwall plpe. headwall_retaining_wall.slo 4.5 poor dry_laid_stone_brick poor Embankment has moderatino	General thorn growth. Nearby RCP no pipe outlets to same location. poor -71.0039 42.65324
B - 322o	11/12/2020 19:52 clear ewm yes yes	paved good 23	yes overhead wires no	round skewed_>45° no_discernable_stream_channel no_dry_bottom	within_culvert_sediment,leaves_at_stream_grade_reinforced_concrete_pipe 18 11 30 0 satisfactory	Outlet is a flarred end section. Minor chipping of concrete at flared end section opening. slope_steeper than 2-1, 3 poor dry_laid_stone_brick poor Moderate erosion and gen no	Could not see through other side of pipe. No nearby closed drainage no system. Inlet was not locatable. poor -71.0039 42.65326
							Culvert inlet could not be found. Pin location is approximate based
B - 322i	11/12/2020 20:18 clear ewm yes yes	paved good 23	yes overhead_wires no	completely_obs	within_culvert,up reinforced_concrete_pipe 18		on angle of pipe. No closed drainage system nearby71.004 42.65343
B - 92o	11/13/2020 13:13 clear ewm yes yes	paved satisfactory 22	47 no yes guardrail satisfactor, Patches along roadway.	round roadway_aligned skewed_<45° stagnant yes_25_50%	upstream,within_sediment,other,le culvert aves at_stream_grade corrugated_metal_pipe 18 15 18 9.5 poor	Severe surface nust throughout pipe. Close to 2 feet of its top 5 and his missing from the opening. headwall_retaining_wall_slo 5 and his missing from the opening. headwall_retaining_wall_slo 5.5 poor dry_laid_stone_brick falling Moderate to severe erosionyes	General tree growth. Erosion on right side of stream. failing -70.9763 42.69003
B - 92i	11/13/2020 13:36 clear ewm yes yes	paved satisfactory 22	47 yes overhead_wires no Hatch along roadway.	round roadway_aligned stream_aligned flowing completely_obs	upstream,within_sediment,leaves,o nucteculvert ther corrugated_metal_pipe 185 failing	Culvert is completely obstructed 5 and uninspectable. headwall_retaining_wall_slope_steeper_t failing dry_laid_stone_brick failing Embankment has severe er no	Inspection of culvert is on uninspectable due to collapsed no headwall. failing -70.9766 42.69016
						Severe surface rust throughout pipe. On left side of culvert	Outlet located on private property. Culvert length from inlet to 5 gallon
B - 90i	11/13/2020 13:57 clear ewm yes yes	paved satisfactory 23	47 no no Patches al ong roadway.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_25_50%	within_culvert,do wnstream sediment,leaves at_stream_grade corrugated_metal_pipe 12 6 12 0 poor	approximately 6 inches of pipe missing. headwall_retaining_wall_slo 3 fair mortared_stone_brick falling Embankment has minor errno	bucket located at mid span, buried no with sediment. poor -70.977 42.69153
						Outlet located on private property, 5 gallon bucket located	
						at mid span of pipe completely buried with aediment. Culvert length is distance from inlet to 5	
B - 90o	11/13/2020 14:11 clear ewm yes yes	paved satisfactory 23	47 yes overhead_wires no Patches along roadway.	round skewed_<45° no_discernable_stream_channel	unknown corrugated_metal_pipe 12	gallon bucket, distance from 5 gallon bucket to outet unknown due to private property. no	-70.9768 42.69162
						Severe rust throughout pipe.	
						Bottom 3 to 6 inches of bottom of pipe missing on both sides. Large puncture and indent on the	
B - 91i	11/13/2020 14:56 rain ewm yes yes	paved satisfactory 23	45 no no Patches along roadway.	round roadway_aligned skewed_<45° no_dry_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe 18 13 18 0 failing	Severe rust throughout pipe.	Fallen trees and limbs from no headwall and throughout stream. failing -70.979 42.69065
					vegetation_growt	Approximately 1 foot on right side missing. Large dent approximately 4 feet inward at	General tree growth throughout stream. Utility pole directly behind
B - 91o	11/13/2020 15:21 rain ewm yes yes	paved satisfactory 23	45 yes overhead_wires no Patches along roadway.	round roadway_aligned stream_aligned no_dry_bottom yes_25_50%	sediment,leaves,o	headwall \$ top of pipe. headwall_retaining_wall.slo 4 satisfactory mortared_stone_brick satisfactory Embankment has minor er no Severe rust throughout pipe. headwall.c Bottom and sides of pipe are	no wing wall. poor -70.9789 42.69049
B - 89o	11/13/2020 15:45 clear ewm yes yes	paved fair 23	51 yes overhead_wires no Patches along roadway with small potholes	. elliptical skewed_>45° no_discernable_stream_channel no_dry_bottom yes_<25%	within_culvert ther at_stream_grade corrugated_metal_pipe 18 11.5 18 0 failing	rate ulvert missing. headwall_retaining_wall_slo 2.5 satisfactory mortared_stone_brick satisfactory Embankment has general v no Severe nut throughout. First 3	poor -70.9796 42.69199
					A contract	feet of jupie is missing top and left alse. Afterward top of culvert has best and concreed in on	Headwall is attached to an inlet control structure made of mortared stone. Inlet control structure is
B - 89i	11/13/2020 16:00 clear ewm yes yes	paved fair 23	51 no no Patches along roadway and small potholes.	elliptical skewed_>45* no_discernable_stream_channel no_dry_bottom yes_75_100%	leaves.sediment,o within_culvert ther at_stream_grade corrugated_metal_pipe 18 10 18 0 failing		failing and has multiple voids no throughout. failing -70.9797 42.69207
n en	11/2/2000 (63) 415	anned file as	Patches along roadway. Minor scaling alon		sediment,branche	Bottom and sides of pipe missing for approximately Fleet. Severe culvert.he rust throughout pipe. Sporadic	General tree growth. Drop inlet
B - 83i	11/13/2020 16:22 rain ewm yes yes	paved fair 23	52 yes overhead_wires no roadway.	round skewed_<45° stream_aligned no_dry_bottom yes_<25%	within_culvert s inlet_drop corrupated_metal_pipe 18 20 22 0 failing vegetation_growt within_culvertdo handiment.leaves.	Severe rust throughout. Sporadic patches of missing metal. Large	no created by a piece of plywood. failing -70.9801 42.69269 48 inch diameter tree directly after
B - 83o	11/13/2020 16:35 rain ewm yes yes	paved fair 23	Patches along roadway. Minor scaling alon, 52 no no roadway.	round skewed_<45° stream_alligned no_dry_bottom yes_25_50%		Pipe has asphalt/tar mix along	culvert, with headwall and wing wall no surrounding it poor -70.98 42.69257
						top and bottom of pipe. Moderate unifica not set to the set of the	
B - 362i	11/13/2020 16:59 rain ewm yes yes	paved satisfactory 22	50 no no Patches along roadway.	round skewed_<45° stream_alligned no_dry_bottom yes_<25%	within_culvert leaves,sediment at_stream_grade corrugated_metal_pipe 15 14.5 11 0 fair	telt side minor deterioration of culture and the side of the side	no General tree growth. fair -70.9842 42.69164
B - 362o B - 88	11/13/2020 17:11 rain ewm yes yes 11/13/2020 17:25 rain ewm culvert_was_not_located_d no	paved satisfactory 22	50 yes overhead_wires evidence_(Guard rall_failing Patches along roadway.	round skewed_<45° stream_alligned no_dry_bottom yes_50_75%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe 15 6.5 15 0 fair	Pipe has suphallytar mix at top of 5 pipe. headwall_retaining_wall_slo 3.5 poor dry_laid_stone_brick poor Embankment has moderate no	no General thorn growth. poor -70.9842 42.6918 Culvert does not exist70.985 42.69191
							2 inch HDPE drain located approximately 6 feet away from inter General two crossists Meaching!
B - 323i	11/13/2020 17:47 rain ewm yes yes	paved satisfactory 23	58 yes gas_line no Patches along roadway.	round skewed_>45° skewed_>45° no_dry_bottom yes_25_50%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe 12 6.5 12 0 fair	rate culvert 4 Severe rust throughout. headwall_retaining_wall.job 2.5 fair dry_laid_stone_brick fair Moderate erosion on emba no	inlet. General tree growth. Headwall 's approximately 1 foot in front of the pipe. fair -70.9838 42.69664

											culvert,he	Severe rust throughout pipe. 1 inch of metal on bottom right Fallen trees throughout outfull
B - 323o	11/13/2020 18:08 rain ewm yes yes	paved satisfactory	23	58 yes gas_line,overhead_wires	no	Patches along roadway.	round skewed_>45° no_discernable_stream_channel	no_moist_bottom yes_50_7	Within_culvert sediment at_stream_grade corrugated_metal_pipe	12 4 12	0 fair minor adwall	incl to literal on locusin light. The side is missing. headwall_retaining_wall_lob 3.5 fair mortared_stone_brick fair Embankment shows modelno no area. fair -70.9836 42.69683 Severe rust throughout pipe. On
B - 324i	11/13/2020 18:35 rain ewm yes yes	paved satisfactory	23	50 yes gas_line	no	Patches along roadway.q	round skewed_<45° no_discernable_stream_channel	no_dry_bottom yes_25_5	Within_culvert sediment at_stream_grade corrugated_metal_pipe	15 9 18	0 poor minor headwall	Soften right side approximately General-regetation growth. Fallen 2.5 poor motared_stone_brick failing Moderate to severe errosion on limbs throughout inlet area. poor -70.9817 42.69004 Severe not throughout pipe.
B - 324o	11/13/2020 18:55 rain ewm ves ves	payed satisfactory	22	50 yes gas line,overhead wires		Patches alone roadway.	round skewed <45° no discernable stream channel	and a base of the	downstream,with other,leaves,sedi 2% in culvert ment at stream erade corrupated metal pipe	15 8 18	0 failing minor headwall	Seeder hat motogeout ppe. Headwall has configured in front Headwall has configured in front General vegetation and tree General vegetation and tree General vegetation and tree ides plot out. Gen
B = 3240	11/12/17(7) 19"30 (mill 6mill Åe? Åe?	paved satisfactory	23	30 yes 8as_me,overneau_wires	110	racties along to auway.	Tourid skewed_445 Ho_uiscernable_scream_channel	no_dry_bottom yes_75_1	nett ar_steam_grade corrugated_inetar_pipe	13 8 18	o ranning minior meadwair	Severe rust throughout. On both right and left sides approximately
8 - 325o	11/13/2020 19:17 rain ewm yes yes	paved satisfactory	21	58 yes gas_line	no	Patches along road way.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_5	sediment,leaves, o sediment,leaves, o within_culvert ther at_stream_grade corrugated_metal_pipe	12 9 12	0 failing moderate headwall	3 feet of metal is missing, sponding patches of missing metal sponding patches of missing metal sponding patches of missing metal throughout. Headwall_retaining_wall_slo \$ poor mortaned_stone_brick fair Moderate to severe erosion no Trees growing into headwall. poor -70.9789 42.69363 throughout. Severe not throughout pipes. On Right side of headwall is actively
B - 325i B - 195	11/13/202019:32 rain ewm yes yes 11/16/202013:16 partly_cloudy ewm culvert_was_not_located_d no	paved satisfactory	21	58 yes overhead_wires,gas_line	no	Patches along roadway.	round roadway_aligned stream_aligned	no_dry_bottom yes_50_7	% within_culvert sediment at_stream_grade corrugated_metal_pipe	12 8.5 15	0 failing minor headwall	telt and right sides of pipe approximately. To a feet of 1 S metal is missing. headwall_retaining_wall_dio 5.5 poor mortared_stone_brick poor Embankment has moderation on erosion on right ade of streambed. Missing 70.5991 42.69354 Output Description of the contraction of the con
B - 190i	11/16/2020 13:25 partly_cloudy ewm yes yes	paved satisfactory	17	31 no	no	Minor to moderate cracks along roadway.	round skewed_<45° stream_aligned	flowing yes_<25%	within_culvert leaves,sediment at_stream_grade corrugated_metal_pipe	18 18 14	4 fair moderate headwall	Minor to moderate surface rust on sider of pipe. Con let side of pipe, bolts are separating along 3 side. Medwall_retaining_wall_side 3 fair dry_laid_stone_brick fair Embankment shows mode no no fair -70.9984 42.65266
												15 inch tree directly in the middle of strem. Stream drops down multiple levels after cut at bottom multiple levels after cut file.
B - 190o	11/16/2020 13:39 partly_cloudy ewm yes yes	paved satisfactory	17	31 no	no		round skewed_<45° skewed_<45°	flowing no	perched corrugated_metal_pipe	18 18 11	2 satisfactory minor headwall	and sides of pipe. General moss foam build up approximately 8 feet 6 growth at outlet opening. headwall_retaining_wall_slo 3 fair dry_laid_stone_brick fair Moderat erosion along emino no away from outlet. fair -70.9983 42.65272 invisible dog ferce located
B - 326i	11/16/2020 14:14 partly_cloudy ewm yes yes	paved satisfactory	23	41 yes other	no	Minor cracking along edges of roadway. Electrical boxes seen up streets. Minor cracking along edges of roadway. Electrical boxes seen up streets. Wooden posts	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_5	Within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	18 10.5 18	0 satisfactory none	Asphalt/far mix throughout pipe. headwall_retaining_wall_ido 2.5 satisfactory mortared_stone_brick satisfactory Embankment has minor enno no inlet. satisfactory 31.001 42.65573 Approximately 10 feet away from satisfactory 71.001 42.65573 Approximately 15 feet into pipe,
B - 326o	11/16/2020 14:27 partly_cloudy ewm yes yes	paved satisfactory	23	41 yes other	yes other poor	used as traffic safety. Certain poses or rotting and/or actively overturning.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_5	Within_culvert leaves, sediment at_stream_grade corrugated_metal_pipe	18 10 18	0 satisfactory none	there is a small dent on the top left side. headwall_retaining_wall_slo 3.5 fair mortared_stone_brick poor Minor erosion throughout no no Culvert outlets into Howes pond. fair -71.001 42.65559
												Pipe material univoxem, possibly day. At solf telegrania, geomate chips of material minoing, Scorod and this clientifies of pine are General vegetation and thorn General vegetation and thorn
B - 327o	11/16/2020 14:51 clear ewm yes yes	paved satisfactory	23	58 yes overhead_wires	no	Minor cracking along roadway/pipe length.	round roadway_aligned stream_aligned	no_dry_bottom yes_<25%	within_culvert,do other,sediment,le wnstream aves at_stream_grade other	18 16 13	0 fair minor headwall	Shifted down and to the left simple down and to the left sopproximately I inch. headwall_retaining_wall,slo 3 fair dry_laid_stone_brick poor Embankment shows moderno no basin along roadway. poor 71.005 42.6604
												Inlet is comprised of a dried lead
												stone culvert, connecting to a Culvert is made up of drined laid catch bissin which outlets out of a stone a catch basin which outlets out of a stone at catch basin and a clay pipe. day pipe some stones are falling Hele on top of culverts is allowing
B - 327i	11/16/2020 15:11 clear ewm yes yes	paved satisfactory	23	58 no	no	Minor cracking along roadway/pipe length.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_<25%	within_culvert other,leaves at_stream_grade dry_laid_stone	22 24 22	0 fair none	in within culvert. 1 foot hole at sediment and leaves to come to pof culvert approximately 15 to hole at through, and pile up within. General feet away from C.B. headwall_retaining_wall_slo 1 fair dry_laid_stone_brick satisfactory Embankment shows minor no no vegetation and they provide. fair -71.005 42.66048
												Minor surface rust throughout. 1 foot diameter true going I foot diameter true going promisely 3 feet awy from through cluster and existing to intel. Verbland filtags marked along
B - 172i	11/16/2020 15:49 clear ewm yes yes	paved satisfactory	24	47 no	no	Minor cracking along roadway.	round roadway_aligned stream_aligned	no_dry_bottom no	at_stream_grade corrugated_metal_pipe	30 30 0	0 satisfactory none	7 unknown location. headwall_retaining_wall_slo 3.5 satisfactory mortared_stone_brick satisfactory Sporadic separation of morno yes stream. satisfactory 71.0096 42.66194
												Minor surface rust throughout. Approximately 4 fee into the pipe there is an anotesta dent on the top right side. Second length of one has researed from first going the side of the side
B - 172o	11/16/2020 16:12 clear ewm yes yes	paved satisfactory	24	47 yes overhead_wires	no	Minor cracking along roadway.	round roadway_aligned stream_aligned	no_dry_bottom no	at_stream_grade corrugated_metal_pipe	30 30 0	0 fair moderate culvert	length. Half finch diameter plastic unknown. Modernate to severe unknown. Modernate to severe unknown. Modernate to severe unknown. Modernate to severe severe unknown. Modernate to severe severe unknown. Modernate to se
B - 170 B - 192i	11/16/2020 16:38 clear ewm culvert_was_not_located_d no 11/16/2020 17:26 clear ewm yes yes	paved satisfactory	24	50 yes overhead_wires	по	Minor cracking along edges of roadway.	round skewed_>45° no_discernable_stream_channel	no_dry_bottom yes_<25%	leaves, sediment, o upstream ther at_stream_grade corrugated_metal_pipe	12 9.5 12	0 satisfactory minor headwall	Minor surface rust throughout pipe. headwall_retaining_wall_slo 2.5 fair dry_laid_stone_brick fair Minor erosion of embankmno no General vegetation growth. fair -71.0037 42.65196
												Approximately 6 feet into the pipe there is a large sediment Outlet was covered in forest debris Outlet was covered in forest debris
B - 192o B - 197i	11/16/2020 17:40 clear ewm yes yes 11/16/2020 18:05 clear ewm yes yes	paved satisfactory	24	50 no 40 yes overhead wires	no	Minor cracking along edges of roadway. Minor scaling at top of roadway.	round skewed_>45° skewed_>45° round skewed <45° stream aligned	no_dry_bottom yes_25_5 no dry bottom yes 25_5		12 7 12 12 6 11	0 fair none 0 satisfactory minor culvert	6 throughout pipe. headwall_retaining_wall_slo 3 fair Minor to moderate erosion no upon arrival. fair -71.0037 42.65196 3 headwall_retaining_wall_slo 2.5 fair dry_laid_stone_brick poor Embankment hax minor to no General vegetation growth. fair -71.0018 42.64973
B - 197o	11/16/2020 18:25 clear ewm yes yes	paved satisfactory	22	40 no	no	Minor cracks along roadway.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_<25%	downstream,with in_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 11 10	0 satisfactory minor culvert	Soour underneath culvert is 2" x
												Sistes of pipe have rusted away, dropping bottom en not stream. Severe rust throughout pipe.
B - 203o	11/16/2020 18:45 clear ewm yes yes	paved good	24	80 no	yes guardrail fair	Guard rail shows minor cracking and rot.	round roadway_aligned skewed_<45°	stagnant yes_<25%	within_culvert branches at_stream_grade corrugated_metal_pipe	24 28 35	culvert,he 6.5 failing severe adwall	Sponadic patches of missing General tree and vegetation growth 7 metal. headwall_retaining_wall_slo 10 fair mortared_stone_brick poor Embankmen shows moder no throughout stream. falling -70.9958 42.64626
B - 203i	11/16/2020 19:06 clear ewm yes yes	paved good	24	80 yes overhead_wires	yes guardrail fair	Guard rail shows minor cracking and rot.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_9	sediment,leaves,b % within_culvert ranches at_stream_grade corrugated_metal_pipe	24 11 24	0 fair none	Visible oil on top of water near inter- inter- Uniformer of between structure located approximately 50 feet away Asphalt/tar mix throughout pipe. headwall_retaining_wall_ido 9 poor mortared_stone_brick poor Embankment has moderat yes yes from lieft. poor 70.9958 42.64601
												Pipe has black asphalt/har mix at bottom of pipe. Sporadic pathols of a property of the pipe of the pi
B - 208i	11/16/2020 19:30 clear ewm yes yes	paved good	25	60 no	no	Minor oil stains on roadway.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom no	inlet_drop corrugated_metal_pipe	12 12 0	0 fair moderate headwall	not throughout. headwall_retaining_wall_slo 3 satisfactory mortared_stone_brick satisfactory Embankment has minor en no inlet area. fair -70.9925 42.64332 Severe rust throughout.
												Approximately 2 feet of metal missing from right side of opening from position of the position
B - 208o	11/16/2020 19:44 clear ewm yes yes	paved good	25	60 yes overhead_wires	no	Minor oil stains on roadway.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_5	6 within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 8 12	0 failing moderate culvert	cruibed inward. Spondic pathbes of metal miking throughout. slope, steeper_than_21_ 4.5 fair Embankment shows minor no no falling -70.9924 42.6434 Moderate to sever rust on
						Moderate cracking along road way. Large crack and dips along edge of roadway. Hydrant	s					women are to severe trau, on the control of the control of t
B - 207i	11/16/2020 20:11 clear ewm yes yes	paved poor	21	45 yes water_line	no	and dips along edge of roadway. Hydrant located approximately 30 feet up street.	round roadway_aligned stream_aligned	stagnant yes_<25%	sediment,leaves,o within_culvert ther,branches at_stream_grade corrugated_metal_pipe	32 32 33	10 failing none	one not appear to be constructed wettanding octate nearry. 6 by missing bottom. headwall_retaining_wall_slo 2 failing mortared_stone_brick fair Embankment.shows.severeno no General tree and vegetand prouts. Halling -70.9817 42.64378 Wetland flass located throughout
B - 207o	11/16/2020 20:39 clear ewm yes yes	paved poor	21	45 yes overhead_wires	no	Moderate cracking along road way. Large crack and dips along edge of roadway.	ss round roadway_aligned stream_aligned	stagnant yes_<25%	sediment,leaves,b within_culvert ranches at_stream_grade corrugated_metal_pipe	32 30 35	11 fair moderate headwall	Severe rust throughout pipe. 7 Apphalt/lar mix on top of pipe. headwall_retaining_wall_ido 2.5 poor mortared_stone_brick fair Embankment shows minor no no feet away from the outlet. poor -70.8817 42.64368
B - 199o	11/17/2020 13:14 clear ewm yes yes	paved satisfactory	20	61 yes other,drainage_system	no	Minor cracking along edges of roadway. Electrical boxes seen up and down the street.	box roadway_aligned skewed_<45°	no_dry_bottom yes_<25%	downstream leaves, sediment at_stream_grade reinforced_concrete_pipe	48 36 0	0 satisfactory none	General tree growth in and around 15 Minor cracking throughout pipe. headwall_retaining_wall.slo 3 satisfactory concrete_poured_ fair General tree growth. Moderno no stream bed. satisfactory -70.9837 42.64822
B - 199i	11/17/2020 13:28 partly_cloudy ewm yes yes	paved satisfactory	20	61 yes drainage_system,other	по	Minor cracking along edges of roadway. Electrical boxes seen up and down the street.	box roadway_aligned stream_aligned	no_dry_bottom no	at_stream_grade reinforced_concrete_pipe	48 36 0	0 satisfactory none	Minor scaling at bottom of pipe. General tree growth in and around 14 Minor surface cracking, headwall retaining wall, lo 2 satisfactory concrete poured satisfactory Moderate vegetation and no stream bed. satisfactory -70.938 42.64328
B - 328i	11/17/2020 13:51 clear ewm yes yes	paved satisfactory	20	53 yes other,drainage_system	no	Minor cracking along edges of roadway. Electrical boxes seen up and down the street.	round roadway_aligned skewed_>45°	no_dry_bottom no	at_stream_grade reinforced_concrete_pipe	48 36 0	0 satisfactory none	General tree and vegetation growth 8 Minor surface cracking, headwall retaining wall,slo 1 satisfactory concrete poured satisfactory General tree growth on emno no at linkt and streambed. satisfactory -70.9855 42.64802
B - 328o	11/17/2020 14:13 clear ewm yes yes	paved satisfactory	20	53 yes other	no	Minor cracking along edges of roadway. Electrical boxes seen up and down the street.	round roadway_aligned stream_aligned	no_dry_bottom no	perched reinforced_concrete_pipe	48 36 0	0 good none	General tree growth at outlet and 15 Minor surface cracking, headwall retaining, wall, slo 2 satisfactory concrete poured fair Embankment has general to no stream bed. satisfactory -70.9852 42.64803
B - 212	11/17/2020 14:49 clear ewm culvert_was_not_located_d no											Culvert does not exist70.9906 42.64002 Minor surface rust at bottom of
B - 214i	11/17/2020 14:55 clear ewm yes yes	paved good	25	52 yes overhead_wires	no		round roadway_aligned no_discernable_stream_channel	no_dry_bottom no	at_stream_grade corrugated_metal_pipe	12 12 0	0 fair none	pipe. Approximately mildow; vinto General vegetation growth and been slightly cruched downward: headwall retaining wall, slo 1.5 satisfactory mortared_stone_brick fair Headwall has sporadic sepa no no stumps. fair -70.9897 42.63959
B - 214o B - 213o	11/17/2020 15:07 clear ewm yes yes 11/17/2020 15:23 clear ewm yes yes	paved good paved good	25 25	52 no 145 no	no no		round roadway_aligned no_discernable_stream_channel round skewed_>45° no_discernable_stream_channel	no_dry_bottom yes_25_5 no_dry_bottom no	66 within_culvert sediment at_stream_grade corrugated_metal_pipe perched hdpe	12 9 12 12 12 0	0 satisfactory none 0 satisfactory minor culvert	headwall_retaining_wall_slo 3.5 poor mortared_stone_brick fair Moderate to severe erosion o no fair - 70.8896 42.6397 slope_steeper_than_2.1_ 3 satisfactory Embankment is reinforced no no satisfactory -70.9897 42.63972 4-12 inch RCP driveway culverts
B - 213i	11/17/2020 15:46 clear ewm yes yes	paved good	25	145 yes overhead_wires	no		round skewed_>45° no_discernable_stream_channel	no_dry_bottom no	at_stream_grade hdpe	12 12 0	0 satisfactory moderate culvert	slope_steeper_than_2:1_ 3 poor Embankment shows model no outfall into pipe linket. fair -70.9902 42.63984 linket opening of pipe is partially
B - 216i	11/17/2020 16:10 clear ewm yes yes	paved satisfactory	26	57 yes overhead_wires	no	Minor cracking on edge of roadway.	round skewed_<45° no_discernable_stream_channel	no_dry_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 7.5 10	0 fair none	crushed and mangled. Minor surface not throughout pipe. headwall retaining, wall,sto 2.5 satisfactory dry_laid_stone_brick fair Headwall has 2 linch layer on no fair 70.9918 42.63807
B - 216o	11/17/2020 16:24 clear ewm yes yes	paved satisfactory	26	57 no	no	Minor cracking on edge of roadway.	round skewed_<45° no_discernable_stream_channel	no_dry_bottom complete	_obstructedownstream other corrugated_metal_pipe	12		Headwall has collapsed in front Culvert was uninjectable due to Culvert has uninjectable due to Culvert has uninjectable due to failing Moderate Erosion around on no collapse headwall. Vegetation and tree roots have If vegetation and root growth is
B - 329i	11/17/2020 16:45 clear ewm yes yes	paved poor	25	60 yes overhead_wires,gas_line	no	Moderate to severe cracking along roadway. Moderate potholes on edges of roadway.	round roadway_aligned no_discernable_stream_channel	flowing yes_50_7.	sediment,leaves,v % upstream egetation_growth_at_stream_grade corrugated_metal_pipe	12 6 8	1.5 fair minor headwall	made inlet into a drop jniet. Minor surface rust throughout surface rust throughout great integrating would go up to satisfactory. General tree and fallen pipe. headwall retaining woll, so 4 fair dry_laid_stone_brick fair Embankment shows minor no no limbs in linet area. fair -70.945 42.63501
B - 329o	11/17/2020 17:04 clear ewm yes yes	paved poor	25	60 yes overhead_wires,gas_line	no	Moderate to severe cracking along roadway.	round roadway_aligned stream_aligned	stagnant yes_25_5		12 9 12	2.5 satisfactory minor headwall	General tree growth in and around Minor surface rust throughout streambed. 7 pipe. headwall retaining, wall, slo 5.5 satisfactory mortared stone brick fair Headwall has bottom 2 fee no no streambed. satisfactory 70.9944 42.63491
						Moderate to severe cracking along roadway.						Inlet drip design could allow for Abutting neighbor mentioned that they have cleaned it out several large amounts of sediment to times over the years. General tree to
B - 330i	11/17/2020 17:20 clear ewm yes yes	paved fair	24	62 yes gas_line	no	Severe crack along pipe length.	round roadway_aligned no_discernable_stream_channel	no_dry_bottom yes_25_9	6 within_culvert other,sediment inlet_drop corrugated_metal_pipe	12 11 9	0 satisfactory minor headwall	flow into the pipe. headwall_retaining_wall,alo 2.5 satisfactory mortared_stone_brick satisfactory Sporadic missing mortar th no no growth. satisfactory '70.9935 42.63226 Minor surface rust at bottom of pipe. Lurge root at mindway point discrete former laree
B - 330	11/17/2020 17:41 clear ewm yes yes	paved fair	24	62 yes overhead_wires,gas_line	no	Moderate to severe cracking along roadway. Severe crack along pipe length.	round roadway_aligned no_discernable_stream_channel	no_moist_bottom no	perched corrugated_metal_pipe	12 12 0	culvert,he 0 satisfactory moderate adwall	of culvert. 2 inch tear on bottom growth account headwall and outfal left side of opening, headwall_retaining_wall_slo 4 satisfactory mortared_stone_brick fair Embankment has general to no area. Large stone in middle of pipe. fair -70.9934 42.63311 2 linch pine tree located
B - 226i	11/17/2020 17:59 partly_cloudy ewm yes yes	paved fair	24	62 no	no	Minor to moderate cracking along roadway.	round skewed_<45° no_discernable_stream_channel	no_dry_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	12 10 11	0 satisfactory minor headwall culvert,he	headwall_retaining_wall_side 6 satisfactory mortared_stone_brick satisfactory Embankment has general to no inlet. satisfactory -70.9892 42.63229 Minor surface nux at bottom
B - 226o	11/17/2020 18:17 partly_cloudy ewm yes yes	paved fair	24	62 yes overhead_wires	yes guardrail satisfacto	on, Minor to moderate cracking along roadway.	round skewed_<45° no_discernable_stream_channel	no_dry_bottom yes_<25%	within_culvert sediment at_stream_grade corrugated_metal_pipe	12 11 8	0 satisfactory minor adwall	Minor surface nust at bottom and top of pipe. headwall_retaining_wall_slo 9 fair mortared_stone_brick satisfactory Minor erosion and general no no General tree growth in outlet area. satisfactory -70.9896 42.63221

							General tree and thorn growth.
n 22F-	11/57/2000 19:55 mask, slowly		Electrical box seen near culvert. Moderate sewere cracking along roadway, Large depre yes other no with pooling water along of roadway.	ession	at stream grade corrugated metal pipe 12 11 6 0.25 satisfactory severe ulvert		Inlet of culvert is unknown. Inlet is either buried and/or part of closed
8 - 2250	11/17/2020 18:56 partly_cloudy ewm yes yes	paved poor 25	yes other no with pooling water along edge of roadway. Moderate to severe cracking along roadwa Large depression along edge of roadway.		at_stream_grade corrugated_metal_pipe 12 11 6 0.25 satisfactory severe ulvert	culvert. headwall_retaining_wall,slo 3.5 satisfactory mortared_stone_brick satisfactory Moderate vegetation grow no n	o drainage system. satisfactory -70.9899 42.63364 Inlet of culvert is unknown. Inlet is either buried and/or part of closed drainage system. Plinned location is
B - 225i	11/17/2020 19:16 partly_doudy ewm culvert_was_not_located_d no	paved poor 25	yes overhead_wires no pooling water. Moderate to severe cracking along roadwa	round skewed_<45° no_discernable_stream_channel completer.	tely_obstructrupstream sediment,leaves unknown corrugated_metal_pipe 12		approximate. failing -70.9899 42.63362 Wetland flags located along stream.
B - 331o	11/17/2020 19:20 partly_cloudy ewm yes yes	paved poor 25	Large depression along edge of roadway wi 400 yes overhead_wires yes Guard Raill satisfactory pooling water. Moderate to severe cracking along roadwa	round skewed_>45° stream_aligned no	at_stream_grade corrugated_metal_pipe 12 12 10 2.5 satisfactory minor Culvert	Minor surface rust throughout 4 pipe. Greater than 2 to 1. 7 fair dry_laid_stone_brick fair Embankment shows minor no n	Pipe length is approximate and was paced out. fair -70.9896 42.63446
B - 331i	11/17/2020 19:29 partly_cloudy ewm yes yes	paved poor 25	400 yes overhead_wires no pooling water.			Minor surface rust throughout II pipe. headwall_retaining_wall_slo 3 poor dry_laid_stone_brick fair Embankment has two area no n	Pipe length was approximate and was paced out. fair -70.9901 42.63335
					within_culvert.up sediment,leaves, o	Minor surface rust at bottom of pipe. Small tear at top of pipe	Headwall has large voids between stones with sediment seeping through. 18 inch tree directly in
B - 223i	11/17/2020 19:52 partly_cloudy ewm yes yes	paved fair 21	85 no no Minor to moderate cracking along roadway	y. round skewed_<45° stream_aligned flowing yes_<25	5% stream ther at_stream_grade corrugated_metal_pipe 36 34 27 1 satisfactory none	7 approximately 1 foot in. 3.5 poor dry_laid_stone_brick fair Embankment has moderate no n Small lip and pipe approximately	o front of headwall. fair -70,9898 42,63457
B - 223o	11/17/2020 20:05 partly_cloudy ewm yes yes	paved fair 21	85 yes overhead_wires yes guardrail satisfactory Minor to moderate cracking along roadway	y. round skewed_<45° stream_alligned flowing no	perched corrugated_metal_pipe 36 36 8 1 satisfactory none	20 feet inwards. Minor surface 4 rust at bottom of pipe. slope_steeper_than_2:1_ 8 fair Moderate erosion through no n	
			Minor cracking along roadway. Electrical b	oxes		Minor scaling at bottom inside the pipe. Minor moss growth at	18 inch RCP dose dralinage system outlet located directly next to culvert opening. Oil stains seen
B - 221o	11/18/2020 13:19 clear ewm yes yes	paved satisfactory 18	50 yes other, drainage_system no seen up street. Minor cracking along roadway. Electrical b	round roadway_aligned stream_aligned stagnant yes_<25	5% within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe 36 36 26 6 satisfactory severe headw	II 11 opening. headwall_retaining_wall_slo 3.5 satisfactory dry_laid_stone_brick satisfactory Embankment has minor errno y Minor scaling at bottom of pipe.	es around culvert opening. satisfactory -70.9819 42.63489
B - 221i	11/18/2020 13:38 clear ewm yes yes	paved satisfactory 18	50 yes drainage_system,other no seen up street.	round roadway_aligned stream_aligned flowing yes_<25	5% upstream leaves,other at_stream_grade reinforced_concrete_pipe 36 36 11 1 satisfactory minor headwo	Il 11 Minor moss growth at opening, headwall_retaining_wall_slo 2.5 satisfactory mortared_stone_brick satisfactory Embankment has minor enves n Moderate surface rust	D Large beaver cage in front of inlet. satisfactory -70.9821 42.63476
B - 224i	11/18/2020 13:57 clear ewm yes yes	paved satisfactory 20	Minor cracking along roadway. Electrical b 29 yes other no seen up street.	ioxes elliptical roadway_aligned stream_aligned stagnant yes_<2!	within_culvert.up headow 5% stream sedimentJeaves at_stream_grade corrugated_metal_pipe 32 24 21 1.fair moderate ulvert	throughout pipe. Approximately Il,c 6 inches into pipe on the right	Fallen trees and limbs in stream es bed. Oil stains in water. fair -70.9831 42.634
B - 224o	11/18/2020 14:12 clear ajs yes yes	named ratisfactory 20	Minor cracks along roadway. Electrical box 29 yes other no seen up street.		headw	Moderate surface rust	General thorn bush growth. Oil
5-1140	and an analysis of the second	particul advantacion y			2000 WHITE-COTTEN COURSE CONTINUES TO 20 20 20 1100 HOUSE WITH	Minor scaling at bottom of pipe.	General tree growth throughout
B - 227i	11/18/2020 14:33 clear ewm yes yes	paved satisfactory 21	Minor cracking along roadway. Electrical b 73 yes other, drainage system no seen up street.	round skewed_>45° stream_aligned flowing yes_<29	seament, eaves, o seament, e seame	minor szaing at octorion cipek. 3 General formos growth. headwali_retaining_wall_slo 1.5 satisfactory dry_laid_stone_brick satisfactory Minor erosion on embankn no n Minor scaling at bottom and sides of glope. On left side of	stream. Culvert slightly goes under pabutting driveway. satisfactory -70.9847 42.63177
			Minor cracks along roadway. Electrical box	xes	downstream,with	outlet opening there is a moderate ship with rebar	18 inch RCP outlet located directly next to culvert outlet. General tree
B - 2270	11/18/2020 14:45 clear ewm yes yes	paved satisfactory 21	73 yes other, drainage system no seen up street.	round skewed_>45° stream_aligned flowing yes_<25		Minor to moderate surface rust	o and limbs falling into stream bed. fair -70.9844 42.63178
B - 228i	11/18/2020 15:02 clear ewm yes yes	paved satisfactory 21	Minor cracks along roadway. Electrical box 40 yes other no seen up street.	xes elliptical roadway_aligned skewed_>45° stagnant yes_<2!	within_culvert.up_sediment_leaves,o 5% stream ther at_stream_grade_corrugated_metal_pipe 26 19 20 1.poor minor headw.	throughout. Approximately 5 feet II 3 in, large indent on top of pipe. headwall_retaining_wall_slo 2.5 fair dry_laid_stone_brick fair Embankment shows moderno n	General tree and vegetation poor -70.9861 42.63187
			Minor cracks along roadway. Electrical box	ves	within culvert,do sediment,leaves,o	Moderate surface must at bottom and sides of pipe. Approximately 25 feet into pipe large indent on	General trees and limbs fallen into stream bed. Electrical boxes and two steel rods directly adjacent to
B = 228o	11/18/2020 15:15 clear ewm yes yes	paved satisfactory 21	40 yes other no seen next to culvert.	elliptical roadway_aligned skewed_>45° stagnant yes_<25	5% wnstream ther,branches at_stream_grade_corrugated_metal_pipe 26 21 20 1 fair minor headw.		o head wall. poor -70.9862 42.63178
B - 229i	11/18/2020 15:55 clear ewm yes yes	paved satisfactory 21	Minor cracks along roadway. Electrical box up street.	cseen round skewed_>45° skewed_<45° no_dry_bottom yes_<25	leaves,zediment,b 5% within_culvert ranches at_stream_grade reinforced_concrete_pipe 18 13 17 0 satisfactory minor headws	bottom and sides of pipe. Minor	General vegetation and fallen tree o limbs in stream bed. fair -70.9838 42.63157
						Minor scaling at bottom and sides of pipe. Second liregit not pipe is signifyed yalfired beckwards	General vegetation growth throughout stream. Two nearby
B - 229o	11/18/2020 16:13 clear ewm yes yes	paved satisfactory 21	Minor cracking along roadway. Electrical b 33 yes other no seen up the street.	oxes round skewed_>45° skewed_<45° no_dry_bottom yes_25_	sediment,leaves, o sediment,leav	with a small amount of sediment	private drainage outlets letting out to the same stream. fair -70.9837 42.63171
B - 219	11/18/2020 16-42 clear wnb culvert was not located d no						Culvert was not found. 18 inch corrugated metal pipe found nearby in the woods, possibly dug up70.9834 42.63514
B - 2150	11/18/2020 16:42 clear wnb culvert_was_not_located_d no 11/18/2020 16:43 clear ewm yes yes	paved satisfactory 22	60 yes overhead_wires no	round no_dry_bottom yes_50_	.75% within_culvert sediment at_stream_grade ductile_iron_pipe 8 4 8 0 satisfactory none	Culvert was covered in obstructed upon arrival. slope_flatter_than_2:1_ 1.5 fair Moderate erosion. General no n	p fair -70.9791 42.63903
							Four-inch PVC private drainage outlet located approximately 10 feet away from inlet. Private
B - 215i	11/18/2020 17:06 clear ewm yes yes	paved satisfactory 22	60 yes overhead_wires no Minor cracking along roadway. Wooden guard rail placed in front of previo			slope_flatter_than_2:1_ 1.5 satisfactory Minor erosion around emb no n Minor scaling at bottom of pipe. General moss growth at first 3	p drainage outlet leads into inlet. satisfactory -70.9793 42.63904
B - 218i	11/18/2020 17:24 clear ewm yes yes	paved good 24	50 yes overhead_wires yes guardrall,b satisfactory existing concrete bollards.	round roadway_aligned stream_aligned flowing no	at_stream_grade reinforced_concrete_pipe 36 36 11 1 satisfactory none	6 feet of culvert. headwall_retaining_wall_slo 3.5 poor dry_laid_stone_brick fair Moderate Erosion through no n Minor scaling at bottom of pipe. Due to downstream obstruction	p fair -70.9801 42.63575 Folded over chain-link fence impeding down stream flow.
B - 218o	11/18/2020 17:35 clear ewm γes γes	paved good 24	Wooden guard rail placed in front of previous 50 yes overhead_wires yes guardrail satisfactor\existing concrete bollards.	ously round roadway_aligned stream_aligned flowing yes_25_	sediment,leaves, b headw 50% downstream ranches, other at_stream_grade reinforced_concrete_pipe 36 36 21 3 satisfactory_minor_ulvert	II,c water is slightly pooling within 8 culvert. headwall_retaining_wall,slo 4.5 fair dry_laid_stone_brick fair Embankment shows minor Ino n	Branches, sediment, and leaves o caught within France. fair -70.9799 42.63589 Culvert does not exist, closed
B - 232	11/18/2020 18:12 clear ewm culvert_was_not_located_d no					Severe rust at bottom and sides	drainage system70.9845 42.6291 Wetlands/survey flaes located next
B - 332i	11/18/2020 18:24 clear ewm yes yes	paved satisfactory 21	47 no yes guardrail satisfactory Guard rail ends directly before culvert.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_25_	50% within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe 12 7.5 12 0 poor none	of pipe. headwall_retaining_wall_slo 5 poor mortared_stone_brick good Embankment has moderaturo n	to culvert. General tree growth. poor -70.9891 42.62966 Utility pole and guy wire directly
B -332o	11/18/2020 18:41 clear ewm yes yes	paved satisfactory 21	47 yes overhead_wires no Minor cracking along roadway.	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_75_		Unable to inspect inside of pipe due to sediment buildup. headwall_retaining_wall_slo 5.5 poor mortared_stone_brick satisfactory Embankment has moderatino in Minor surface rust on bottom.	above and below headwall. Fallien limbs throughout outlet area. General vegetation and tree
B - 235o	11/18/2020 19:03 clear ewm yes yes	paved satisfactory 21	42 yes guardrall satisfactory Minor cracks and potholes in roadway.	round skewed_<45° stream_aligned stagnant yes_<25	5% leaves,zediment at_stream_grade corrugated_metal_pipe 24 18 23 2 satisfactory none	9 and sides of pipe. headwall_retaining_wall_slo 4.5 poor dry_laid_stone_brick poor Moderate to severe erosiono n	
						Mary Country	located approximately 20 feet away from inlet. Wetland flags located around stream. Large amount of
B - 235i	11/18/2020 19:16 clear ewm yes yes	paved satisfactory 21	42 yes overhead_wires yes guardrall satisfactor ₁ Minor cracks and potholes in roadway.	round skewed_<45° skewed_<45° stagnant yes_<29	sediment/leaves,d 5% within_culvert ebris_trash at_stream_grade corrugated_metal_pipe 24 20 22 1.5 satisfactory none	Minor surface rust at bottom 7 and sides of pipe. Cubert opening is a flared end	
B - 333i	11/18/2020 19:47 clear ewm yes yes	paved good 21	50 yes other no Electrical box seen up street.	round roadway_aligned no_discernable_stream_channel no_dry_bottom no	at_stream_grade reinforced_concrete_pipe 18 18 0 0 satisfactory minor headw	section. Moderate chip missing from concrete with rebar Il showing through. headwall_retaining_wall_slo 3 satisfactory dry_laid_stone_brick good Minor erosion on right side no n	o satisfactory -70.9866 42.62334
						Outlet is a flared end section. Moderate thorn growth	Outlet is part of a basin and is
B - 333o B - 185i	11/18/2020 19:55 clear ewm yes yes 11/19/2020 13:15 clear ewm yes yes	paved good 21 paved satisfactory 20	50 yes sewer_line,other yes guardrail good Electrical box seen up street. 41 no no	round roadway_aligned no_discernable_stream_channel no_dry_bottom yes_<25 round roadway_aligned stream_aligned no_dry_bottom yes_<25	5% within_culvert leaves at_stream_grade reinforced_concrete_pipe 18 16 40 0 satisfactory none 5% within_culvert sediment,leaves at_stream_grade corrugated_metal_pipe 12 10 0 0 good none	prevented in-depth observations. slope_steeper_than_2:1_ 2.5 satisfactory	elevated above the bottom line. satisfactory -70.9867 42.62326 General tree growth/fallen limbs in stream bed satisfactory -70.9819 42.65432
B - 185o	11/19/2020 13:25 clear ewm yes yes	paved satisfactory 20	41 no no	round roadway aligned stream aligned no dry bottom yes 25	50% within culvert sediment, leaves at stream grade corrugated metal pipe 12 7 0 0 satisfactory none	Flared end section, top right side 4 of connection is slightly bent slope_steeper_than_2:1	General tree growth along stream bank, minor erosion/cutting of banks satisfactory -70.982 42.65436
						Minor to moderate scaling to high water, second length of pipe	
						shifted off center, minor chips in end of pipe, inter and outlet connected through cash basins	
Approximately 100 fee B - 1810 off road	t 11/19/2020 13:43 clear ewm yes yes	paved fair 24	120 no no	round skewed_<45° stream_aligned no_dry_bottom yes_<25	sediment,leaves, o 5% within_culvert ther at_stream_grade reinforced_concrete_pipe 12 9.5 0 0 fair none	on roadway, appears to bend mid 12 span from inlet to CB slope_flatter_tham_2:1_ 3 satisfactory General tree and vegetation no n	fair -70.9805 42.65709 Roots of abutting tree in stream
B - 181i	11/19/2020 13:55 clear ewm yes yes			round roadway_aligned skewed >45° no_dry_bottom yes_<2	upstream,within_leaves,sediment,v 5% culvert egetation_growth_inlet_drop reinforced_concrete_pipe 12 9.5 0 0-satisfactory_none	Minor scaling at bottom of pipe, minor chips missing at inlet S opening S opening S opening Misc placed rocks around eno n	bed, abutter noted high amounts of flow to to culvert in times of heavy
8 - 1811	11/19/2020 13:55 clear ewm yes yes	paved fair 24	120 yes overhead_wires,drainage_system no Surficial asphalt spalling and longitudinal	round roadway_aligned skewed_>45° no_dry_bottom yes_<25		Minor scaling to high water, CB	
B - 175i	11/19/2020 14:13 clear ewm yes yes	paved satisfactory 23	32 yes overhead_wires,drainage_system no cracking	round skewed_<45° skewed_<45° stagnant yes_<29	within_culvert.up other,leaves,sedi 5% stream ment at_stream_grade reinforced_concrete_pipe 36 28.5 33 3 satisfactory minor headwi		Catch basin connected through core p in top of pipe satisfactory -70.9792 42.65846
						Milror to moderate chipping in culvert opening with exposed rebar, milror scaling at bottom of	Large accumulation of leaves within
B - 1750	11/19/2020 14:29 clear ewm γes γes	paved satisfactory 23	32 yes drainage_system no	round skewed_<45° stream_alligned stagnant yes_<2!	within_culvert.up 5% stream leaves,zediment at_stream_grade reinforced_concrete_pipe 36 28 33 3 satisfactory_none	pipe, first and second length of pipe shifted apart, CB cored 12 through top of pipe slope_steeper_than_2-1_ 2 fair Minor to moderate Erosion no n	downstream stream bed, fallen trees and limbs, CB cored through to top of pipe satisfactory -70.9793 42.65848
			Surficial spalling and cracking, longitudinal		branches, sedimen	Milnor scaling at bottom and sides of pipe due to water, minor to moderate chips in concrete	General tree growth throughout stream, 1ft wooden retaining wall at
B - 334i	11/19/2020 14:44 clear ewm yes yes	paved satisfactory 22	41 yes overhead_wires,drainage_system no cracking	round skewed_>45° stream_alligned no_dry_bottom yes_25_		II 6 inlet opening, headwall_retaining_wall_slo 3 failing dry_bid_stone_brick fair Severe erosion on right side no n Minor scaling at bottom and sides of jone due to water,	o top of right embankment. poor -70.9819 42.65884
					branches, leaves, ot	since of specials water, seems and third length of pipes separated, moderate chips of concrete missing from pipe	Wetland flagging, general tree
B - 334o B - 335i	11/19/2020 14:58 clear ewm yes yes 11/19/2020 15:12 clear ewm yes no	paved fair 22 paved satisfactory 23	41 yes drainage system no 61 yes overhead_wires	round skewed_>45° stream_aligned no_dry_bottom yes_25_			o growth/fallen limbs in stream failing -70.982 42.65894
B - 3350	11/19/2020 15:17 clear ewm yes no 11/19/2020 15:17 clear ewm yes yes	paved poor 23	61 no DMH mid span of culvert sunken in	round skewed_<45° stream_alligned no_dry_bottom no	tey_oostnuctecuwert ourse_coggec_conspace tauing at_stream_grade reinforced_concrete_pipe 12 12 0 0 satisfactory_minor culvert sediment.leaves.o	nesdowal retaining wall,sio 1 satisfactory mortared_stone_onck tailing Headwaln has collapsed on no n 6.5 General moss growth on pipe slope_steeper_than_2.1_ 3 poor Moderate to severe erosion no n	Moderate to severe bank cutting
B - 182i	11/19/2020 15:41 clear ewm yes yes	paved good 29	58 yes overhead_wires,drainage_system,gas no	elliptical skewed_<45° skewed_>45° no_dry_bottom yes_<25°	ther, vegetation gr	Obstructions within pipe were Il 7 washed in from streambed. headwall_retaining_wall_slo 2.5 satisfactory dry_laid_stone_brick satisfactory Headwall is part of a retain no n	satisfactory -70.9847 42.6556 General tree growth fallen limbs in
B - 182o	11/19/2020 16:01 clear ewm yes yes	paved good 29	58 yes drainage_system, gas line no	elliptical skewed_<45° stream_aligned no_dry_bottom no	at_stream_grade corrugated_metal_pipe 54 40 0 0 good moderate culvert		
B - 191i	11/19/2020 16:09 clear ewm yes yes	paved satisfactory 20		eet. open_botkroadway_aligned stream_aligned flowing no	at_stream_grade reinforced_concrete_pipe 48 18 48 2 good none	Culvert has a natural stream bed 8 as bottom. headwall_retaining_wall_slo 3.5 fair concrete_poured_ good Embankment shows minor no n	satisfactory -70.9929 42.65244
B-191o	11/19/2020 16:31 clear ewm yes yes	paved satisfactory 20	Minor cracking along roadway. Electrical b \$3 yes drainage_system,water_line,other no seen up street. Fire hydrant located up stre	oxes set. open_bottxroadway_aligned stream_aligned flowing no	at_stream_grade reinforced_concrete_pipe 48 19 48 1 good none	Bottom of culver it a natural 9 streambed. headwail_retaining_wall.slo 3 satisfactory concrete_poured_ good Minor erosion on embankn no n	General tree growth and fallen b limbs throughout stream. satisfactory -70.993 42.65241 Culvert does not exist/on private
B - 189 B - 193	11/19/2020 16:42 overcast ewm culvert_was_not_located_d no 11/19/2020 16:46 overcast ewm culvert_was_not_located_d no						property70.9898 42.65336 Culvert does not exist/on private property70.9884 42.65199

						Minor cracking along roadway. Electri	al boxes		branches, vegetati					Severe tree and vegetation growth.
B - 194i	11/19/2020 16:52 overcast ewm yes	yes	paved satisfactory	22	50 yes drainage_system,other	yes guardrail satisfactory seen up street.	box skewed_<45° no_discernable_stream_chan	nnel no_dry_bottom yes_<25%	upstream on_growth_other inlet_drop reinforced_concrete_pipe vegetation_growt	48 36 0	0 satisfactory none	Minor scaling throughout pipe. headwall_retaining_wall_slo 1.5 fair concrete_po First length of pipe is slightly	ured_ good Moderate erosion through no ne	Severe tree and vegetation growth
B - 194o	11/19/2020 17:07 overcast ewm yes	yes	paved satisfactory	22	50 yes drainage_system,other	Minor cracking along roadway. Electri no seen up street.		nnel no_dry_bottom yes_<25%	h,branches,sedim downstream ent,leaves,other at_stream_grade reinforced_concrete_pipe	48 36 0	0 satisfactory none	shifted upwards, and has a small lip. headwall_retaining_wall_slo 2.5 fair concrete_po Pipe opening is cone shaped.	ured_ satisfactory Minor to moderate erosion no ne	within an outlet area. Fallen trees no and limbs throughout outfall area. satisfactory -70.9915 42.65128
B - 174i	11/19/2020 17:41 overcast ewm yes	yes	paved good	26	75 yes overhead_wires	no	round skewed_<45° skewed_<45°	flowing no	perched pvc	34 41 14	2 satisfactory none	Yellow film seen on inside of 10 pipe. headwall_retaining_wall_slo 2.5 fair dry_laid_sto	ne_brick satisfactory Embankment shows minor no ne	no satisfactory -70.9928 42.65954
									other,branches,le			Culvert opening has a cone shape. Yellow film scene throughout pipe. Bottom of pipe		Streambed shows moderate
B - 174o	11/19/2020 17:56 overcast ewm yes	yes	paved good	26	75 yes overhead_wires	yes guardrail satisfactor, Moderate surface rust on guard rail. Electrical boxes seen up street. Mino	round skewed_<45° stream_aligned	flowing yes_<25%	downstream aves at_stream_grade pvc within culvert,do leaves,vegetation	31 36 26	5 fair minor culvert	6 is cracked and slightly shifted. headwall_retaining_wall_slo 4.5 satisfactory dry_laid_sto	ne_brick good Embankment shows minor no ne	no erosion along sides. satisfactory -70.9931 42.65951 Vegetation growth directly in front
B - 336o B - 336i	11/19/2020 18:29 partly_cloudy ewm yes 11/19/2020 18:38 clear ewm yes	yes	paved satisfactory	20	40 yes other 40 yes other	no along edge of roadway. Electrical boxes seen up street. Minor no along edge of roadway.	round roadway_aligned no_discernable_stream_char cracking round roadway_aligned no_discernable_stream_char		wnstream _growth at_stream_grade ductile_iron_pipe upstream leaves at stream grade ductile_iron_pipe	12 12 7 12 12 0	0 good moderate headwall culvert,he 0 good minor adwall	headwall retaining_wall.slo 1.5 satisfactory dry_laid_sto headwall retaining_wall.slo 1.5 satisfactory dry laid sto	-	Large accumulation of leaves
B - 337i	11/19/2020 18:46 partly_cloudy ewm yes	yes	paved satisfactory	47	58 yes other,overhead_wires	Minor cracking along roadway. Electr			upstream leaves at_stream_grade ductile_iron_pipe	12 12 0	0 satisfactory none	Minor surface cracking. headwall_retaining_wall_slo 1 satisfactory dry_laid_sto		Large accumulation of leaves at satisfactory -70.9856 42.62234 Large accumulation of leaves at
B - 337o	11/19/2020 18:55 partly_cloudy ewm yes	yes	paved satisfactory	47	58 yes overhead_wires,other	Electrical boxes seen up street. Minor no along edge of roadway.	cracking round roadway_aligned no_discernable_stream_char	nnel no_dry_bottom yes_<25%	downstream, with in_culvert sediment, leaves at_stream_grade ductile_iron_pipe	12 10.5 10	0 satisfactory minor culvert	headwall_retaining_wall.slo 1 satisfactory dry_laid_sto	ne_brick satisfactory Right side of embankment no ne	outlet. Utility pole located
B - 236i	11/19/2020 19:05 partly_cloudy ewm yes	yes	paved good	25	65 no	no	round skewed_>45° stream_aligned	stagnant yes_<25%	within_culvert sediment,leaves at_stream_grade pvc	21 21 19	4.5 satisfactory none	Top of pipe is slightly crushed from headwall. PVC pipe is cut at 5 an angle. headwall_retaining_wall_slo 4 fair concrete_po	ured_ fair Embankment shows minor no ne	General tree and vegetation growth throughout stream. fair -70.9852 42.62247
														PVC pipe was laid within existing corrugated metal pipe. General tree
B - 236o	11/19/2020 19:21 clear ewm yes	yes	paved good	25	65 yes overhead_wires	no	round skewed_>45° stream_aligned	no_moist_bottom no	at_stream_grade pvc	22 22 0	0 satisfactory minor culvert	PVC pipe was laid within prior 6 existing corrugated metal culvert. headwall_retaining_wall_slo	ured_ fair Minor to moderate erosion no no	and vegetation growth. Dry laid stone retaining wall approximately no 15 feet away from outlet. fair -70.9852 42.62266
B - 231	11/19/2020 19:55 partly_cloudy wnb culvert_was_not_lo	cated_d no										Pipe appears to have a rubber		Culvert does not exist70.975 42.62923
						Minor patches long roadway. Guard r moderate surface rust. Some posts are						coating throughout and on outside of pipe. Due to high water depth, visual inspection		inlet and outlet area are covered with marshland. Inner portion of culvert was unexpectable due to
B - 29o	11/20/2020 13:15 overcast ewm yes	no	paved satisfactory	25	35 yes overhead_wires	yes guardrail fair overturning.	round roadway_aligned no_discernable_stream_chan	nnel stagnant yes_<25%	within_culvert at_stream_grade other	60 58.5 60	48 fair	within pipe is inaccessible. headwall_retaining_wall_slo 2.5 fair dry_laid_sto Outside and inside of pipe has a	ne_brick fair Embankment shows minor yes no	no highwater. fair -71.0478 42.71275 Inlet and outlet area are comprised of marshes. Inside of culvert was
n 20i	11/20/2020 13:32 overcast ewm yes			25	35 no	Minor patches along roadway. Moder yes guardrail fair surface rust on guard rail.	te round roadway aligned no discernable stream char		at stream grade other	60 60 60	49	outside of pipe are inaccessible due to high water. headwall retaining wall, slo 3 poor mortared st	one brick fair Embankment shows moderyes no	unexpectable due to high water. Water level is quite high for time of
D · 251	11/20/2020 15:52 OVEICEST EWIT YES	110	paved satisfactory	23	33 110			iner stagnant	at_stream_grade office	60 60 65	42	Inlet of culvert is inaccessible in	one_brick fail Elibankinen silows hoberyes in	Oil stains seen within inlet area.
B - 48i	11/20/2020 14:02 overcast ewm yes	no	paved poor	15	23 no	Moderate to severe cracking along ce roadway. Large dips and depressions v yes guardrail satisfactor, roadway.		nnel				non-visible due to vegetation overgrowth, high water and steep embankment. headwall_retaining_wall_slope_steeper_t fair dry_laid_sto	ne_brick Severe vegetation overgrovyes ye	Abutting neighbor mentioned that culvert was placed between 20 to yes 30 years ago71.0662 42.70452
												Minor to moderate scaling at bottom of pipe due to water.		
												Bottom lip of culvert is missing at entrance. Exposed rebar on outside lip of pipe. Beaver grate		
B - 45i	11/20/2020 14:49 overcast ewm yes	yes	paved satisfactory	28	66 yes water_line	Moderate cracking along roadway. Gu yes guardrail fair has portions with severe rust and smal	rd rall bends. round skewed_>45° no_discernable_stream_char	nnel no_moist_bottom yes_25_50%	within_culvert,up sediment,leaves,b stream ranches at_stream_grade reinforced_concrete_pipe	42 37 45	0 satisfactory none	placed in front of culvert, with large pile of sediment behind it. headwall_retaining_wall_slo 2.5 failing dry_laid_sto	ne_brick poor Embankment shows severe yes no	Fire hydrant located directly next to no and above culvert. poor -71.0696 42.7067 Large area of oil and sediment
														buildup at outlet. RCP close drainage system outlet located approximately 10 feet away from
R - 450	11/20/2020 15:10 overcast ewm yes	wes.	paved satisfactory	28	66 ves overhead wires drainage system	Moderate cracking along roadway. Gu shows large bends and portions of the yes guardrail failing are actively overturned.		no_dry_bottom no	perched reinforced_concrete_pipe	42 42 0	0 fair minor culvert	Moderate scaling at bottom of pippe. In middle length of pipe 10 water is pooling. headwall_retaining_wall_sio 2.5 poor dry_laid_sto	ne brick fair Embankment shows moderyes ye	outlet. Utility pole located approximately 15 feet away from yes outlet. fair -71.0696 42.7065
		,	,	-		Minor crack along roadway. Mortar at walls along edges of roadway. Closed I and electrical boxes seen up street. Fir	d stone rainage					Minor chipping and scaling on		General tree growth and fallen
B - 46i	11/20/2020 15:35 overcast ewm yes	yes	paved good	21	30 yes Closed Drainage, Other, water lin		round roadway_aligned no_discernable_stream_char distone	nnel no_dry_bottom yes_<25%	upstream other,leaves inlet_drop reinforced_concrete_pipe	24 24 0	0 good minor culvert	outside of cubert. headwall_retaining_wall 2.5 satisfactory dry_laid_sto Minor scaling at bottom of pipe.	ne_brick fair Headwall has sporadic patc no ne	
B - 460	11/20/2020 15:49 overcast ewm yes	yes	paved good	21	30 yes Closed Drainage, other, water line	waiis along edges of roadway. Closed i and electrical boxes seen up street. Fir e yes guardrail satisfactory seen up street	annage hydrant round roadway_aligned no_discernable_stream_char	nnel no_dry_bottom no	at_stream_grade reinforced_concrete_pipe	24 24 0	0 good minor culvert	Millior scaling at contorn or pipe. Small chips of concrete missing from outlet opening. headwall_retaining_wall 3 satisfactory dry_laid_sto	ne_brick fair Headwall has voids betwee no no	General tree and vegetation growth. Fallen trees and limbs. fair -71.07 42.70519
B - 20	11/20/2020 16:21 overcast wnb culvert_was_not_lo													Potential culvert located in North Andover71.0927 42.71922 Potential culvert was located in
B - 75	11/20/2020 16:41 overcast wnb culvert_was_not_lo	cated_d no							within_culvert,up_leaves,sediment,o			Moderate to severe scaling		North Andover -71.0665 42.69481 Fallen trees, tree limbs, and vegetation growth throughout
B - 338o	11/20/2020 17:05 overcast ewm yes	yes	paved good	27	yes overhead_wires	no	round skewed_>45° stream_aligned	flowing yes_<25%	stream ther at_stream_grade reinforced_concrete_pipe vegetation_growt	24 20 20	1 fair none	8 throughout pipe. headwall_retaining_wall_slo 4 poor dry_laid_sto	ne_brick poor Embankment has moderati no ni	no streambed. poor -71.0811 42.71138 Inlet cannot be found likely buried
B - 338i	11/20/2020 17:33 overcast ewm yes	no	paved good	27	no	ves guardrail satisfactory								no or collapsed71.0813 42.71137
						, 6,	round skewed_>45° no_discernable_stream_char	nnel completely_o	h,leaves,sediment buried_clogged_col reinforced_concrete_pipe	24		slope_steeper_than_2:1_ fair	Large amounts of vegetatic no ne	
						,	round skewed_>45* no_discernable_stream_chai	nnel completely_o		24			Large amounts of vegetatic no na	Culvert outlet is located approximately 10 feet away from 24 inch RCP culvert. Culvert likely
B - 339i	11/20/2020 17-40 overcast ewm yes	yes	paved good	27	yes overhead_wires	no		nnel no_dry_bottom yes_25_50%	sediment,leaves, b upstream ranches at_stream_grade dry_laid_stone	20 9.5 20	0 fair none	slope_steeper_than_2:1_ tair Natural bottom sediment increases with culent length. headwall_retaining_wall.slo 4 fair dry_laid_sto		Culvert outlet is located approximately 10 feet away from 24 inch RP cyclent. Culvert likely and control of the
B - 339i B - 339o	11/20/2020 17-40 overcast ewm yes 11/20/2020 17-55 overcast ewm yes	yes	paved good	27 27	yes overhead_wires	no yes guardrail satisfactory		nnel no_dry_bottom yes_25_50%	sediment, leaves, b	20 9.5 20 20	0 fair none	Natural bottom sediment		Culvert outlet is located approximately 10 feet every from 24 approximately 10 feet approximately 10 feet every from 10 feet every from 10 feet for every from 10 feet
B - 339i B - 339o		yes no	paved good	27 27	yes overhead_wires		open_botts:kewed_>45* no_discernable_stream_char open_botts:kewed_>45* no_discernable_stream_char	nnel no_dry_bottom yes_25_50%	sediment,lewes, b upstream ranches at_stream_grade_dry_laid_stone sediment,branche	24 20 9.5 20 20	0 fair none	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2:1_ fair First 2 feet of pipes bottom is routed away and missing.	ne_brick fair Embankment shows minor no ni	Culvert outlet is located approximately 10 feet away from 24 inch RIC outlet Culvert Culvert likely abandoned and replaced with nesety 24 inch RIC. 4 fair 71.0811 42.71143 inlet of culvert cannot be found. 10 likely burnier, diagogad, or collapsed. 12 link corrupted metal drivwawy culvert outlet located approximately 8 feet away from injet.
8 - 3390 8 - 3390 8 - 3401		yes no yes	paved good paved good paved fair	27 27 26	yes overhead_wires no	Moderate cracks along center and ed roadway. Guard rall above culvert is in	open_botts:kewed_>45° no_discernable_stream_char open_botts:kewed_>45° no_discernable_stream_char	nnel no_dry_bottom yez_25_50% nnel completely_o	sediment,lewes, b upstream ranches at_stream_grade_dry_laid_stone sediment,branche	20 9.5 20 20 18 18 0	0 fair none Culvert,he 0 failing moderate advail	Natural bottom sediment increases with cultert length. headwall_retaining_wall.slo 4 fair dry_laid_sto slope_steeper_tham_2:1_ fair First_2 feet of gipes bottom is. Fermulated from the company of the	ne_brick fair Embankment shows minor no ni	Culvert outlet is located approximately 10 feet even from 24 approximately 16 feet away there is a stone retaining wall in front of
8 - 3390 8 - 3390 8 - 340i	11/70/2020 17:55 overcast ewm yes	yes no yes	•	27 27 26	no	Modernate cracks along center and ed- readway, Coast of all above culters in high yes guardrall failing nuted and actively overtured and Moderate crack large edges and em- readway, Coast of all is beauty of years.	open_botts:kewed_>45° no_discernable_stream_char open_botts:kewed_>45° no_discernable_stream_char ss of profits:compared_stream_char profits:compared_stream_char ord_stream_c	nnel no_dry_bottom yez_25_50% nnel completely_o	sediment,leaves,b upstream ranches at_stream_grade dry_laid_stone sediment.branche structerupstream s,leaves buried_clogged_col dry_laid_stone			Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair drv_laid_sto slope_steeper_than_2-1_ fair Fixt 2 feet of pipes bottom is rusted away and missing. Removate of application are increased away and missing. Removate of application are increased away and missing. Approximately 10 feet into pipe. Approximately 10 feet into pipe. Approximately 10 feet into pipe. Beadwall_retaining_wall_slo 2.5 poor dry_laid_sto paired rail post has power all the post and the pipe.	ne_brick fair Embankment shows minor no ne Moderate vegetation and t no ne	Culvert outlet is located approximately 10 feet away from 24 sich KIR Culvert Culvert fillerly sich control of the control of
B - 3390 B - 3390 B - 3400 B - 3400	11/70/2020 17:55 overcast ewm yes	yes no yes	•	27 27 26	no	Moderate crasks along center and edi radeway, Goard all above calvert in li yes guardrail failing rusted and actively overturned with la Moderate crasks ling edges and cent	open_botts:kewed_>45° no_discernable_stream_char open_botts:kewed_>45° no_discernable_stream_char ss of profits:compared_stream_char profits:compared_stream_char ord_stream_c	nnel no_dry_bottom yez_25_50% nnel completely_o	sediment,leaves,b upstream ranches at_stream_grade dry_laid_stone sediment.branche structerupstream s,leaves buried_clogged_col dry_laid_stone			Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2:1_ fair slope_steeper_than_2:1_ fair First 2 feet of pipes bottom is nuterial away and missing. Advantage away and missing the slope	ne_brick fair Embankment shows minor no ne Moderate vegetation and t no ne	Culvert outlet is located approximately 10 feet away from 24 inch RIO Culvert. Culvert likely abundanced and replaced with nearby 2 leich RIO. The culture of the culture o
8 - 3390 8 - 3390 8 - 3400 8 - 3400	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes	yes no yes	paved fair	27 27 26 26	no 50 no	Moderate cracks along center and edi radelmay, Guaderi all above cultert in yes guardrail failing unsted and actively overturned with is Moderate cracks long edges and cent roadway, Guardrail is heavily rusted cracks introplopat the center for tallen off posts and posts are actively yes guardrail failing overturning.	open_botts:kewed_345* no_discernable_stream_char open_botts:kewed_345* no_discernable_stream_char ss of why bends round skewed_445* no_discernable_stream_char of si hi kegs round roadway_siligned stream_siligned way. Gas	nnel no_dry_bottom yes_25_50% completely_a nnel no_moist_bottom no no_moist_bottom no	sediment leaves, b upstream ranches at _stream_grade dry_laid_stone sediment branche structrupstream s,leaves buried_clogged_col dry_laid_stone at_stream_grade corrugated_metal_pipe	18 18 0	0 failing moderate adwall	Natural bottom sediment increases with cultert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_tham_2:1_ fair First 2 feet of pipes bottom is sediment increases with culter length. The sediment is supported by the sediment of supposition to the feet of pipes bottom in the feet of pipes. See result at the bottom of pipe. See result at the bottom of pipe. Appositionally 10 feet into pipe, guard rail post has given at the way through pipe. Remainst of application of pipe. Severe nat at short of pipe. Severe nat at short on of pipe. The second of pipes is the second of pipes and the second of pipes is the second of pipes. The second of pipes is the second of pipes is the second of pipes in the second of pipes is the second of pipes in the second of pip	ne_brick fair Embankment shows minor no ni Moderate vegetation and t no ni me_brick poor Embankment shows moderno ni	Culvert outlet is located approximately 10 feet every from 74 approximately 10 feet every from
8 - 3390 8 - 3390 8 - 3400 8 - 3400 8 - 3411	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes	yes yes yes	paved fair	27 27 26 26 28	no 50 no	Moderate cracks along center and edi- readway. Coaser of all above culvent in the readway. Coaser of all above culvent in the Moderate cracks long edges and cent roadway. Coaser of all is beauty roated or Moderate cracks long edges and cent roadway. Coaser of all is beauty roated or fallen of posts and posts and yes guardrall failing Moter to moderate cracking slong can less craces roadway slong callent for yes guardrall fail rail shows sever roats with misor burn	open_botts.kewed_345* no_discernable_stream_char open_botts.kewed_345* no_discernable_stream_char ss of open_botts.kewed_345* no_discernable_stream_char open_botts.comd_skewed_445* no_discernable_stream_char open_botts.comd_skewed_445* no_discernable_stream_char open_botts.kewed_445* no_discernable_stream_char	nnel no_dry_bottom yes_25_50% completely_a nnel no_moist_bottom no no_moist_bottom no	sediment leaves, b upstream ranches at _stream_grade dry_laid_stone sediment branche structrupstream s,leaves buried_clogged_col dry_laid_stone at_stream_grade corrugated_metal_pipe	18 18 0	0 failing moderate adwall	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_tham_2.1_ fair slope_steeper_tham_2.1_ fair slope_steeper_tham_2.1_ fair stated away and mining_Remounts of asphalt/lar mining_steeper_steeper_tham_2.1_ fair stated away and mining_Remounts of asphalt/lar mining_wall_slope_steeper_tham_2.1_ fair slope_steeper_tham_2.1_ fair s	ne_brick fair Embankment shows minor no ni Moderate vegetation and t no ni me_brick poor Embankment shows moderno ni	Culvern outlet is located approximately 10 feet evany from 24 approximately 10 feet away there is a stone retraining wall in frost of culvert. Fallen trees and limbs throughout streembed. Westign 8 green in streammers. 24 inch ductile iron outlet located approximately 10 feet away from in streammers. 25 inch correct proximately 10 feet away there is a stone retraining wall in frost of failing. 71.0782 42.7091 26 inch ductile iron outlet located approximately 10 feet away from incl. Westign 10 feet away from incl. We
B - 3390 B - 3400 B - 3400 B - 3411 B - 3410	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes	yes yes yes yes	paved fair	27 27 26 26 28	50 no 50 yes overhead_wires	Moderate cracks along center and edi radelmay, Guaderi all above cultert in yes guardrail failing unsted and actively overturned with is Moderate cracks long edges and cent roadway, Guardrail is heavily rusted cracks introplopat the center for tallen off posts and posts are actively yes guardrail failing overturning.	open_botts:kewed_345* no_discernable_stream_char open_botts:kewed_345* no_discernable_stream_char ss of why why bonds round skewed_445* no_discernable_stream_char of sit hisgs round roadway_siligned stream_siligned way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas Gas Gard	nnel no_dry_bottom yez_25_50% completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no	sediment_leaves_b upstream ranches at_stream_grade dry_laid_stone sediment_branche structrupstream s.leaves buried_clogged_col dry_laid_stone at_stream_grade corrugated_metal_pipe perched corrugated_metal_pipe	18 18 0	0 failing moderate adwall 0 failing none	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair drv_laid_sto slope_steeper_than_2-1_ fair slope_steeper_than_2-1_ fair Fixt 2 feet of pipes bottom is rusted away and missing. Rememate to apposition from its rusted away and missing. Rememate to apposition from throughout pipe. Severe must be bottom of pipe is bedwall_retaining_wall_slo 2.5 poor dry_laid_sto between the apposition from the pipe all the way through pipe. Rememate of apposition from throughout pipe. Severe must are bottom of pipe is not fair to pipe in the dwall_retaining_wall_slo 2 fair mortaned_st first. I feet of bottom of pipe is noted away and missing.	he_brick fair Embankment shows minor no ni Moderate vegetation and t no ni be_brick poor Embankment shows moderno ni one_brick fair Minor erosion on embanknno ni	Culvers outlet is located approximately 18 feet away from 24 ton NR 70-20 event Culvers flish and ton NR 70-20 event Culvers flish and ton NR 70-20 event Culvers flish and ton less than 18 event Culvers flish and ton less than 18 event Culvers flish and the NR 71-0811 42.71145 event Culvers flish and the NR 71-0814 42.71145 event flish and
8 - 3390 8 - 3390 8 - 3401 8 - 3410 8 - 3410	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:26 overcast ewm yes	no yes yes yes yes	paved fair paved fair paved satisfactory	27 27 26 26 28	50 no 50 yes overhead_wires yes overhead_wires,gss_line	Moderate cracks along center and ed readway. Guard rail allong collect in he readway. Guard rail allong collect in he readway. Guard rail allong collect in he moderate cracks long edges and center roadway. Guard rail is heavily rusted or cracks from comploute the center. In the control of the cracks and posts are actively yet guardrail failing eventually. Minor to moderate cracking along road line crosses roadway solong culent length of the control of	open_botts:kewed_345* no_discernable_stream_char open_botts:kewed_345* no_discernable_stream_char ss of why why bonds round skewed_445* no_discernable_stream_char of sit hisgs round roadway_siligned stream_siligned way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas Gas Gard	nnel no_dry_bottom yez_25_50% completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no	sediment_leves_b ranches at_stream_grade dev_laid_stone sediment_branche structs upstream s_tears: buried_clagged_col dev_laid_stone at_stream_grade corrugated_metal_pipe perched corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade corrugated_metal_pipe vegetation_growt	18 18 0	0 failing moderate adwall 0 failing none	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2.1_ fair	he_brick fair Embankment shows minor no ni Moderate vegetation and t no ni be_brick poor Embankment shows moderno ni one_brick fair Minor erosion on embanknno ni	Culvert outlet is located approximately 10 feet every from 74 approximately 10 feet every from 10 feet outlet for
8 - 3390 8 - 3390 8 - 3400 8 - 3410 8 - 3410	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:26 overcast ewm yes	yes no yes yes yes yes	paved fair paved fair paved satisfactory	27 27 26 26 28	50 no 50 yes overhead_wires yes overhead_wires,gss_line	Moderate cracks along center and ed readway. Guard rail allong collect in he readway. Guard rail allong collect in he readway. Guard rail allong collect in he moderate cracks long edges and center roadway. Guard rail is heavily rusted or cracks from comploute the center. In the control of the cracks and posts are actively yet guardrail failing eventually. Minor to moderate cracking along road line crosses roadway solong culent length of the control of	open_botts:kewed_345* no_discernable_stream_char open_botts:kewed_345* no_discernable_stream_char ss of why why bonds round skewed_445* no_discernable_stream_char of sit hisgs round roadway_siligned stream_siligned way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas Gas Gard	nnel no_dry_bottom yez_25_50% completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no	sediment_leves_b ranches at_stream_grade dev_laid_stone sediment_branche structs upstream s_tears: buried_clagged_col dev_laid_stone at_stream_grade corrugated_metal_pipe perched corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade corrugated_metal_pipe vegetation_growt	18 18 0	0 failing moderate adwall 0 failing none	Natural bottom sediment increases with cultert length. slope_steeper_than_2:1_ fair First 2 feet of pipes bottom is consistent of the co	he_brick fair Embankment shows minor no ni Moderate vegetation and t no ni be_brick poor Embankment shows moderno ni one_brick fair Minor erosion on embanknno ni	Culvert outlet is located approximately 10 feet every from 74 approximately 10 feet every from 10 feet outlet for
8 - 3390 8 - 3390 8 - 3400 8 - 3410 8 - 3410	11/20/2020 17:55 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:26 overcast ewm yes	yes no yes yes yes yes	paved fair paved fair paved satisfactory	27 27 26 26 28 28	50 no 50 yes overhead_wires yes overhead_wires,gss_line	Moderate cracks along center and ed readway. Guard rail allong collect in he readway. Guard rail allong collect in he readway. Guard rail allong collect in he moderate cracks long edges and center roadway. Guard rail is heavily rusted or cracks from comploute the center. In the control of the cracks and posts are actively yet guardrail failing eventually. Minor to moderate cracking along road line crosses roadway solong culent length of the control of	open_botts:kewed_345* no_discernable_stream_char open_botts:kewed_345* no_discernable_stream_char ss of why why bonds round skewed_445* no_discernable_stream_char of sit hisgs round roadway_siligned stream_siligned way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas no_discernable_stream_char way. Gas Gas Gard	nnel no_dry_bottom vez_25_50% nnel completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_moist_bottom no no_moist_bottom no	sediment_leves_b ranches at_stream_grade dev_laid_stone sediment_branche structs upstream s_tears: buried_clagged_col dev_laid_stone at_stream_grade corrugated_metal_pipe perched corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade corrugated_metal_pipe vegetation_growt	18 18 0	0 failing moderate adwall 0 failing none	Natural bottom sediment increases with cultert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_tham_2:1_ fair	he_brick fair Embankment shows minor no ni Moderate vegetation and t no ni be_brick poor Embankment shows moderno ni one_brick fair Minor erosion on embanknno ni	Culvert outlet is located approximately 10 feet even from 74 approximately 10 feet even from 10 failing 10 feet for 10
8 - 3390 8 - 3390 8 - 3400 8 - 3410 8 - 3410	11/20/2020 18:06 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:02 overcast ewm yes 11/20/2020 18:02 overcast ewm yes	yes no yes yes no	paved fair paved fair paved satisfactory	27 27 26 26 28 28	50 no 50 yes overhead_wires yes overhead_wires,gas_line yes overhead_wires,gas_line	Moderate cracks along center and edi- yes guardrall failing mated and actively overtured with Is Medieste crack long edges and ener- rossens, could not ill seek produced with Is Medieste crack long edges and ener- rossens, could not ill seek pedges and ener- rossens could not include the center. Guard or failing of posts and posts are actively overturating. Minner to moderate cracking along ros line crosses rosdewly along collent feet rall shows severe rostedy along collent feet all failing values severe rosdewly along pile leggli yes guardrall failing values feet of posts and is severely as have failed out of the country	open_botts.kewed_345* no_discernable_stream_char open_botts.kewed_345* no_discernable_stream_char six of open_botts.kewed_345* no_discernable_stream_char open_botts.kewed_445* no_disce	nnel no_dry_bottom vez_25_50% nnel completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_moist_bottom no no_moist_bottom no	sediment.leaves.b ranches at_stream_grade dvy_laid_stone sediment.branche structerupstream st_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade corrugated_metal_pipe corrugated_metal_pipe vegetation_growt h corrugated_metal_pipe	18 18 0 18 18 0 18 17 0	O falling moderate adwall O falling none Culvert.he O falling moderate adwall	Natural bottom sediment increases with culvert length. slope_steeper_than_2:1_ fair First 2 feet of pipes bottom is considered away and missing. First 2 feet of pipes bottom is considered away and missing. First 2 feet of pipes bottom is considered away and missing. Approximately 10 feet into pipe. Approximately 10 feet into pipe. Approximately 10 feet into pipe. Severe nust storoug/host pipe. First 2 feet of bottom of pipe. First 2 feet of bottom of pipe. First 2 feet of bottom of pipe. Could do set broughout pipe. First 2 feet of bottom of pipe. Could do set broughout pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 2 feet of pipe has bottom of pipe. First 3 feet of pipe has bottom of pipe in missing. First 2 feet of pipe has bottom of pipe. First 3 feet of pipe has bottom of pipe in missing. First 3 feet of pipe has bottom of pipe. First 3 feet of pipe has bottom of pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 3 feet of pipe has bottom of pipe from pipe. First 4 feet of pipe has bottom of pipe from pipe. First 5 feet of pipe has bottom of pipe from pipe. First 6 feet of pipe has bottom of pipe from pipe. First 6 feet of pipe has bottom of pipe from pipe. First 6 feet of pipe has bottom of pipe from pipe. First 6 feet of pipe has bottom of pipe from pipe. First 6 feet of pipe has bottom of pipe from pipe. First 7 feet of pipe has bottom of pipe from pipe. First	he_brick fair Embankment shows minor no no Moderate vegetation and tino no Moderate vegetation and tino no he_brick poor Embankment shows moderno no me_brick fair Minor erosion on embanknino no me_brick fair Embankment shows minor no no	Culvert outlet is located approximately 10 feet even from 24 approximately 10 feet even from 12 feet for 10 feet for
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B - 342o B - 67o B - 671	11/20/2020 18:06 overcast ewm yes 11/20/2020 18:02 overcast ewm yes 11/20/2020 19:07 overcast ewm yes	yes no yes yes no yes no yes	paved fair paved fair paved satisfactory paved good paved good paved good	26	50 no 50 yes overhead_wires yes overhead_wires,gas_line yes overhead_wires,gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires	Moderate cracks along center and ed roadway. Coard rist allower culerts in a comment of the coard readway coard rist allower culerts in a coard rist in Seward and a coard rist in Seward readway in the center. Coard rist in Seward risk risk risk risk in Seward risk risk risk risk risk risk r	open_botts:kewed_45' no_discernable_stream_char open_botts:kewed_45' no_discernable_stream_char sis of open_botts:kewed_45' no_discernable_stream_char open_botts:comed_stewed_445' no_discernable_stream_char tourd roadway_aligned stream_stigned way. Gas no_discernable_stream_char round skewed_45' no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char ts of round roadway_aligned no_discernable_stream_char copen_botts:roadway_aligned no_discernable_stream_char open_botts:roadway_aligned no_discernable_stream_char ts een open_botts:kewed_445' skewed_445' skewed_445'	nnel no_dry_bottom ves_25_50% nnel completely_o nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel flowing no	sediment_leves_b tranches at_stream_grade dny_laid_stone sediment_branche sediment_branche sediment_branche sediment_branche sediment_branche sediment_branche sediment_branche sediment_branche sediment_branche at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe st_stream_grade dny_laid_stone st_stream_grade dny_laid_stone st_stream_grade dny_laid_stone within_colvert avel_ment_le seq. st_stream_grade dny_laid_stone	18 18 0 18 18 0 18 17 0 18 24 26 0 24 24 0 40 56 40 40 47 40	O failing moderate adwall O failing none Culvert.he O failing moderate adwall O failing minor uvert O failing moderate adwall. 27 fair 36	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2:1_ fair	be_brick fair Embankment shows minor no no material shows minor no no material shows minor no no material shows moderno no material shows moderno no material shows moderno no material shows minor no no material shows moderno no material shows mode	Culvert outlet is located approximately 10 feet even from 24 approximately 10 feet even from 10 likely burnet, diagogal, or collapsed. 12 linch corrupted metal divieway culvert outlet located approximately is feet away from linch sa stone retaining wall in front of culvert. Fallen trees and limbs throughout streembed. Wetland flag seen in streem area. In the culvert is a stone retaining wall in front of culvert. Fallen trees and limbs throughout streembed. Wetland flag seen in streem area. 24 linch ductile tone and let located approximately 10 feet away from linch. Wetland flag seen in streem area. 25 linch ductile located between linet and outlet. In the located between linet are ductile. Wetland flag seen in the streem area. 26 linch ductile located between linet are ductile. Wetland flag seen in the streem area. 27 linch ductile located between linet are ductile. Wetland flag seen in the streem area. 28 linch ductile located between linet are ductile. Wetland flag seen in the streem area. 29 people with the street seen and then growth, preventing access for impection. 20 linch street in the street seen and limbs. 20 people with street seen and limbs. 21 linch course in street seen and limbs. 22 linch ductile seen and fallen tree and limbs. Throughout countil area is a pond while ducted in a marsh. power dark located approximately 20 feet away from linck. In the area is a pond while ducted in a linch seen and limbs. 22 linch ducted in linch seen ducted on with a span of 35 feet. Gas linch seen ducted on the street. Wetland linch tree sided look with a span of 35 feet. Gas linch seen ducted on and cutting of the lashes along a streem. 23 linch ducted in ship water depth. power cannot cutted of the lashes along a streem.
8 - 3420 8 - 670 8 - 671 8 - 660	11/20/2020 18:26 overcast ewm yes 11/20/2020 19:27 overcast ewm yes 11/20/2020 19:21 overcast ewm yes 11/20/2020 13:04 rain ewm yes	no no yes no no yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved satisfactory	26 20 20 22	no 50 no 50 yes overhead_wires yes overhead_wires.gas_line yes overhead_wires.gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires 30 no 34 yes gas_line.other	Moderate cracks along center and ed yes guardrall failing nuclear actively overturned with Is Moderate cracks along center and ed roadway, Coast of all selevation with a Moderate crack and actively overturned with Is white actively overturned with Is for a guardrall failing control of posts and posts are actively yes guardrall failing control of posts and posts are actively yes guardrall failing control of Minor to moderate cracking along road yes guardrall failing rail shows severe roadway slong pipe length yes guardrall failing rail shas failen off posts and of severely yes guardrall satisfaction Minor cracking along edge of roadway. Minor cracking and dips along roadway yes guardrall fail Minor cracks and dip on roadway. Electrical box seen up street. Fire byd peter satisfaction, to treet approximately 100 feet away yes other satisfaction, to treet approximately 100 feet away peter satisfaction, to treet approximately 100 feet away the satisfaction of satisfaction of the satis	open_botts:kewed_45' no_discernable_stream_char open_botts:kewed_45' no_discernable_stream_char sis of open_botts:kewed_45' no_discernable_stream_char open_botts:comed_stewed_445' no_discernable_stream_char tourd roadway_aligned stream_stigned way. 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Rememate of black spills/lar missing wall, slo Discondition of pipe in susted away and missing. Rememate of black spills/lar missing wall, slo Discondition of pipe in susted away and missing. Water depth inside of culver the color of pipe. Water depth inside of culver the color of pipe. Bottom side of culvert. Water depth inside of culvert in a three lated bout in a resoluble headwall_retaining_wall, slo Discondition of concrete due to Discondi	ne_brick fair Embankment shows minor no no material designation and too not be brick fair Embankment shows minor no no material designation and too not be brick fair Embankment shows model no not be brick failing Severe erosion around ent no not be brick failing Severe erosion around ent no not be brick failing Severe erosion around ent no not be brick failing Severe erosion around ent no not be brick good Embankment shows minor no not not be brick good Embankment shows minor no not not be brick good Embankment shows minor no not not not be brick good Embankment shows minor no not not not be brick good Embankment shows minor no not not not not be brick good Embankment shows minor no not not not not not not not not not	Culvert outlet is located approximately 10 feet every from 74 approximately 10 feet every from 164 approximately 10 feet every from 164 approximately 12 inch corrupted metal driveway culvert outlet board approximately 16 feet every from 164 as a store retaining wall in front of 164 approximately 16 feet every from 164 as a store retaining wall in front of 164 approximately 164 approximat
8 - 3420 8 - 670 8 - 671 8 - 660 8 - 661 8 - 501 8 - 500	11/20/2020 18:06 overcast ewm yes 11/20/2020 18:02 overcast ewm yes 11/20/2020 18:02 overcast ewm yes 11/20/2020 19:07 overcast ewm yes 11/20/2020 13:04 rain ewm yes	no no yes no yes no no yes yes yes yes yes yes yes yes yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved satisfactory paved good paved good paved good paved good paved good paved satisfactory paved good paved good paved good paved satisfactory paved satisfactory	26 20 20 22 22 25 25	50 no 50 yes overhead_wires yes overhead_wires_gas_line yes overhead_wires_gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires 30 no 34 yes gas_line_other 34 yes gas_line_other 34 yes gas_line_other 35 yes overhead_wires 36 yes overhead_wires	Moderate cracks along center and ed readway. Coast of all above culters in the cracks along center and ed readway. Coast of all sheaping days and end and catheyly overtured with is a moderate cracks in a class of a class	open_botts.skewed_345* no_discernable_stream_char open_botts.skewed_345* no_discernable_stream_char six of open_botts.skewed_345* no_discernable_stream_char the photo- photo- photo- round roadway_aligned stream_aligned way. Gas no_discernable_stream_char round skewed_45* no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char the seen open_botts.stweed_45* skewed_45* goen_botts.roadway_aligned skewed_445*	nnel no_dry_bottom vez_25_50% nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel flowing no flowing vez_25_50% flowing vez_25_50% flowing no	sediment_leves_b tractive statem_grade dry_laid_stone sediment_leves_b structrupstream sciences_be structrupstream sciences_be structrupstream sciences_be structrupstream sciences_be structrupstream sciences_be st_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe st_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe at_stream_grade corrupsted_metal_pipe at_stream_grade dry_laid_stone at_stream_grade dry_laid_stone at_stream_grade dry_laid_stone at_stream_grade reinforced_concrete_pipe sediment_leves_v within_culvert_up_equal_concrete_pipe at_stream_grade reinforced_concrete_pipe	18 18 0 18 18 0 18 17 0 18 24 26 0 24 26 0 40 56 40 40 47 40 180 44 46 180 44 80 96 48 96 96 54 86	O failing moderate adwall O failing none Culvert.he O failing moderate adwall O failing minor ulvert Culvert.he O failing minor ulvert 27 fair 36 1 satisfactory none 2 satisfactory none 6 good none headwall 8 good none	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_tham_2:1_ fair	the brick fair Embankment shows minor no no make brick fair Embankment shows moderno no make brick fair Minor erosion on embankn no no brick fair Embankment shows moderno no brick fair Embankment shows minor no no brick fair Embankment shows moderno no brick fair Embankment shows severe yes no brick fair Moderate erosion around emt no no brick fair Moderate erosion around emt no no brick good Embankment shows minor no no brick good Embankment shows minor no no no brick good Embankment shows minor no no no brick good Embankment shows minor no no no good Embankment shows minor no good Embankment shows mi	Culvert outlet is located approximately 10 feet even from 24 approximately 10 feet away from 12 inch corrupted metal diveway culvert outlet board approximately 2 feet inches 10 approximately 2 feet away from 12 approximately 2 feet approximately
8 - 3420 8 - 670 8 - 671 8 - 660	11/20/2020 13:05 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 19:27 overcast ewm yes 11/20/2020 19:27 overcast ewm yes 11/20/2020 19:27 overcast ewm yes 11/20/2020 19:37 overcast ewm yes 11/20/2020 13:04 rain ewm yes 11/23/2020 13:04 rain ewm yes 11/23/2020 13:04 rain ewm yes 11/23/2020 13:04 rain ewm yes	yes no yes yes no yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved satisfactory paved satisfactory	26 20 20 22 22 22 25	no 50 yes overhead_wires yes overhead_wires_gas_line yes overhead_wires_gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires 30 yes overhead_wires 30 yes overhead_wires 30 yes overhead_wires 31 yes overhead_wires	Moderate cracks along center and ed roadway. Coast of all above culers in a common control of the coast of a c	open_botts.skewed_345* no_discernable_stream_char open_botts.skewed_345* no_discernable_stream_char six of open_botts.skewed_345* no_discernable_stream_char the photo- photo- photo- round roadway_aligned stream_aligned way. Gas no_discernable_stream_char round skewed_45* no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char the seen open_botts.stweed_45* skewed_45* goen_botts.roadway_aligned skewed_445*	nnel no_dry_bottom ves_25_50% nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel flowing no flowing ves_25_50% flowing ves_25_50%	sediment_leves_b trackers at_stream_grade dry_laid_stone scorrupted_metal_pipe at_stream_grade corrupted_metal_pipe at_stream_grade dry_laid_stone at_stream_grade reinforced_concrete_pipe	18 18 0 18 18 0 18 17 0 18 24 26 0 24 26 0 40 56 40 40 47 40 180 44 46 180 44 80 96 48 96	O failing moderate adwall O failing none Culvert.he O failing minor ulvert O failing minor ulvert 27 fair 36 1 satisfactory none 2 satisfactory none 6 good none headwall	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2.1_ fair	ne_brick fair Embankment shows minor no ni Moderate vegetation and too ni Moderate fair Minor erosion on embankin no ni Moderate fair Embankment shows minor no ni Moderate erosion around ent too ni Moderate erosion	Cubern outlet is located approximately 10 feet even from 24 approximately 10 feet even from 25 approximately 10 feet even from 25 approximately 10 feet even from 25 approximately 16
8 - 342a 8 - 67c 8 - 671 8 - 66c 8 - 66i 8 - 50i 8 - 50c 8 - 105i	11/20/2020 18:26 overcast ewm yes 11/20/2020 18:21 overcast ewm yes 11/20/2020 18:21 overcast ewm yes 11/20/2020 18:27 overcast ewm yes	yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved good paved good paved good paved good paved good paved satisfactory	26 20 20 22 22 25 25 25 22 22 22 22 22 22 22 22	no 50 yes overhead_wires yes overhead_wires,gas_line yes overhead_wires,gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires 30 no 34 yes gas_line,other 34 yes gas_line,other 34 yes gas_line,other 34 yes overhead_wires 33 no 41 yes overhead_wires 41 yes overhead_wires	Moderate cracks along center and edi- yes guardrall failing unset and actively overtured with Is Moderate cracks along center and edi- randway, Coast of all sheaty editors with the Robert of the coast of a sheaty of the center. Guard or failing on of posts and posts are actively overtured yes guardrall failing overturening. Minor to moderate cracking along collent len yes guardrall failing vorturening. Minor cracking along collent len yes guardrall failing rail has failen off posts and post are yes guardrall failing rail has failen off posts and os severely yes guardrall failing rail has failen off posts and is severely yes guardrall failing rail has failen off posts and is severely yes guardrall failing rail has failen off posts and is severely yes guardrall failing rail has failen off posts and is severely yes guardrall failing rail has failen off posts and is severely yes guardrall failing rail has failen off posts and	open_botts:kewed_45* no_discernable_stream_char open_botts:discernable_stream_char open_botts:kewed_45* skewed_45* open_botts:discerdawy_aligned open_discernable_stream_char open_botts:kewed_45* skewed_45* open_botts:discerdawy_aligned open_discernable_stream_char open_botts:discerdawy_aligned open_discernable_stream_char open_botts:discerdawy_aligned skewed_45* skewed_45* open_botts:discerdawy_aligned open_discernable_stream_char open_discernable_stream_char open_botts:discerdawy_aligned skewed_45* skewed_45* open_botts:discerdawy_aligned open_discernable_stream_discernable_stream_char open_discernable_stream_char open_discernabl	nnel no_dry_bottom vel_25_50% nnel completely_o no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel flowing no flowing vel_25_50% flowing vel_25_50% flowing vel_25_50% flowing vel_25_50% flowing vel_25_50% flowing vel_25_50%	sediment_leaves_b structcupstream grade dry_laid_stone st_stream_grade reinforced_concrete_pipe st_stream_grade reinforced_concrete_pipe st_stream_grade reinforced_concrete_pipe st_stream_grade corrupated_metal_pipe st_st_stream_grade corrupated_metal_pipe st_st_stream_grade corrupated_metal_pipe st_st_stream_grade corrupated_metal_pipe st_st_stream_grade corrupated_metal_pipe st_st_stream_grade corrupated_metal_pipe st_st_st_st_st_st_st_st_st_st_st_st_st_s	18 18 0 18 18 0 18 18 0 18 17 0 18 24 26 0 24 26 0 40 40 47 40 180 44 46 180 44 80 96 48 96 96 54 86 12 11 12	O failing moderate adwall O failing none Culvert.he O failing moderate adwall O failing minor ulvert O failing minor ulvert Culvert.he O failing moderate adwall.c 1 satisfactory none 2 satisfactory none 6 good none headwall 8 good none 3 satisfactory moderate headwall 1 satisfactory moderate headwall	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2.1_ fair	ne_brick fair Embankment shows minor no no model fair Embankment shows minor no no model fair Embankment shows model no no model fair Embankment shows model no no model fair Embankment shows model no no model fair Embankment shows minor no no model fair Embankment shows model no no model fair Embankment shows model no no model fair Embankment shows severa yes no model fair Moderate erosion along edityes no model fair Embankment shows minor no	Culvert outlet is located approximately 10 feet easy from 24 part of 10 feet and 10 feet a
8 - 3420 8 - 670 8 - 671 8 - 660 8 - 661 8 - 500 8 - 1051	11/20/2020 18:06 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:26 overcast ewm yes 11/20/2020 18:32 overcast ewm yes 11/20/2020 19:37 overcast ewm yes 11/20/2020 19:37 overcast ewm yes 11/20/2020 19:37 overcast ewm yes 11/20/2020 13:48 overcast ewm yes 11/20/2020 13:49 rain ewm yes 11/20/2020 13:49 rain ewm yes 11/20/2020 13:43 rain ewm yes 11/20/2020 13:43 rain ewm yes 11/20/2020 13:43 rain ewm yes 11/20/2020 13:45 rain ewm yes 11/20/2020 13:45 rain ewm yes	yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved satisfactory paved good paved good paved good paved satisfactory paved satisfactory paved satisfactory	20 20 22 22 22 25 25 25	50 yes overhead_wires yes overhead_wires,gas_line yes overhead_wires,gas_line 53 yes overhead_wires 30 yes overhead_wires 30 no 34 yes gas_line_other 34 yes gas_line_other 34 yes gas_line_other 34 yes overhead_wires 33 no 41 yes overhead_wires	Moderate cracks along center and ed readway. Coast of all above culerts in the cracks along center and ed yes guardrall failing nuclear cands along center and ed and actively overtured with his moderate cracks and actively overtured with his candidate control of the coast of a liberate coast of a liberate cracks throughout the center. Guard or failing line of posts and posts are actively overturning. Whinto to moderate cracking along road line crosses roadway along pipe length overturning. Whinto to moderate cracking along road line crosses roadway along pipe length yes guardrall failing rails has failen off posts and is severely as guardrall satisfactory Minor cracking along edge of roadway. Whinto cracking along edge of roadway. Minor cracking and dips along roadway is guardrall fair guardral and posts. Minor cracking and dips along roadway. Whinto cracking and dips along roadway of the control of the coast	open_botts.skewed_345* no_discernable_stream_chainers open_botts.skewed_345* no_discernable_stream_chainers open_botts.skewed_445* no_discernable_stream_chainers open_botts.skewed_445* no_discernable_stream_chainers round roadway_aligned stream_siligned way. Gas no_discernable_stream_chainers round skewed_445* no_discernable_stream_chainers round roadway_aligned no_discernable_stream_chainers round roadway_aligned no_discernable_stream_chainers round roadway_aligned no_discernable_stream_chainers open_botts.roadway_aligned no_discernable_stream_chainers open_botts.roadway_aligned no_discernable_stream_chainers open_botts.skewed_445* skewed_445* open_botts.skewed_445* skewed_445* round skewed_345* stream_aligned round skewed_345* stream_aligned round skewed_345* stream_aligned	nnel no_dry_bottom ves_25_50% nnel no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel flowing no flowing ves_255.60% flowing ves_25_50%	upstream ranches at stream_grade dry_laid_stone sediment_leaves_b structerupstream statisteam_grade dry_laid_stone sediment_leaves buried_clogged_col dry_laid_stone statistream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade corrugated_metal_pipe at_stream_grade dry_laid_stone at_stream_grade reinforced_concrete_pipe stream_grade reinforced_concrete_pipe at_stream_grade corrugated_metal_pipe	18 18 0 18 18 0 18 17 0 18 24 26 0 24 26 0 40 47 40 180 44 46 180 44 46 180 44 80 96 48 96 96 54 86 12 11 12	O failing moderate adwall O failing none Culvert.he O failing moderate adwall O failing minor uvert O failing moderate adwall 27 fair 36 1 satisfactory none 6 good none headwall 8 good none 3 satisfactory moderate headwall	Natural bottom sediment increases with culvert length. If it is a proper process of the control	the brick fair Embankment shows minor no no make brick fair Embankment shows moderno no be brick fair Minor erosion on embanknoo no be brick fair Embankment shows moderno no be brick fair Embankment shows moderno no be brick fair Embankment shows moderno no brick fair Moderate erosion around emt no no brick fair Moderate erosion around emt no no brick fair Moderate erosion around emt no no brick good Embankment shows minor no no put no brick good Embankment shows minor no no put no brick good Embankment shows minor no no put no brick good Embankment shows minor no no put no brick good Embankment shows minor no no put no brick good Embankment shows minor no no put no brick satisfactory Water pools on top right si no no put	Culvers outlet is located approximately 10 feet even from 24 submitted and anti-replaced with abundanced and replaced and replac
8 - 3420 8 - 670 8 - 671 8 - 660 8 - 661 8 - 500 8 - 1050 8 - 1050 8 - 1050 8 - 1050	11/20/2020 13:05 overcast ewm yes 11/20/2020 18:06 overcast ewm yes 11/20/2020 19:07 overcast ewm yes 11/20/2020 13:04 rain ewm yes 11/20/2020 13:05:08 rain ewm yes 11/20/2020 15:08 rain ewm yes 11/20/2020 15:08 rain ewm yes 11/20/2020 15:08 rain ewm yes 11/20/2020 15:09 rain ewm yes 11/20/2020 15:09 rain ewm yes	yes no yes yes no yes	paved fair paved fair paved satisfactory paved satisfactory paved good paved good paved satisfactory paved satisfactory paved satisfactory paved good paved good paved good paved satisfactory paved satisfactory	26 20 22 22 25 25 25 22 22 25 25 26 20 20 20 20 20 20 20 20 20 20 20 20 20	no 50 yes overhead_wires yes overhead_wires,gas_line yes overhead_wires,gas_line 53 no 53 yes overhead_wires 30 yes overhead_wires 30 yes overhead_wires 34 yes gas_line,other 34 yes gas_line,other 34 yes gas_line,other 34 yes overhead_wires 41 yes overhead_wires 41 yes overhead_wires 41 yes overhead_wires 65 no 50 yes overhead_wires	Moderate cracks along center and edi- yes guardrall failing unset and actively overtured with Ia Moderate cracks along edges and center readway, Coast of all sheatyly vacted with Ia Moderate cracks and actively overtured with Ia Moderate cracks and actively overtured with Ia Read of the coast of all sheatyly vacted with Ia Moderate cracks program of the center. Coast of a failing or cracks throughout the center. Coast of a failing or cracks throughout the center. Coast of a failing overturening. Whinor to moderate cracking along coliver feet and Incomment of the coast of the co	open_botts.kewed_345* no_discernable_stream_char open_botts.kewed_345* no_discernable_stream_char six of open_botts.kewed_345* no_discernable_stream_char open_botts.com round roadway_aligned stream_stigned way. Gas no_discernable_stream_char round skewed_45* no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char round roadway_aligned no_discernable_stream_char stream_char round roadway_aligned no_discernable_stream_char tested. open_botts.roadway_aligned skewed_445* skewed_445* skewed_445* stream_aligned round skewed_345* stream_aligned round skewed_445* stream_aligned	nnel no_dry_bottom ves_25_50% nnel completely_o no_moist_bottom no no_moist_bottom no no_moist_bottom no no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel no_dry_bottom no nnel flowing no flowing ves_25_50% flowing ves_25_50%	sediment_leaves_b structerupstream ranches structerupstream sciences_be structerupstream sciences_be structerupstream sciences_comment_leaves sciences_comment_leaves structerupstream sciences_comment_leaves structerupstream sciences_comment_leaves structerupstream sciences_comment_leaves structerupstream sciences_comment_leaves structerupstream grade corrupsted_metal_pipe st_stream_grade corrupsted_metal_pipe st_stream_grade corrupsted_metal_pipe st_stream_grade corrupsted_metal_pipe st_stream_grade dry_laid_stone st_stream_grade reinforces_concrete_pipe st_stream_grade reinforces_concrete_pipe st_stream_grade corrupsted_metal_pipe	18 18 0 18 18 0 18 17 0 18 24 26 0 24 26 0 40 56 40 40 47 40 180 44 80 96 48 96 96 54 86 12 11 12	O failing moderate adwall O failing none Culvert.he O failing moderate adwall O failing minor ulvert O failing minor ulvert Culvert.he O failing moderate adwall 27 fair 36 1 satisfactory none 6 good none headwall 8 good none 3 satisfactory moderate headwall 1 satisfactory moderate headwall 1 satisfactory moderate headwall 0 good none 0 good none 0 satisfactory moner culvert	Natural bottom sediment increases with culvert length. headwall_retaining_wall_slo 4 fair dry_laid_sto slope_steeper_than_2:1_ fair	ne_brick fair Embankment shows minor no no model fair Embankment shows minor no no model fair Embankment shows model no no no help fair Embankment shows severe yes no no no help fair Moderate erosion around ent no no no help fair Moderate mosion godges no no help fair Moderate mosion no no no no help fair Embankment shows minor no no no no help fair Embankment shows minor no no no no help fair Embankment shows minor no no fambankment shows minor no no fambankment shows minor no no fambankment shows model	Culvern coullet is located approximately 10 feet eavany from 24 approximately 10 feet eavany from 12 feet feet for 10 feet feet feet feet feet feet feet fee

											Headwall stones have distorted
B - 344i	11/23/2020 16:30 rain ewm yes yes	paved s	tisfactory 22	58 yes gas_line		pipe length. round roadway_aligned skewed_<45°	stagnant no	at_stream_grade hdpe	36 36 18	2 satisfactory minor culvert	round shape at right side for the satisfactory of y laid, stone brick satisfactory Headwall and embankmen no no General tree growth. satisfactory -70.9905 42.69139
B - 345i	11/23/2020 16:47 rain ewm ves ves	paved s	tisfactory 24	65	Minor cracking along edges of ro Multiple bollards within steel cab yes guardrail,stfailing are crumbling.	dway. e guard rail round roadway aligned no discernable stream chan	inel stagnant ves <25%	within_culvert,do leaves,sediment,b wnstream ranches at stream grade corrugated metal pipe	18 16 18	4 failing none	Majority of pipe has rusted away. Severe rust throughout. remaining but of pipe. headwall retaining wall, slo 4.5 falling concrete poured failing Severe erosion of embankino no failing 70.99 42.68586
B = 3451	11/23/2020 16:47 Fain ewm yes yes	paved s	tisfactory 24	65	yes guardrau, strailing are crumbling. Minor cracking along edges of ro Multiple bollards within steel cab	dway.	inei stagnant yes_<25%	wnstream rancnes at_stream_grade corrugated_metal_pipe	18 16 18	4 failing none	remaining dist of pipe. Inestinating wall, so 4.5 rating concrete_pource_ rating Severe elstoon of embanation no rating -7(0.99 42.66566) 1st foot of pipe has bottom likely slid within prior existing
B - 345o	11/23/2020 16:59 partly_cloudy ewm yes yes	paved s	tisfactory 24	65 yes overhead_wires	yes guardrail,stfailing are crumbling.	round roadway_aligned no_discernable_stream_chans		perched hdpe branches,leaves,s	12 12 5	1 failing none	completely missing. slope_steeper_than_2:1_ 3.5 fair Embankment shows minor no yes culvert. poor -70.9901 42.68572 Moderate scaling on pise OUrfall area leads into closed
B - 1180	11/23/2020 17:35 rain ewm yes yes 11/23/2020 17:48 rain ewm yes yes		tisfactory 21 tisfactory 24	52 yes gas_line,drainage_system			stagnant yes_25_50% stagnant yes <25%		15 10 14 15 12 12	2 satisfactory none 2 satisfactory none	9 opening, headwall_retaining_wall_slo 2_satisfactory concrete_soured_ fair Moderate scaling throughon on drainage system. Subvert leads into closed drainage Culvert leads into closed drainage
B - 118i B - 117	11/23/2020 17:48 rain ewm yes yes 11/23/2020 18:27 rain wnb yes no	paved s	tistactory 24	52 yes gas_line	no Minor cracking along edges of ro	dway. round skewed_>45° skewed_<45°	stagnant yes_<25%	within_culvert leaves, sediment at_stream_grade reinforced_concrete_pipe	15 12 12	2 satisfactory none	3 headwall_retaining_wall_slo 1.5 good mortared_stone_brick good no no system. good -70.9905 42.68228 Inlet was part of closed drainage -70.9815 42.6829 -70.9815 42.6829
											High velocity flow going through
B - 119o	11/23/2020 18:33 rain ewm yes yes	paved s	tisfactory 95	125 yes overhead_wires,gas_line,	drainage_sys no Minor cracks and dips along road	way. round roadway_aligned stream_aligned	flowing no	perched hdpe	15 15 13	5 satisfactory	pipe. Flow speed prevented stream. Roadway is comprised of a stream. Roadway is comprised of a stream. Roadway is comprised of a stream. Control of the compression of the control of the compression of the control of
											Inlet of culvert is submerged and covered by miscellaneous stones,
B - 119i	11/23/2020 18:43 rain ewm yes no	paved sa	tisfactory 95	125 yes overhead_wires,drainage	_system,gas_no Minor cracks and dips along road	way. round roadway_aligned no_discernable_stream_chann	inel flowing	hdpe	15		making culvert not visible. slope_flatter_than_2-1. fair dry_laid_stone_brick Embankment shows minor no General thorn brush growth70,9816 42,68243 Stone plated on top of culvert is
B - 346i	11/23/2020 19:04 overcast ewm yes yes	paved s	tisfactory 22	51	no Moderate cracking along edge of	roadway. round roadway_aligned no_discernable_stream_chann	nel flowing no	at_stream_grade hdpe	15 15 13	4 satisfactory minor culvert	crushing top of pipe due to fallen Large beaver cage placed in front of tree. slope_steeper_than_2:1_ 3 poor Moderate to severe erosionyes no inlet. fair -70.9841. 42.68394
											HDFE pipe was slid within a prior
B - 346o	11/23/2020 19:20 overcast ewm yes yes	paved s	tisfactory 22	51 yes gas line	no Moderate cracking along edge o	roadway. round roadway aligned skewed <45°	flowing no	at stream grade hdpe	15 15 10	1 satisfactory moderate headwall	existing corrugated metal pipe. Small gap between pipes allowing General tree, thorn, and vegetation 7 water to pass unmult HDF pipe. headwall retaining wall, slo 3 poor dry_laid_stone_brick poor Embankment shows mode no no growth. poor -70.9839 42.68386
B = 3460	11/23/2020 19:20 overcast ewm yes yes	paved s	instactory 22	31 Ac2 892 mis	no woderate tracking along edge of	tosuway. Tourid Tosuway_angried salewed_543	nowing no	at_stream_grade mope	15 15 10	1 satisfactory moderate neadwar	/ water to pass around more pipe. Tecomen_tearing_was,too 3 poor ortg_ami_toot_aminers shows indicating those indicating the grown.
											Culvert opening is a flared end section. There are two boards of section. There are two boards of wood placed in front of the flared
											woo paces in more or the harde end section preventing direct ent: Exposed rebar throughout Culvert opening is a flared and
B - 347i	11/24/2020 13:20 partly_cloudy ewm yes yes	paved fa	ir 18	115 yes overhead_wires	no Moderate cracking along edges o	roadway. round skewed_>45° no_discernable_stream_chani	nel no_moist_bottom yes_<25%	downstream other at_stream_grade reinforced_concrete_pipe	18 18 0	0 satisfactory none	flared end section. General moss section with a board of wood section with a board of wood growth. Using steeper_than_21_ 5 satisfactory Minor erosion and general no no preventing deteractess. satisfactory 70.9707 42.68399
											Cultert opening is a flarted and Section. Milror scaling at sides of Abutting neighbor mentioned that pipe due to water. General moss they desend out the stream area
B - 347o	11/24/2020 13:36 partly_cloudy ewm yes yes	paved fa	ir 18	115 yes overhead_wires	no Moderate cracking along edges of	roadway. round skewed_>45° stream_aligned	no_moist_bottom no	at_stream_grade reinforced_concrete_pipe	18 18 0	0 satisfactory none	1.5 growth. slope_steeper_than_2:1_ 5.5 saltisfactory General tree, vegetation, a no no once a year. satisfactory -70.9702 42.68396 Culvert was completely buried
B - 348o	11/24/2020 14:05 partly_cloudy ewm yes no	paved fa	ir 27	57 yes overhead_wires	no	round skewed_<45° no_discernable_stream_chans	nel completely_c	within_culvert,up sediment,leaves,o bstructestream ther buried_clogged_col corrugated_metal_pipe	12 0 12	failing none	upon arrival is lunable to flow water. headwall_retaining_walf,slo 3 fair dry_laid_stone_brick satisfactory Embankment shows minor no no Wetland flags located nearby. falling -70.9714 42.68516
B - 348i	11/24/2020 14:26 partly_cloudy ewm yes no	paved fa	ir 27	57 yes overhead_wires	no Moderate cracking along edges of	roadway. round skewed_<45° no_discernable_stream_chani	nel completely o	upstream,within_sediment,leaves,o bstructeculvert ther buried_clogged_col corrugated_metal_pipe	12		Cuivert is completely buried by We were able to locate the outlet sediment, leaves, and rocks. headwall retaining, wall, lope_steeper t poor dry_laid_stone_brick Embankment shows mode no no but unable to view) locate the inlet. falling -70.9712 42.68523
											Pipe opening is a flared end
B - 116i	11/24/2020 14:48 partly cloudy ewm yes yes	paved fa	ir 19	85 yes overhead wires	Moderate cracking along center no roadway.	n edges of round skewed <45° no discernable stream chan	inel no dry bottom ves <25%	within culvert sediment,leaves at stream grade reinforced concrete pipe	18 18 15	0 satisfactory none	section. Exposed rebar within Rared end section. Minor scaling a bottom of lopic due to water. slope steeper than 2:1 4.5 fair Moderate Erosion. General no no fair 70.9703 42.68309
		,	-		·		, ,				Pipe opening as a flared end
					Moderate cracking along center	- desired		sediment,leaves,b within_culvert,up ranches,debris_tra			section. Minor scaling on sides of pipe due to high water. Large 18 inch tree located directly Discoloration of contrale. above culvert. Roots have grown
B - 116o	11/24/2020 15:11 partly_cloudy ewm yes yes	paved fa	ir 19	85 yes overhead_wires	no roadway.	round skewed_<45° no_discernable_stream_chani	inel no_dry_bottom yes_25_50%	stream sh at_stream_grade reinforced_concrete_pipe	18 13 24	0 satisfactory none	Consolicitation of Cubis Res. Control Immographia provides. Control Immographia provides. Autor Expert. Industrializing youten above Listenic Autor Sizing youten above L
											Three side-by-side 4 inch PVC outlet pipes approximately 10 feet
											First foot of culvert is missing metal on both sides. Remnants of out the linet are multiple times a
B - 349i	11/24/2020 15:28 partly_cloudy ewm yes yes	paved s	tisfactory 19	47 yes overhead_wires	Minor to moderate cracking alor no roadway.	edge of round roadway_aligned no_discernable_stream_chans	inel no_dry_bottom no	at_stream_grade corrugated_metal_pipe	15 15 0	0 failing minor headwall	black apphalftar mik. Severe rust year, and it hasn't been touched year, and it hasn't been touched on sides of jepe. headwall_retaining_wall_sio 2.5 satisfactory mortared_stone_brick fair Large 24 inch tree on left sin o no since the 70's. poor -70.9722 42.68216
											1st foot of opening has left side Abutting neighbor mentioned that completely rusted away. Remnants of black sephalt/par multiple times a year. Also
B - 349o	11/24/2020 16:14 partly_cloudy ewm yes yes	paved s	tisfactory 19	47 yes overhead_wires	no Moderate cracking along edges o	roadway. round roadway_aligned no_discernable_stream_chan:	inel no_dry_bottom yes_<25%	within_culvert sediment,other at_stream_grade corrugated_metal_pipe	15 11 15	0 failing minor headwall	neniamics of tack aspinary and initiative minitiative forms a series of the control of the contr
					Madazata anakina alam adaza			underson while and leave b			1st foot of pipe is mangled. Plink
B - 99i	11/24/2020 16:38 partly_cloudy ewm yes yes	paved fa	ir 22	40 no	Moderate cracking along edges of evidence_cGuard rail vfailing sections of asphalt eroding off.	round roadway_aligned no_discernable_stream_chann	inel no_dry_bottom yes_<25%	upstream,within_sediment,leaves,b culvert ranches,other at_stream_grade hdpe	24 23 15	0 fair moderate culvert	grate placed in front of culvent, Sign focused nearby mentions area likely acting as a beaver grate slope_steeper_than_2:1_ 1 failing Severe erosion on either skyes no is a Rowley water supply area. poor -70.9616 42.68731
											Within the first 2 feet of pipe there are some first of the first 2 feet of pipe there are spraids holes within
B - 99o	11/24/2020 16:54 partly_cloudy ewm yes yes	paved fa	ir 22	40 no	Moderate cracking along edges of evidence_sguardrail,steel_cable sections of asphalt eroding off.	roadway with round roadway_aligned skewed_>45*	no_moist_bottom no	perched hdpe	24 24 0	0 fair none	the HDPE. Water is pooling 7 within the middle of pipe. slope flatter than 2-1 1 poor Embankment is reinforced yes no fair -70.9616 42.68748 Moderate out throughout pice.
B - 109o	11/24/2020 17:15 partly_cloudy ewm yes yes	paved s	tisfactory 27	86 yes overhead_wires	no Minor cracking along edge of roa	dway. round roadway_aligned no_discernable_stream_chan	nel no_dry_bottom yes_25_50%	upstream leaves,sediment at_stream_grade corrugated_metal_pipe	12 8 12	0 fair none	Reminence of black asphaltylar mix. headwall_retaining_wall_ulo 1 good concrete_poured_ satisfactory Headwall_shows minor to nno no fair -70.5794 42.68518
B - 109i	11/24/2020 17:25 partly_cloudy ewm yes yes		sisfertania 27	86 yes overhead_wires	Minor cracking along edge of roz yes bollard satisfactory bollard is cracking along face.	tway. One round roadway_aligned stream_aligned	no_dry_bottom yes_<25%	within_culvert sediment,leaves at stream grade corrugated metal_pipe	12 10 11	0 fair none	Culmert shows suphalt/Larm ins. Utility guy wire is placed in middle 3 toottom sides of pipe. headwall_retaining_wall_sio 1 satisfactory concrete_poured_ good no no of stream. satisfactory -70.9796 42.68529
B - 1051	11/24/2020 17.25 partiy_cububy ewin yes yes	paved s	usiactory 27	oo yes overnead_wires	yes bosaid satisfactorybosaid is cracking along race.	Tourid toatway_angried scream_angried	no_ury_boctoni yes_C23%	within_tuwert seminent, leaves at stream_grade corrugated_metal_pipe	12 10 11	O fair Hone	S contain sees on pape. I recommand reasoning was poor 1 sets on the containing was poor 1 seed to 10 of section from 1 seed to 10 o
			tisfactory 35		system.wat ves guardrail good Minor cracking along edge of roa				48 36 48	29	Inspection of inside of culvert sock located upland behind guard was inaccessible due to high was inaccessible due to high waster. headwall retainine wall-slo 4.5 fair mortaned stone brick satisfactory Embankment shows minor no no lack decreasibility to culvert 70.9823 42,67832
B - 130i	11/24/2020 17:51 partly_cloudy ewm yes no	paved si	tistactory 35	70 yes overhead_wires,drainage	system,wat yes guardraii good Milnor cracking along edge or roa	way. round roadway_aligned no_discernable_stream_chani	inei stagnant	at_stream_grade corrugated_metal_pipe	48 36 48	29	water. headwall_retaining_wall_slo 4.5 fair mortaned_stone_brick satisfactory Embankment shows minor no no lack of accessibility to culvert70.9823 42.67832 Inside of culvert was Overall rating not given due to high
B - 130o	11/24/2020 18:26 partly_cloudy ewm yes no	paved sa	tisfactory 35	70 yes overhead_wires,drainage		round roadway_aligned no_discernable_stream_chan	inel stagnant yes_25_50%		48 29 48	26	uninopectable due to high water. Neadwall_retaining_wall_silo 5 satisfactory mortared_stone_brick satisfactory Embankment has general t no no water making culvert uninopectable70,9824 42,67815 Culver it as there sided box with
B - 140i	11/24/2020 18:58 clear ewm yes yes	paved s	tisfactory 25	38 yes Drainage system	Minor cracking on edge of roadw existing concrete bollard and gua yes guardrail failing mostly rotted away.		no_moist_bottom yes_<25%	sediment,leaves,b ranches,vegetatio upstream n_growth at_stream_grade mortared_stone	132 19 132	0 satisfactory none	sides of morate and stone and a Small black wire located on top of top that is with reinforced wing walls, 12 inch R.P. drainage 11 concrete. headwall retaining wall, so 4 poor concrete poured satisfactory Moderate to severe erosion no no outler located within left wing wall. fair -70.9803 42.67431
								sediment,leaves,v egetation_growth,			Culvert is a three sided box with
B - 140o	11/24/2020 19:10 clear ajs yes yes	paved s	tisfactory 25	38 yes overhead_wires	no Minor cracking along edge of ros Minor cracking along center of ro		no_moist_bottom yes_25_50%	downstream branches at_stream_grade mortared_stone	120 18 120	0 satisfactory none	12 a grante top. headwall_retaining_wall,slo 3 poor concrete_poured_ satisfactory Embankment shows severe no no fair -70.9802 42.67424 Like 1 & 3: pile of leaves and branches 1 & 3: pile of leaves and branches
B - 144i	11/24/2020 19:24 clear ewm yes yes	paved s	tisfactory 20	28 no	guard rail posts still existing and a evidence_sguardrail overturned.		flowing yes_<25%	upstream leaves,branches at_stream_grade corrugated_metal_pipe	363 36 263	5 satisfactory minor headwall	at Intel opening, All: minor: through a With 1 being closest to through 2 with 1 being closest to 12 surface runt throughout. Headwell retaining well, slo 2.5 poor dry_laid_stone_brick fair Moderate to severe erosion n Depth Road, fair 70.9802 42.67353
											Culvert comprised of three corrugated metal pipes numbered
					Minor cracking along center of re						1: Rocks down stream one through three with one being obstructing outflow 2.62: does to Depot Road. Weltland
B - 144o	11/24/2020 19:40 clear ewm yes yes	paved s	tisfactory 20	28 yes overhead_wires	existing guard rail still has posts t evidence_sguardrail overturned.	at are actively round skewed_<45° stream_aligned	flowing no	perched corrugated_metal_pipe	363 36 233	3 satisfactory minor culvert	Minor moss growth, 2: Location flags located nearby, Severe erosion 6 of scour. All: Minor surface rust. headwall_retaining_wall,slo 2 poor dry_laid_stone_brick fair Moderate to severe erosion no of streambed embankmeant. fair .70,98 42,67357
B - 154	11/24/2020 19:58 clear wnb culvert_was_not_located_d no										Culvert opening as a flared end section. Severe scaling
								leaves,sediment,o			throughout pipe. Sediment obstruction increases with pipe
B - 350i	11/24/2020 20:07 clear ewm yes yes	paved s	ntisfactory 22	55 yes drainage_system,other	yes guardrail, c satisfactory Electrical box seen up street.	round skewed_<45° no_discernable_stream_chan	inei no_dry_bottom yes_50_75%	within_culvert ther at_stream_grade reinforced_concrete_pipe	12 6 12	0 fair none	length. headwall_retaining_wall_slo 3 poor dry_laid_stone_brick poor Embankment has moderaturo no poor 70.9815 42.66604 Minor scaling throughout pipe.
								sediment,leaves,b			Opening of pipe is a flared end section. See a flared end section. Large gile of leaves and Bictrical box located
B - 350o	11/24/2020 20:19 clear ewm yes yes	paved s	tisfactory 22	55 yes other,drainage_system	yes guardrail satisfactory Electrical boxes seen up street.	round skewed_<45° no_discernable_stream_chann	nel no_dry_bottom yes_50_75%	downstream,with ranches, vegetatio in_culvert n_growth at_stream_grade reinforced_concrete_pipe	12 6 12	0 satisfactory none	thom buth growth within flared approximately 20 feet away from end section. slope_steeper_than_2:1_ 3.5 poor Moderate to severe erosion no outlet. fair -70.9816 42.66608
								leaves, branches, ot			Twin 18" RCP with flarned end section, separation between FES
B - 161i	11/25/2020 13:16 overcast ewm yes yes	paved fa	ir 22	60 yes other	yes guardrail satisfactory Underground utilities	round skewed_<45° no_discernable_stream_chann	nel no_dry_bottom yes_<25%	upstream her at_stream_grade reinforced_concrete_pipe	18 18 0	0 satisfactory none	and pipe, minor most growth slope_steeper_tham_2.1_ 4 fair Embankment reinforced wit no no General tree growth fair 70.9812 42.6647 What 18** ROY, minor scaling to Outstall area is layer with stones,
B - 161o B - 142i	11/25/2020 13:29 overcast ewm yes yes 11/25/2020 13:50 overcast ewm yes yes	paved for paved p	= 22 oor 20	60 yes other 55 yes overhead_wires	yes guardrail satisfactory Underground utilities no General fatigue cracking	round skewed_ <as* no_discernable_stream_chani="" round="" skewed_="">45° stream aligned</as*>	flowing no	within_culvert sediment at_stream_grade reinforced_concrete_pipe at_stream_grade reinforced_concrete_pipe	18 17 0 36 35 28	0 satisfactory minor 6 satisfactory moderate headwall	8 Minor cracking in sides of pipe headwall retaining, wall,slo 1.5 fair mortared stone brick satisfactory Minor to moderate Erosion no no growth, general tree growth satisfactory 70.9726 42.67383
							-				Minor surface cracking on sides General tree growth, recently of pipe, chips of outlet pipe surveyed - "Its come" stake in
B - 142o	11/25/2020 14:01 overcast ewm yes yes	paved p	por 20	55 no	no General fatigue cracking	round skewed_>45° skewed_<45°	flowing yes_<25%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	36 26 35	7 satisfactory none	14 missing headwall_retaining_wall_slo 2 fair mortared_stone_brick poor Embankment-minor to morno no embankment fair -70.9725 42.67376 Minor scaling to high water, 2°22° spall in side of pipe
B - 138i	11/25/2020 14:17 overcast ewm yes yes	paved fa	ir 22	75 no	no Minor rutting in north lane whee	rut round skewed_<45° no_discernable_stream_chani	nel stagnant yes_<25%	upstream,within_leaves,sediment,o culvert ther,branches at_stream_grade reinforced_concrete_pipe	30 29 26	8 satisfactory none	(Interior), beaver grate on grant of nine short steeper than 2-1 by 2 fair mortared stope brick fair Minor to moderate Fresion sees no fair - 70.9383, 42.67533.
								sediment,leaves,o			Beaver grate at pipe opening, minut to moderate scaling to high water, piths side of pipe
B - 138o	11/25/2020 14:28 overcast ewm yes yes	paved fa	ir 22	75 no	no Minor rutting/fatigue cracking	round skewed_<45° no_discernable_stream_chann		downstream ther at_stream_grade reinforced_concrete_pipe		4 satisfactory none	opining exposed rebar. headwall_retaining_wall_slo 2 poor dry_laid_stone_brick poor Embankment moderate eroyes no General tree growth poor 70.9746 42.67541 General tree growth, electric dog General tree growth, electric dog
B - 351i B - 351o	11/25/2020 14:42 overcast ewm yes yes 11/25/2020 14:50 overcast ewm yes yes		tisfactory 20 tisfactory 20	42 yes overhead_wires 42 no	no no	round roadway_aligned stream_aligned round roadway_aligned skewed_>45°	no_dry_bottom no no_dry_bottom no	at_stream_grade hdpe at_stream_grade hdpe	15 15 0 15 15 0	0 satisfactory minor culvert 0 satisfactory minor culvert	8 pipe headwall retaining wall,slo 3 satisfactory dry_laid_stone_brick satisfactory Headwall is extension of retno no fence flagging, satisfactory -70.9739 42.67764 8 headwall_retaining_wall_slo 3 fair dry_laid_stone_brick satisfactory Minor erosion on embankum no no satisfactory -70.9739 42.67771
			tisfactory 20	no	no	round skewed_>45° no_discernable_stream_chani	inel no_dry_bottom ves 25 50%	leaves,sediment,b ranches,vegetatio upstream n.growth at_stream_grade reinforced_concrete_pipe	12 12	satisfactory none	Flared end section perpendicular to road slope flatter than 2-1 1 fair no no Unclear of outlet location fair -70.975 42.67786
B - 352i	11/25/2020 15:03 overcast ewm yes yes	paved s						buried_clogged_collapsed			
	11/25/2020 15:03 overcast ewm yes yes ation 11/25/2020 15:07 overcast djn culvert_was_not_located_d no	paved s	instactory 20	yes overhead_wires							
B - 3520 Potential outlet loc	ation 11/25/2020 15:07 overcast djn culvert_was_not_located_d no				BO Repoliume natrik from entimet mat	cement round skewerd cd5° classical <a5°< td=""><td>no dry hottom</td><td>at tream grade hidos</td><td>12 12 0</td><td>0 good none</td><td>Appears to have been recently replaced, inlet opening slightly tree and vegetation growth,</td></a5°<>	no dry hottom	at tream grade hidos	12 12 0	0 good none	Appears to have been recently replaced, inlet opening slightly tree and vegetation growth,
			tisfactory 19	yes overhead_wires	no Roadway patch from culvert repl	cement round skewed_<45° skewed_<45°	no_dry_bottom no	at_stream_grade hdpe	12 12 0	0 good none	Appears to have been recently replaced, inlet opening slightly replaced, inlet opening slightly and tree and vegetation growth, recently replaced due to headwall retaining_wall_sio 2 satisfactory dry_laid_stone_brick satisfactory Minor voids between stone no no recently replaced. satisfactory 70.973 42.68038
B - 3520 Potential outlet loc	ation 11/25/2020 15:07 overcast djn culvert_was_not_located_d no	paved s			no Roadway patch from culvest repline	coment round skewed_45° skewed_45° round skewed_45° skewed_545°	no_dry_bottom no no_dry_bottom yes_<25%	at stream grade hippe leaves, branches, s within_culvert ediment at_stream_grade hippe	12 12 0 12 11 8	0 good none 0 good minor headwall	Appears to have been recently supplied in the property of the
B - 352o Potential outlet loc	11/25/2020 15:97 overcast djn culvert_was_not_located_d no 11/25/2020 15:13 overcast ewm yes yes	paved s	ttisfactory 19	11 no			no_dry_bottom yes_<25%	leaves, branches, s	12 11 8	0 good minor headwall	Appears to have been recently replaced, inter opening slightly replaced, inter opening slightly replaced, inter opening slightly a satisfactory dry_laid_stone_brick satisfactory Minor voids between stone no no recently replaced. satisfactory -70.973 42.68038 Recently replaced culvert, utility pole and gray were immediately

										A CONTRACTOR OF THE PROPERTY O	
B - 354o	11/25/2020 15:41 overcast ewm yes yes	paved satisfactory	4 61 yes overhead_wires	no	round roadway_aligned stream_aligned no_dry_	y_bottom yes_<25%	within_culvert sediment,leaves at_stream_grade reinforced_concrete_pipe	18 17 8	0 fair none	Second length of pipe is shifted off center by approximately 3 13 inches headwall_retaining_wall_slo 5 poor dry_laid_stone_brick fair Moderate to severe erosion no	no Fallen trees and limbs downstream fair -70.9721 42.67837
Adjacent to Perkin Woods entrance, o							other,sediment,le within culvert,up aves,branches,veg				Drop inlet to pipe, pipe visible but not accessible to inspect, general
B - 355i inlet structure	11/25/2020 15:53 overcast ewm yes yes	paved satisfactory	24 38 yes overhead_wires	no	round roadway_aligned no_discernable_stream_channel no_dry_	y_bottom yes_50_75%	stream etation_growth inlet_drop ductile_iron_pipe	12	none	slope_flatter_than_2:1_ 1.5 fair Minor to moderate Erosion no	no fallen trees and limbs -70.968 42.67409 Outlets to private residents yard,
B - 2550	11/25/2020 16:01 overcast ewm yes no	paved satisfactory	24 38 no		roadway_aligned no_discernable_stream_channel		buried_clogged_collapsed			Bricks laid in front of assumed pipe headwall_retaining_wall 3 poor dry_laid_stone_brick failing Precast headwall under dry no	apparent outlet buried/ clogged with sediment and brick stacked no above opening failing -70.9682 42.67399
B - 150i	11/25/2020 16:18 overcast ewm yes no	paved satisfactory	t1 40 no	yes guardrail fair	flowing	-	submerged		46	Completely submerged headwall_retaining_wall_slope_flatter_th:fair mortared_stone_brick Unable to inspect, submerged	no -70.9694 42.67092
B - 150o	11/25/2020 16:23 overcast ewm yes no	paved fair	11 40 no	yes guardrail fair	roadway_aligned flowing	8	submerged sediment,leaves,v		54	Submerged and unable to inspect headwall_retaining_wall,slope_steeper_t fair mortared_stone_brick Submerged and unable to inyes	no Submerged and unable to inspect -70.9692 42.67085 Private 12inch CMP adjacent to outlet south of culvert, fallen trees
B -153o B - 153i	11/25/2020 16:34 partly_cloudy ewm yes yes 11/25/2020 16:43 overcast ewm culvert_was_not_located_does_no	paved fair	11	no Minor pothole above potential culveri	ocation round skewed_>45° no_discernable_stream_channel no_dry_	y_bottom yes_25_50%		12 9 11	0 satisfactory none		outlet south of curvert, failen trees no and limbs fair -70.9737 42.66918 -70.9736 42.66929
										Approximately 10 feet into pipe, pipe separation and	Council to a new the and fellow
B - 158i	11/25/2020 16:54 overcast ewm yes yes	paved	14 100 yes other	no Underground electrical	round roadway_aligned no_discernable_stream_channel no_dry_	y_bottom no	at_stream_grade corrugated_metal_pipe downstream,with	18 18 0	0 fair minor headwall	disformation, minor surface rust at bottom of pipe headwall_retaining_wall.slo 10 fair mortared_stone_brick satisfactory Minor to moderate Erosion no Minor surface rust at bottom of	General tree growth and fallen no limbs fair -70.9717 42.66673 General vegetation growth
B - 158o	11/25/2020 17:04 overcast ewm yes yes	paved satisfactory	14 100 no	no	round roadway_aligned no_discernable_stream_channel no_dry_	y_bottom yes_<25%	in_culvert sediment,leaves at_stream_grade corrugated_metal_pipe	18 18 9	0 satisfactory minor headwall	pipe headwall_retaining_wall.slo 13 fair mortared_stone_brick satisfactory Minor to moderate Erosion no	no throughout outlet area satisfactory -70.9712 42.66652
										Sand bags placed in culvert opening to prevent flow, sediment buildup behind	
							vegetation_growt			sandbags, 4 ft into pipe, large deformation on top side with	
B -165o	11/25/2020 17:19 overcast ewm yes yes	paved satisfactory	13 90 no	no	round skewed_>45° no_discernable_stream_channel no_dry_	y_bottom yes_75_100%	h,other,debris_tra upstream sh at_stream_grade corrugated_metal_pipe	15 6	0 poor none	possible hole, separation 10 ft into pipe with sediment observed headwall_retaining_wall_slo 7 satisfactory mortared_stone_brick satisfactory Heavily vegetated, sporadic no	Sand bags placed to stop flow from no outlet poor -70.9733 42.66411
B - 165i	11/25/2020 17:31 overcast ewm yes yes	paved satisfactory	13 90 yes other	no Underground electrical utilities	round skewed >45° no discernable stream channel no dry	y bottom yes 50 75%	upstream,within_sediment,leaves,o culvert ther,branches at stream grade corrugated metal pipe	15 7 14	0 satisfactory none	Large sediment buildup at inlet headwall retaining wall slo 8 poor mortared stone brick satisfactory Severe erosion of embankir no	Electrical boxes immediately no adjacent to headwall fair -70.9736 42.66436
B - 169	11/25/2020 18:08 overcast djn culvert_was_not_located_does_no			Minor to moderate cracking along room						Moderate scaling at bottom and	-70.9713 42.66357
B - 200i	11/30/2020 13:24 overcast ewm yes yes	paved fair	14 125 yes drainage_system,oth	Electrical boxes seen up street. Two c	tch	y_bottom yes_25_50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe	18 12 18	0 fair none	sides of pipe due to water. Minor	inlet connects as part of a closed drainage system. General tree no growth. fair -70.964 42.64836
	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	,	.,			,				Small piece of plywood propped up in front of outlet by a rock.	-
				Minor to moderate cracking along row Electrical box of seen up street. Two c	lway.					Small hole, likely created by animals, at top of plywood. Minor to moderate scaling at	Outlet is part of a closed drainage system. Outlet is boarded up by a sheet of plywood propped up by a
B - 200o	11/30/2020 13:42 overcast ewm yes yes	paved fair	14 125 yes drainage_system,oth		round skewed_>45° no_discernable_stream_channel no_dry_	y_bottom yes_50_75%	downstream other at_stream_grade reinforced_concrete_pipe	12 4 0	0 fair none	bottom of pipe. headwall_retaining_wall.slo 1.5 satisfactory dry_laid_stone_brick fair Moderate tree growth and no	no rock. fair -70.9638 42.64802
										Pipe lengths are shifted away from each other and upwards	
										and downwards in certain locations. Between first and second lengths there is a	
				Moderate cracking and small pothole						moderate amount of sediment buildup at base of pipe. Minor	
B - 356i	11/30/2020 14:13 overcast ewm yes yes	paved fair	11 65 yes overhead_wires	no throughout roadway.	round skewed_<45° skewed_>45° no_dry_	y_bottom yes_<25%	within_culvert sediment at_stream_grade reinforced_concrete_plpe	12 11 6	0 poor none	3 scaling throughout pipe. slope_steeper_than_2:1_ 5.5 fair Moderate erosion of emba no Minor scaling at bottom of pipe.	no poor -70.9629 42.6573 Driveway culvert outlets the same
							vegetation_growt			Fallen tree limb/l/og directly in front of culvert impeding direct	area. Essex County bound located directly in front of outlet. Culvert
B - 356o	11/30/2020 14:28 overcast ewm yes yes	paved fair	11 65 yes overhead_wires	no Moderate cracking and potholes and	no_dry_ skewed_>45° stream_aligned no_dry_	y_bottom yes_50_75%	h,sediment,leaves, downstream other at_stream_grade reinforced_concrete_pipe	12 12 0	0 fair moderate culvert	inspection of pipe.	pipe length goes directly under a no utility pole. poor -70.9631 42.65706
										Minor scaling at bottom sides of pipe due to water. On top left	
				Minor cracking long edges of roadway			sediment, leaves, b			side of opening there is exposed rebar. Minor moss growth	Culvert is part of a close drainage system that's connected by two
B - 198o	11/30/2020 15:08 overcast ewm yes yes	paved satisfactory	11 55	no regulator seen up beginning of roadwa	 round roadway_aligned no_discernable_stream_channel no_dry_ 	y_bottom yes_50_75%	within_culvert ranches at_stream_grade reinforced_concrete_plpe	18 9 17	0 satisfactory none	outside of pipe. slope_steeper_than_2:1_ 5 poor Moderate to severe erosion o Second length of pipe is shifted	no catch basins. fair -70.9677 42.64829 Fallen trees and limbs throughout
				Minor cracking long edges of roadwar			sediment,leaves,b			Section reign on pipe is sinted downwards and away approximately 3 inches with	inlet area. Culvert is part of a close drainage system connected by two
B - 198i	11/30/2020 15:20 overcast ewm yes yes	paved satisfactory	11 55 yes overhead_wires,drai	nage_system,gas_no regulator seen up beginning of roadwa	round skewed_<45° no_discernable_stream_channel no_dry_	y_bottom yes_25_50%	within_culvert ranches at_stream_grade reinforced_concrete_pipe	18 14 16	0 poor none		no catch basins. poor -70.9676 42.64815
										Minor scaling at bottom of pipe due to water. All pipe lengths are shifted away from each other shifted away from each other	Culvert as part of a closeddrainage system connected by two catch
B - 357o	11/30/2020 15:44 rain ewm yes yes	paved satisfactory	2 60 yes overhead_wires	no Minor cracking along edges of roadwa	round roadway_aligned no_discernable_stream_channel no_dry_	y_bottom yes_<25%	within_culvert leaves,sediment at_stream_grade reinforced_concrete_pipe	12 12 8	0 poor minor headwall	with sediment seeping through. headwall_retaining_wall_slo 3.5 poor mortared_stone_brick falling Moderate erosion with sev no	no basins . failing -70.9688 42.64767 Culvert is part of a closed drainage
											system connected by to catch basins. Inside of culvert is uninspectable due to collapse
B - 357i	11/30/2020 15:57 rain ewm yes no	paved satisfactory	12 60 yes drainage_system,ovi	erhead_wires no Minor cracking along edges of roadw.	r. round skewed_<45° no_discernable_stream_channel	completely obstru	sediment,leaves,o ctrupstream ther buried_clogged_col.corrugated_metal_pipe	12		headwall_retaining_wall_slope_steeper_t fair dry_laid_stone_brick failing Embankment shows minor no	headwall directly in front of no opening. falling -70.9687 42.64767
						competery_dution	treupsteam the bureo_clogged_cor.cortugateo_metal_pipe	**		Second length of pipe shifted	
				Roadway is the on ramp to I-95. Unat roadway and culvert length measuren	t to get	compacey_outro	terupuream uiei <u>tureu_ruggeu_rur curugateu_metar_p</u> ipe				CMP drainage outlet located directly next to inlet. General fallen
B - 196i	11/30/2020 16:32 rain ewm yes yes	paved satisfactory	yes drainage_system,gar	roadway and culvert length measuren	t to get	g yes_<25%	within_culvert sediment at_stream_grade reinforced_concrete_pipe	30 29 20	3 fair none	Second length of pipe shifted approximately 2 inches downwards Mosteriate scaling on downwards Mosteriate scaling on downwards Mosteriate scaling on downwards Mosteriate scaling on downwards on downwards on downwards on downwards on downwards	CMP drainage outlet located directly next to inlet. General fallen no trees and limbs. falr -70.9754 42.65017
B - 196i	11/30/2020 16:32 rain ewm yes yes	paved satisfactory	yes drainage_system.ga:	roadway and culvert length measuren is_line yes guardrail satisfactor, to heavy traffic.	s to get tis die round skewed_>45° stream_sligned flowing			30 29 20	3 fair none	Second length of pipe shifted approximately 2 inches to describe the programment of the p	directly next to inlet. General fallen no trees and limbs. fair -70.9754 42.65017
8 - 196i 8 - 196o		paved satisfactory paved satisfactory	yes drainage_system.ga: yes gas_line.drainage_sy	Lline yes guardrall satisfactory to heavy traffic. Roadway at the en ramp to 1-95. Unat roadway and culvert length measurem	s to get tis die round skewed_>45° stream_sligned flowing			30 29 20 30 12 30	3 fair none	Second length of pipe shifted approximately 2 inches to downwards Moderate scaling on bottom of pipe. Spondic patches of missing concrete, shoped, stepter, than 2:1_he 10 fair concrete_poured_ poor Moderate erosion along en no Opening is a flared and section.	directly next to inlet. General failen no trees and limbs. fair -70.9754 42.65017 RCP drainage outlet located approximately 5 feet away from no culver rougher. fair -70.9757 42.6496
B - 1960 B - 1960 B - 201 B - 3588		paved satisfactory paved satisfactory paved satisfactory		Lline yes guardrall satisfactory to heavy traffic. Roadway at the en ramp to 1-95. Unat roadway and culvert length measurem	t to get tts due round skewed_345° stream_aligned flowing t to get tins due	yes_<25%	within_culvert sediment at_stream_grade reinforced_concrete_pipe	30 29 20 30 12 30 36 36 20		Second length of pipe shifted approximately 2 inches downward. Moderate scaling on bottom of pipe. Spondic patches of bottom of pipe. Spondic patches of missing concrete. Spondic patches of concrete poured poor Moderate erosion along en no Opening is a fluered end section. Large amount of water and section. Large amount of water and section. Section 1 of pipe. Spondic patches pipe. Spondic patc	directly next to inlet. General fallen trees and limbs. RCP drainage outlet located approximately 5 feet away from
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	11/30/2020 16:46 rain ewm yes yes 11/30/2020 17:07 rain who culvert_was_not_located_d_no 11/30/2020 17:11 rain ewm yes yes 11/30/2020 17:24 rain ewm yes no 11/30/2020 17:41 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory	yes gas_line,drainage_n 2 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires,sew	Line yes guardrall satisfaction to heavy traffic. Roadway and culvert length measurem Roadway and culvert length measurem roadway and culvert length measurem ro guardrall satisfaction to heavy traffic. ro ro ro sever manhole seen on roadway, po	to get mis due round skewed_45° stream_aligned flowing to get mis due round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round roadway_aligned skewed_45° flowing round roadway_aligned stream_aligned flowing round skewed_45° stream_aligned flowing	16 yes25% 16 yes50_75% 16 yes425% 16 no	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe parches_leaves_stream_grade reinforced_concrete_pipe branches_leaves_stream_grade reinforced_concrete_pipe upstream at_stream_grade reinforced_concrete_pipe	30 12 30 36 36 20 30 30 20 36 36 17	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none	Second length of pipe shifted approximately 2 inches will provide the provided of the pipe shifted approximately 2 inches will provide a provided provided by the provided pro	no directly not to inite. General fallen trees and limbs. RCP drainage outlet located approximately 3 feet away from Culvert does not exist. Culvert does not exist. Culvert does not exist. Culvert does not exist. Satisfactory General fallen trees and limbs route stresses have been and limbs route route been and limbs route stresses have been and limbs route stresses have been and limbs route stresses have been and limbs route route been and limbs route stresses have been and limbs route route been and li
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B - 201 B - 2588 B - 2580 B - 2021	11/20/2020 16-46 rain ewm yes yes 11/20/2020 17:07 rain wnb 11/20/2020 17:11 rain ewm yes 11/20/2020 17:24 rain ewm yes no 11/20/2020 17:24 rain ewm yes no 11/20/2020 17:41 rain ewm yes yes 11/20/2020 17:51 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_ny 2 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires_sew	Liline yes guardrali satisfaction to heavy traffic. Roadway in the on ramp to 195. Unatter Roadway in the on ramp to 195. Unatter readway and culvert length measuren readway is the on ramp to 195. Unatter readway is the on readway and culvert length measuren readway is the on readway and culvert length measuren readway is the on readway and culvert length measuren readway is the on ramp to 195. Unatter readway and culvert length measuren readway is the on ramp to 195. Unatter readway and culvert length measuren readway is the on ramp to 195. Unatter readway and culvert length measuren readway is the on ramp to 195. Unatter readway and culvert length measuren readway an	to get rets due round skewed_45° stream_aligned flowing sto get resuld round skewed_45° stream_aligned flowing round roadway_aligned skewed_45° flowing round roadway_aligned stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing	yes_r25% yes_r250_75% yes_r250_75% yes_r25% yes_r25%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves_s perched reinforced_concrete_pipe upstream ediment at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe	30 12 30 36 36 20 30 30 20 36 36 17	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none	Second largely of pipe shifted downward. Moderate scaling on bottom of pipe, Sponder parches stope, Steeper_than_2:1_ht 10 fair concrete_pound_ poor Moderate resion along en no Opening is little and section. Large amount of water and section of pipe and section of pipe and pipe section of pipe are pipe section of pipe and pipe section of pipe and pipe section of pipe and pipe section pipe section of pipe and pipe section	mo directly next to infet. General faller tress and limbs tres
B - 201 B - 2588 B - 2580 B - 2021	11/20/2020 16-46 rain ewm yes yes 11/20/2020 17:07 rain wnb 11/20/2020 17:11 rain ewm yes 11/20/2020 17:24 rain ewm yes no 11/20/2020 17:24 rain ewm yes no 11/20/2020 17:41 rain ewm yes yes 11/20/2020 17:51 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_ny 2 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires_sew	Liline yes guardrall satisfaction to heavy radiculver length measurem Roadway in the on ramp to 195. Unattern yes guardrall satisfaction to heavy traffic. Roadway is the on ramp to 195. Unattern yes guardrall satisfaction to heavy traffic. no or guardrall satisfaction to heavy traffic. see r_line no Sewer manifold seen on roadway, por closed drainage system. Sewer manifold seen on roadway, por closed drainage system.	to get rets due round skewed_45° stream_aligned flowing sto get resuld round skewed_45° stream_aligned flowing round roadway_aligned skewed_45° flowing round roadway_aligned stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing	rg yes125% rg yes_50_75% rg yes_50_75% rg ves25% rg no rg yes_25_50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe branches_leaves_s ediment at_stream_grade reinforced_concrete_pipe branches_leaves_s ediment at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe	30 12 30 36 36 20 30 30 20 36 36 17 36 36 12	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none	Second length of pipe shifted approximately 2 inches will be a second length of pipe shifted approximately 2 inches will be a second length of pipe shifted approximately 2 inches will be a second length of pipe shifted approximately 3 feet of the second length of pipe shifted approximately 3 feet of the second length of pipe shifted approximately 3 feet of the second length of pipe shifted approximately 3 feet of the second length of pipe shifted length approximately 3 feet of the second length of pipe shifted approximately 3 feet of the second length of pipe shifted length approximately 3 feet of the second length of pipe shifted length approximately 3 feet of the second length of pipe shifted length approximately 3 feet of the second length of pipe shifted length approximately 3 feet of the special length in special length approximately 3 feet of the special length in special length approximately 3 feet of the special length in special length approximately 3 feet of the special length in special length approximately 3 feet of the special length in special length approximately 3 feet of the special length in special length approximately 3 feet of the special length a	mo directly next to infet. General fallen trees and limbs trees and limbs fair 70,9794 42,65017 RE7 drainage outlet located approximately 5 feet away from curver opening. 10,000 feet away from curver opening. 10,000 feet away from curver opening. 10,000 feet away from satisfactory 70,0375 42,6432 assistant polymorphisms. 10,000 feet away from curver outlet inspection of culturer away from curver outlet inspection of culturer away from curver outlet of the fair fair fair fair fair fair fair fair
B - 201 B - 2588 B - 2580 B - 2021	11/20/2020 16-46 rain ewm yes yes 11/20/2020 17-07 rain who culvert_was_not_located_d-no yes yes 11/20/2020 17-24 rain ewm yes no 11/20/2020 17-24 rain ewm yes yes 11/20/2020 17-51 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drahage_sy 22 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires,sew 14 55 yes overhead_wires,sew 14 52 yes overhead_wire_drai 15 yes overhead_wire_drai 16 50 yes overhead_wire_drai 17 yes overhead_wire_drai 18 50 yes overhead_wire_drai	Line yes guardrall satisfactory to heavy traffic. Roadway and culvert length measurem Roadway and culvert length measurem resterm yes guardrall satisfactory to heavy traffic. no regular no Sewer manifule seen on roadway, por closed drainage system no Minor cracking on roadway.	to get mis due round skewed_455* stream_aligned flowing round skewed_465* stream_aligned flowing round round skewed_465* stream_aligned flowing round round-round skewed_455* flowing round skewed_465* stream_aligned flowing round skewed_465* stream_aligned flowing round skewed_465* stream_aligned flowing round skewed_465* no_discernable_stream_channel no_dry_ round skewed_465* no_discernable_stream_channel stagnam	16 yes25% 18 yes_50_75% 16 yes425% 16 no 16 yes25,50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe	30 12 30 30 36 20 36 36 17 36 36 12 12 7 0	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 falling none	Second largeth of pipe shifted downward. Moderate to saling on bottom of pipe, Sponder parches slope_steeper_than_2:1_ht 10 fair concrete_pound_ poor Moderate revision along en no Opening is fair slope_steeper_than_2:1_ht 10 fair concrete_pound_ poor Moderate revision along en no Opening is fair slope_steeper_than_2:1_ht 10 poor Moderate revision along en no Opening is fair slope_steeper_than_2:1_ 10 poor Moderate to severe erosion no Solven incorrete at opening. 4 headwall_retaining_wall.slo 1.5 fair concrete_pound_ satisfactory Minor to moderate erosion no Moderate to severe erosion no Moderate to severe erosion no Solven severe. 5 fair concrete_pound_ fair Severe erosion throughout no Minor scaling at bottom of pipe headwall_retaining_wall.slo 2 fair concrete_pound_ good Embankment shows minor no Discoloration of concrete_steeper_than_2:1_ 2 fair concrete_pound_ good Embankment shows minor no Discoloration of concrete Strong-poung of pipe is best appointantly? I feel into the pipe large team of let side allowing sediment to seep through_poundate() and the pipe large team of let side allowing sediment to seep through_poundate() and the pipe large team of let side allowing sediment to seep through_poundate() and the rest is dominantly. The limits the rest is dominantly. The limits the rest is dominantly feel of the season of pipe. Separated appointantly is feel of the season of pipe. Separated appointantly is feel of the season of pipe. Separated appoint pipe is them.	mo directly next to infet. General faller trees and limbs trees and limbs could be cared approximately 5 feet away from curver opening. RCP drainage outlet located approximately 5 feet away from curver opening. Curver to gening. General fallers trees and limbs route stream head Severe curling along the country of the
B - 201 B - 2588 B - 2580 B - 2021	11/20/2020 18-46 rain ewm yes yes 11/20/2020 17-97 rain wnb 11/20/2020 17-11 rain ewm yes yes 11/20/2020 17-24 rain ewm yes no 11/20/2020 17-24 rain ewm yes yes 11/20/2020 17-24 rain ewm yes yes 11/20/2020 17-51 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_ny 22 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires_sew 14 55 yes overhead_wires_sew 14 52 yes overhead_wires_sew	Liline yes guardrall satisfaction to heavy radiculver length measurem Roadway and culvert length measurem roadway and cu	to get residue round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round roadway_aligned skewed_45° flowing round roadway_aligned stream_aligned flowing round skewed_45° stream_aligned flowing	yes25% yes250,75% yes25% yes25% yes25% no no	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe	30 12 30 36 36 20 30 30 20 36 36 17 36 36 12	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none	Second largith of pipe shifted downward. Moderate to saling on bottom of pipe, Sponder parches stope_steeper_than_2:1_ht 10 fair concrete_pound_ poor Moderate revision along en no Opening is filled and section. Large amount of water and sediment obstructed full observation of culters. Micro: 1 Object in concrete at opening. 1 Object in concrete at opening. 2 Object in concrete at opening. 3 Object in concrete at opening. 4 Indicate the section of pipe headwall_retaining_wall.slo 2 failing concrete_pound_ fair Severe erosion on throughout no Micro scaling at bottom of pipe. Headwall_retaining_wall.slo 2 failing concrete_pound_ good Embankment shows minor no Discoloration of concrete 8 throughout pipe. Headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Bottom congruing of pipe is best. Discoloration of concrete 8 throughout pipe. Headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Bottom congruing of pipe is best. Approximately 2 feel into the pipe large team of left side allowing sediment to seep through_Approximately 2 feel into the pipe large team of left side allowing sediment to seep flatter_than_2:1_ 1.5 poor Moderate to severe erosion no flow while the rest is domewards. Tar mix. 5 Second length of pipe is specified than 2:1_ 4 poor Moderate to severe erosion no flower erosion no fipe. Second length of pipe is specified and pipe of the part and approximately 2 feel into the pipe specified and pipe is specified and pipe is specified and pipe is specified pipe in second length of pipe is specified pipe pipe specified pipe in second length of pipe is specified pipe in second length of pipe is specified pipe pipe spe	mo directly next to inlef. General faller RCP drainage outlet located approximately 5 feet away from Cultert does not exist. RCP drainage outlet located approximately 5 feet away from Cultert does not exist. RCP drainage outlet located approximately 5 feet away from Dank, Direct impection of culvert awa inaccusted due to steep part. Direct impection of culvert awa inaccusted due to steep approximately 1 feet away from Dank Direct impection of culvert awa inaccusted due to steep part. PC part of due to the control of culvert away from the control of culter of
B - 201 B - 2588 B - 2580 B - 2021	11/20/2020 16-46 rain ewm yes yes 11/20/2020 17-07 rain who culvert_was_not_located_d-no yes yes 11/20/2020 17-24 rain ewm yes no 11/20/2020 17-24 rain ewm yes yes 11/20/2020 17-51 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drahage_sy 22 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires,sew 14 55 yes overhead_wires,sew 14 52 yes overhead_wire_drai 15 yes overhead_wire_drai 16 50 yes overhead_wire_drai 17 yes overhead_wire_drai 18 50 yes overhead_wire_drai	Line yes guardrall satisfactory to heavy traffic. Roadway and culvert length measurem Roadway and culvert length measurem resterm yes guardrall satisfactory to heavy traffic. no ro or Jine no Sewer manifule seen on roadway, por closed drainage system no Minor cracking on roadway.	to get mis due round skewed_455* stream_aligned flowing round skewed_465* stream_aligned flowing round round skewed_465* stream_aligned flowing round round-round skewed_455* flowing round skewed_465* stream_aligned flowing round skewed_465* stream_aligned flowing round skewed_465* stream_aligned flowing round skewed_465* no_discernable_stream_channel no_dry_ round skewed_465* no_discernable_stream_channel stagnam	16 yes25% 18 yes_50_75% 16 yes425% 16 no 16 yes25,50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe	30 12 30 30 36 20 36 36 17 36 36 12 12 7 0	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 falling none	Second largeh of pipe shifted downward. Moderate to saling on bottom of pipe, Sponder parches slope_steeper_than_2:1_ht 10 fair concrete_poured_ poor Moderate revision along en no Opening is filled and section. Large amount of water and sediment obstructed full observation of culters. Micro: 1 Office in the pipe shifted and section. Large amount of water and sediment obstructed full observation of culters. Micro: 2 Organic part is disperted and section. Large amount of water and sediment obstructed full observation of culters. Micro: 3 Organic part is disperted and section. Large amount of water and sediment obstructed full observation of culters. Micro: 4 Indicate the section of concrete section of pipe headwall_retaining_wall_slo 2 failing concrete_poured_ good Embankment shows minor no Discoloration of concrete. Poured_ good Embankment shows minor no Discoloration of concrete as throughout pipe. Headwall_retaining_wall_slo 2 failing dry_laid_stone_brick failing Severe erosion on embands no Brottom comping of pipe is bent approximately 2 feel into the pipe large team on left side allowing sediment to seep through_Approximately 2 feel into the pipe large team on left side allowing sediment to seep through_Approximately 2 feel into the pipe large team of left side allowing sediment to seep through_Approximately 2 feel into the pipe large team of side allowing sediment to seep through_Approximately 2 feel into the pipe large team of side allowing sediment to seep side pipe large team of side allowing sediment to seep side pipe large team of side allowing sediment to seep side pipe large team of side allowing sediment to seep flatter_than_2:1_ 4 poor Moderate to severe erosion no Moderate to severe erosion no fipe. Second largeh of pipe is side side side side side side side	mo directly next to infect General faller trees and limbs to test and limbs could be controlled by a feet away from culter topening. General fallers trees and limbs route stream head. Severe curring along the control of the country of the countr
B - 201 B - 2588 B - 2580 B - 2022 B - 2020 B - 2066 B - 2066	11/30/2020 18-66 rain ewm yes yes culvert_was_not_located_e no yes wm yes yes 11/30/2020 17-21 rain ewm yes not_located_e no yes 11/30/2020 17-24 rain ewm yes no 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 18-21 rain ewm yes yes 11/30/2020 18-12 rain ewm yes yes 11/30/2020 18-12 rain ewm yes yes 11/30/2020 18-12 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drahage_sy 22 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires,sew 14 55 yes overhead_wires,sew 14 52 yes overhead_wire_drai 15 yes overhead_wire_drai 16 50 yes overhead_wire_drai 17 yes overhead_wire_drai 18 50 yes overhead_wire_drai	Line yes guardrall satisfactory to heavy traffic. Roadway and culvert length measurem Roadway and culvert length measurem resterm yes guardrall satisfactory to heavy traffic. no ro or Jine no Sewer manifule seen on roadway, por closed drainage system no Minor cracking on roadway.	to get residue round skewed_45° stream_aligned flowing round roadway_aligned stream_aligned flowing round roadway_aligned stream_aligned flowing round skewed_45° no_discernable_stream_channel no_dry_ round skewed_45° no_discernable_stream_channel stagnan round skewed_45° no_discernable_stream_channel stagnan round skewed_45° stream_aligned stagnan	16 yes25% 18 yes_50_75% 16 yes425% 16 no 16 yes25,50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_netal_pipe tal_stream_grade reinforced_concrete_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe	30 12 30 30 36 20 36 36 17 36 36 12 12 7 0	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 3 satisfactory none 0 failing none 0.5 fair minor culvert 8 none	Second length of pipe shifted spipe minds (2 inches) and the state of the spiritude of the	no directly not to indet. General faller trees and limbs. RCP drainage outlet located approximately 6 feet away from colored approximately 6 feet away and downgrading from colored approximately 6 feet away and downgrading from colored approximately 6 feet away for form colored approximately 6 feet away feet approximately 6 feet away feet approximately 6 feet away for from colored approximately 6 feet away for colored approximately 6 feet away
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B - 201 B - 2588 B - 2580 B - 2022 B - 2020 B - 2066 B - 2066	11/30/2020 18-66 rain ewm yes yes culvert_was_not_located_e no yes wm yes yes 11/30/2020 17-21 rain ewm yes not_located_e no yes 11/30/2020 17-24 rain ewm yes no 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 18-21 rain ewm yes yes 11/30/2020 18-12 rain ewm yes yes 11/30/2020 18-12 rain ewm yes yes 11/30/2020 18-23 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_sy 2 35 yes overhead_wires 10 35 no 14 55 yes overhead_wires,cow 14 55 yes overhead_wires,cow 14 50 yes overhead_wires,das 14 50 yes overhead_wires,das 14 50 yes overhead_wires,das 14 50 yes overhead_wires,das	Line yes guardrall satisfactor, to heavy traffic. Roadway and culvert length measurem Roadway and culvert length measurem Roadway and culvert length measurem readway and cul	to get residue round skewed_45° stream_aligned flowing round roadway_aligned stream_aligned flowing round roadway_aligned stream_aligned flowing round skewed_45° no_discernable_stream_channel no_dry_ round skewed_45° no_discernable_stream_channel stagnan round skewed_45° no_discernable_stream_channel stagnan round skewed_45° stream_aligned stagnan	yes25% yes25% yes25% yes25% yes25% yes25% no yes25,50% yes25,50%	within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves_at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_metat_pipe branches_sedimen within_culvert Lother at_stream_grade reinforced_concrete_pipe downstream_with sediment_branche in_culvert s submerged reinforced_concrete_pipe downstream_with sediment_branche in_culvert s submerged reinforced_concrete_pipe downstream_with sediment_branche in_culvert s submerged reinforced_concrete_pipe	30 12 30 36 36 20 30 36 36 17 36 36 12 12 7 0 10 9 8 10 10 9 10	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 3 satisfactory none 0 failing none 0.5 fair minor culvert 8 none	Second largeth of pipe shifted downward. Moderate scaling on bottom of pipe, Sporder partners stope, Exepper_than_2-1_ht 10 fair concrete_poured_ poor Moderate revision along en no Concrete_poured_ poor Moderate to severe erosion no Large amount of water and concrete_moderate revision no Large amount of water and concrete_poured_ poor Moderate to severe erosion no Moderate to severe erosion no Concrete_poured_ poor Moderate to severe erosion no Moderate to severe erosion no Concrete_poured_ poor Moderate to severe erosion no Concret	mo directly not to indet. General faller trees and limbs. fair 70,9794 42,65017 RCF drainage outlet located approximately 6 feet away from control fair 70,9795 42,6499. The control fair 70,9795 42,6499. The control fair 70,9795 42,64787 Culhert does not exist. Satisfactory 70,9795 42,64787 General fallers trees and limbs roade stream back Soerce cutting loop from control fair fair 70,9795 42,64787 Satisfactory 70,9795 42,64787 Beautiful fair fair fair fair fair fair fair fair
B - 201 B - 2588 B - 2580 B - 2022 B - 2020 B - 2066 B - 2066	11/20/2020 18-06 rain ewm yes yes 11/20/2020 17-07 rain who culvert_was_not_located_d-no yes yes 11/20/2020 17-24 rain ewm yes no 11/20/2020 17-24 rain ewm yes yes 11/20/2020 17-24 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes 11/20/2020 18-23 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes ges_line_drainage_system 22	Line yes guardrall satisfactor, to heavy radiculver length measurem Roadway in the contain to 195. Unatter Roadway in the on ramp to 195. Unatter Roadway in the one ramp to 195. Unatter Roadway in the on ramp to 195. Unatter Roadway in the orange to 195.	to get mis due round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round roadway_aligned skewed_45° flowing round skewed_45° stream_aligned flowing round skewed_45° no_discernable_stream_channel stagnam round skewed_45° stream_aligned stream_channel stagnam round skewed_45° stream_aligned stream_channel flowing round skewed_45° stream_aligned stream_channel flowing round roadway_aligned stream_aligned flowing flowing round skewed_45° no_discernable_stream_channel flowing flowing round skewed_45° no_discernable_stream_channel flowing	yes25% yes25% yes25% yes25% yes25% no yes25,50% no yes25,50% ant yes25,50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe downstream_with sediment_branche lin_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe	30 12 30 36 20 36 36 17 36 36 12 12 7 0 10 9 8 10 10 9 10	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none 0.5 fair minor cubert 8 none 0 failing severe cubert	Second length of pipe shifted specimizely 2 inches in control region of pipe shifted specimizely 2 inches. Micros Sport	mo directly not to indet. General faller trees and limbs. RCP drainage outlet located approximately 5 feet away from Culvert does not exist. RCP drainage outlet located approximately 5 feet away from Culvert does not exist. RCP drainage outlet located approximately 5 feet away from Culvert does not exist. RCP drainage outlet located approximately for the way from Culvert does not exist. RCP drainage outlet located approximately 1 food sway from Culvert does not exist. RCP drainage outlet located approximately 1 food sway from Culvert does not exist. RCP drainage outlet located approximately 1 food sway from Culvert does not exist. RCP drainage outlet located approximately 1 food sway from Culvert does not not not sway from Culvert does not not not sway from Culvert for the Culvert located approximately feet away from Culvert popening. For located approximately feet away from Culvert popening. Railing 70,9722 42,64458 Wettered file Josend nearby. Culvert located approximately feet away from Culvert comprised from Culvert popening. Railing 70,9723 42,64448 Wettered file Josend nearby. Culvert located approximately feet away from Culvert comprised of two pages with pipe 1 being closest to Perley. Culvert comprised of two pipes.
B - 201 B - 2588 B - 2580 B - 2022 B - 2020 B - 2066 B - 2066	11/20/2020 16-66 rain ewm yes yes yes 11/20/2020 17:07 rain who cubert_was_rot_located_e no yes yes 11/20/2020 17:07 rain ewm yes no 11/20/2020 17:24 rain ewm yes no 11/20/2020 17:24 rain ewm yes yes 11/20/2020 17:51 rain ewm yes yes 11/20/2020 18:12 rain ewm yes yes 11/20/2020 18:23 rain ewm yes yes 11/20/2020 18:39 rain ewm yes yes 11/20/2020 18:31 rain who cubert_was_rot_located_e no 12/1/2020 18:12 overcast ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_system_cd 22	Line yes guardrall satisfactor, to heavy radiculver length measurem Roadway in the contain to 195. Unatter Roadway in the on ramp to 195. Unatter Roadway in the one ramp to 195. Unatter Roadway in the on ramp to 195. Unatter Roadway in the orange to 195.	to get residue round skewed_45° stream_aligned flowing round roadway_aligned skewed_445° flowing round roadway_aligned stream_aligned flowing round skewed_45° no_discernable_stream_channel stagnan round skewed_45° no_discernable_stream_channel stagnan round skewed_45° stream_aligned stagnan round roadway_aligned stream_aligned so_dry_ round roadway_aligned stream_aligned flowing	yes25% yes25% yes25% yes25% yes25% no yes25,50% no yes25,50% ant yes25,50%	within_culvert sediment at_stream_grade reinforced_concrete_pipe within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe within_culvert t_other at_stream_grade reinforced_concrete_pipe downstream_with sediment_branche lin_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert s submerged reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe	30 12 30 36 20 36 36 17 36 36 12 12 7 0 10 9 8 10 10 16 18 0 602 59 482	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none 0.5 fair minor cubert 8 none 0 failing severe cubert	Second largeth of pipe shifted synometry. Moderate saling on bottom of pipe, Sporder partners stope, Everyor, than 2-1_ht 10 fair concrete_poured_ poor Moderate rosion along en no Concrete_poured_ poor Moderate rosion no Concrete_poured_ poor Moderate rosion no Concrete_poured_ poor Moderate rosion no pope pour pour pour pour pour pour pour pour	mo directly not to indic. General faller trees and limbs. REP drainage outlet located approximately 5 feet away from colored faller trees and limbs rode streems between the streems and limbs rode streems and limbs rode streems between the streems and limbs rode streems rode st
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B - 201 B - 3580 B - 3580 B - 2021 B - 2020 B - 2046 B - 2046 B - 2046 B - 210 B - 1780	11/30/2020 18-66 rain ewm yes yes culvert_was_rot_located_e no yes yes 11/30/2020 17-07 rain who yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-21 rain ewm yes yes 11/30/2020 17-51 rain ewm yes yes 11/30/2020 18-32 rain ewm yes yes 11/30/2020 18-39 rain ewm yes yes 11/30/2020 18-39 rain ewm yes yes 11/30/2020 18-31 rain ewm yes yes 11/30/2020 18-31 rain ewm yes yes 11/30/2020 18-32 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_system_ot 2	Line yes guardrall satisfactor, to heavy traffic. Roadway and culvert length measurem readway and culvert length measurem guardrall satisfactor, to heavy traffic. Roadway and culvert length measurem readway traffic. Sewer manifole seen on roadway, por closed drainage system. Sewer manifole seen on roadway, por closed drainage system. Minor cracks along roadway. Minor cracks along roadway. Minor cracks along roadway. Minor cracking on roadway. Minor cracking on roadway. Minor cracks along roadway. Minor cracking on roadway. Minor cracking on roadway. Minor cracking on roadway. Minor cracking on roadway. Minor cracking along roadway. Minor cracking along roadway. Top or extends up approximately 2 feet functions and satisfactor, percent up along roadway. Top or extends up approximately 2 feet functions and satisfactor, percent up along roadway. Top or extends up approximately 2 feet functions and satisfactor, percent up along roadway. Top or extends up approximately 2 feet functions and satisfactor, percent up along roadway. Top or extends up approximately 2 feet functions and satisfactor, percent up along roadway.	to get residue round skewed_45° stream_aligned flowing round roundway_aligned stream_aligned flowing round roundway_aligned stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° stream_aligned flowing round skewed_45° no_discernable_stream_channel stagnan round skewed_45° no_discernable_stream_channel stagnan round skewed_45° stream_aligned stagnan round skewed_45° stream_aligned stagnan round roundway_aligned stream_aligned no_dry_ round roundway_aligned stream_aligned flowing round roundway_aligned stream_aligned flowing these these round roundway_aligned stream_aligned flowing these	yes25% yes50_75% yes25% yes25% yes25% no yes25,50% yes25_50% yes25_50% yes25_50%	within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe perched corrugated_metal_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe downstream.with sediment_branche is_culvert s submurged reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe sediment_branche	30 12 30 36 36 20 36 36 17 36 36 12 12 7 0 10 9 8 10 9 10 602 59 482 602 4553 552	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none 0 failing none 0 failing severe culvert 14 satisfactory none	Second largeth of pipe a lafter downward. Moderate scaling on bottom of pipe, Sponder partners stope, Steeper_than_2:1_ht 10 fair concrete_pound_ poor Moderate revision along em no Opening is fair discondered the steeper and sediment obstructed full observation of culters. Micro-calling at bottom of pipe, headwall_retaining_wall.slo 1.5 fair concrete_pound_ satisfactory Moderate to severe erosion on headwall_retaining_wall.slo 2 failing concrete_pound_ fair Severe erosion on embanks no Bottom or pipe headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Bottom or pipe headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Bottom or pipe headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Moderate to severe erosion on embanks no Bottom or pipe headwall_retaining_wall.slo 2 failing dry_laid_stone_brick failing Severe erosion on embanks no Moderate to severe erosion no embanks no Moderate to severe erosion no embanks no pipe so the pipe large team of the side allowing sediment to seep large team of the pipe is set to severe erosion no embanks no pipe sediment to seep severe erosion no embanks no pipe sediment to seep severe erosion no embanks no pipe sediment to seep severe erosion no embanks no pipe sediment to seep severe erosion no embanks no pipe severe erosion no embanks no pi	mo directly not to indet. General faller trees and limbs. REP drainage outlet located approximately 5 feet away from Collection of Collection
B - 201 B - 3580 B - 3580 B - 2021 B - 2020 B - 2046 B - 2046 B - 2046 B - 210 B - 1780	11/30/2020 18-66 rain ewm yes yes culvert_was_rot_located_e no yes yes 11/30/2020 17-07 rain who yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-24 rain ewm yes yes 11/30/2020 17-21 rain ewm yes yes 11/30/2020 17-51 rain ewm yes yes 11/30/2020 18-32 rain ewm yes yes 11/30/2020 18-39 rain ewm yes yes 11/30/2020 18-39 rain ewm yes yes 11/30/2020 18-31 rain ewm yes yes 11/30/2020 18-31 rain ewm yes yes 11/30/2020 18-32 rain ewm yes yes	paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory paved satisfactory	yes gas_line_drainage_system_ot 2	Line yes guardrall satisfactory to heavy traffic. Roadway and culvert length measurem tradition in the property traffic. Roadway is the on ramp to 195. Unatternated the property traffic. Roadway is the on ramp to 195. Unatternated the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway and culvert length measurem to readway in the control of the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the on ramp to 195. Unatternated to the property traffic. Roadway is the or ramp to 195. Unatternated to the property traffic. Roadway is the or ramp to 195. Unatternated to the property traffic. Roadway is the or ramp to 195. Unatternated to the property traffic. Roadway is the or ramp to 195. Unatternated to 195. Unat	to get residue round skewed_45° stream_sligned flowing round roadway_aligned skewed_45° flowing round roadway_aligned skewed_45° flowing round skewed_45° stream_sligned flowing round skewed_45° no_discernable_stream_channel stagnan round skewed_45° no_discernable_stream_channel stagnan round roadway_aligned stream_sligned stagnan round roadway_aligned stream_sligned flowing flowing round roadway_aligned no_discernable_stream_channel flowing flowing round roadway_aligned no_discernable_stream_channel flowing round roadway_aligned round roadway	yes25% yes50_75% yes25% yes25% yes25% no yes25,50% yes25,50% yes25,50% yes25,50%	within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe perched corrugated_metal_pipe perched reinforced_concrete_pipe perched corrugated_metal_pipe downstream.with sediment_branche is_culvert s submurged reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe sediment_branche	30 12 30 36 36 20 36 36 17 36 36 12 12 7 0 10 9 8 10 9 10 602 59 482 602 4553 552	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none 0 failing none 0 failing severe culvert 14 satisfactory none	Second length of pipe shifted approximately 2 inches some concrete. Stope, steeper_than_2:1_hz to fair concrete_poured_ poor Modernate recision along em no Destinate of pipe. Special partners stope, steeper_than_2:1_hz to fair concrete_poured_ poor Modernate recision along em no Destinate of pipe. Special partners stope, steeper_than_2:1_ 10 poor Modernate router and steel ment obstruction of culvert. Minor scaling at bottom of pipe. Incadewall_retaining_wall_slo	mo trees and limbs. fair 70,9794 42,65017 REP drainage outlet located approximately 5 feet away from Colored fores on crist. REP drainage outlet located supporting the feet of the colored fores on crist. REP drainage outlet located street on colored street in the Seven criting and street outlets. Seven criting and street outlets for the street outlets for the street outlets and street outlets. Seven criting and street outlets for the street outlets for the street outlets and street outlets for the
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B - 201 B - 2581 B - 2580 B - 2022 B - 2022 B - 2020 B - 2060 B - 2060 B - 2060 B - 21781 B - 1780	11/20/2020 18-06 rain ewm yes yes culvert_was_rot_located_d-no yes yes 11/20/2020 17-07 rain wnb culvert_was_rot_located_d-no yes 11/20/2020 17-24 rain ewm yes no 11/20/2020 17-24 rain ewm yes yes 11/20/2020 18-17 rain ewm yes yes 11/20/2020 18-17 rain ewm yes yes 11/20/2020 18-12 rain ewm yes yes 11/20/2020 18-23 rain ewm yes yes 11/20/2020 18-23 rain ewm yes yes 11/20/2020 18-30 rain ewm yes yes 12/1/2020 18-32 cvercast ewm yes yes 12/1/2020 18-32 cvercast ewm yes yes	paved satisfactory paved satisfactory	yes gas_line_drainage_system_cd 22	Line yes guardrall satisfactory to heavy rad culvert length measurem guardrall satisfactory to heavy traffic. Roadway and culvert length measurem transport of the satisfactory to heavy traffic. Roadway and culvert length measurem transport of the satisfactory to heavy traffic. Roadway and culvert length measurem transport traffic. Roadway and culvert length me	to get mis due round skewed_45° stream_sligned flowing round skewed_45° stream_sligned flowing round roadway_sligned skewed_55° flowing round skewed_45° stream_sligned stream_thannel stagnam round skewed_45° no_discernable_stream_thannel stagnam round roadway_sligned stream_sligned stagnam round roadway_sligned stream_sligned flowing round roadway_sligned stream_sligned flowing flowing round roadway_sligned ro_discernable_stream_thannel flowing round roadway_sligned ro_discernable_stream_thannel flowing round roadway_sligned	yes25% yes25% yes25% yes25,075% yes25,50% yes25,50% yes25,50% yes25,50% yes25,50% yes25,50%	within_culvert sediment_leaves at_stream_grade reinforced_concrete_pipe upstream branches_leaves at_stream_grade reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe proched reinforced_concrete_pipe proched corrugated_metal_pipe proched corrugated_metal_pipe proched corrugated_metal_pipe downtramm_with adment_branche in_colvert s submerged reinforced_concrete_pipe within_culvert branches_leaves at_stream_grade reinforced_concrete_pipe	30 12 30 36 36 20 36 36 36 17 36 36 12 12 12 7 0 10 10 9 8 10 10 16 18 0 602 59 482 602 4553 552 12 12 12 12 10 10 10 10 10 10 10 10 10 10 10 10 10	7 fair none 3 satisfactory none 3 satisfactory none 2 satisfactory none 1 satisfactory none 0 failing none 0 failing none 0 failing severe cubert 14 satisfactory none 14 satisfactory none	Second largeth of pipe shifted downward. Moderate scaling on bottom of pipe, good partners show a stope, steeper_than_2-1_ht to fair concrete_poured_ poor Moderate revision along en no Concrete_poured_ poor Moderate revision no steeper_than_2-1_ to poor to revision and the concrete_poured_ poor Moderate revision no Moderate revision no Moderate revision no Moderate revision no Concrete_poured_ poor to concrete_poured_ poor Moderate revision no Concrete_poured_ poor to Concrete_poured_ poor Moderate revision no Concrete_poured_ poor to Concret	mo trees and limbs. fair 70,9794 42,65017 RCP drainage outlet located approximately 6 feet away from control of the control o

8 - 209o	12/1/2020 15:30 overcast ewm	yes yes	paved	good	24	52 no	yes guardrail satisfactory	open_bottrskewed_>45" stream_aligned	flowing no	at_stream_grade reinforced_concrete_pipe	162 55 162 12 good none	8 Date stamp on concrete is 9-6-18 headwall_retaining_wall_slo Approximately 5 feet into the	3 satisfactory concrete_pre_cast_ good	General vegetation growth no no	Wetland flag located nearby. Fallen trees and limbs in stream. good -70.9921 42.64213
B = 222i	12/1/2020 15:53 overcast ewm	yes yes	paved	satisfactory	20	32 yes other	Electrical box seen up street. Mini no along roadway.	cracking elliptical skewed_<45° no_discernable_stream_c	hannel flowing no	at stream grade corrugated metal pipe	33 27 32 9 poor severe culvert	pipe there's a large dent inwards. Moderate amount of rust on sides of pipe due to water. headwall_retaining_wall,slo	1 fair mortared_stone_brick fair	Embankment shows minor yes no	Rusted great directly next to culvert, possibly beaver grate. fair -70.9874 42.63452
B - 222o	12/1/2020 16:05 overcast ewm	ves ves	paved	satisfactory	20	32 yes other	Electrical box seen up roadway. No along roadway.	or cracking elliptical skewed_<45° no_discernable_stream_c	hannel flowing no	at stream grade corrugated metal pipe	35 25 34 12 satisfactory moderate culvert	Moderate surface rust at bottom and sides of pipe due to water. headwall_retaining_wall,slo	1.5 failing mortared_stone_brick poor	Severe washout on right skyes no	poor -70.9873 42.63457
		,					Minor to moderate cracking along	sadway.							Culvent comprised of 2 - 10 floot wide corrugated metal pipes with a span of 24 feet. Pipe number one is closest to Middleton Boad. Large 6 floot trail culvent outlets to same
8 - 205o	12/1/2020 16:37 overcast ewm	yes yes	paved	satisfactory	22	30 no	yes guardrail fair Minor chips taken out of guard rai Minor to moderate cracking on ro	round roadway_allgned stream_allgned	flowing no	at_stream_grade corrugated_metal_pipe	1202 60 1202 24 good none	22 headwall_retaining_wall.slo 2: small lip in top of pipe	2 satisfactory concrete_poured_ satisfactory	y Embankment shows gener; yes no	stream. satisfactory -70.9901 42.64477 Cubert is comprised of 2-10 foot state companied may be with a special may be seen as the same of 24 foot state companied may be seen as the same of 24 foot state state to Middleton Road. Small area foom accumulation on right side of
B - 205i Location not found/d B - 14 not exist.	12/1/2020 17:01 overcast ewm es 12/7/2020 13:30 rain djn	yes yes	paved	satisfactory	20	30 yes overhead_wires	yes guardrail fair chips along guard rail.	round roadway_aligned skewed_>45°	flowing no	at_stream_grade corrugated_metal_pipe	1202 60 1202 36 good none	16 opening. headwall_retaining_wall,slo	2 fair concrete_poured_ satisfactor	y Embankment shows minor yes no	headwall. satisfactory -70.9902 42.64489 Culvert was not located/does not exist71.0658 42.72546
B - 15o	12/7/2020 13:46 rain ajs	yes no	paved	satisfactory	20	60 yes overhead_wires	no	round skewed_<45° stream_aligned	no_moist_bottom yes_<25%	within_culvert,do sediment,vegetati wnstream on_growth,leaves at_stream_grade corrugated_metal_pipe	18 none	Culvert could not be fully inspected due to heavy 13 vegetation with thorns. headwall_retaining_wall,slo	6 satisfactory mortared_stone_brick	Concrete bollard at top of h no no	Abutter noted sinkhole above culvert filled several years ago71.0663 42.72501
B - 15i	12/7/2020 14:27 clear ajs	yes yes	paved	satisfactory	20	60 no	no	round skewed_<45° stream_alligned	no_dry_bottom yes_25_509	leaves, sediment, d is within_culvert ebris_trash at_stream_grade corrugated_metal_pipe	18 13 18 0 satisfactory none	Rusting to high water mark, 100% section loss at first 3 feet of 13 bottom half of pipe. No flow.	7 satisfactory mortared_stone_brick satisfactory	y Sporadic areas of missing mno no	satisfactory -71.0665 42.72508
B - 13i	12/7/2020 14:43 rain ais	ves ves	paved	satisfactory	20	33 yes overhead wires	Minor edge failure and transverse no about pipe.	scking round roadway_aligned no_discernable_stream_c	nannel no dry bottom no	at stream grade corrugated metal pipe	12 12 0 0 good none	Low point on roadway swale. Minor surface rusting, and asphalt coating still present on bottom of pipe. slope_flatter_than_2:1hea	1 satisfactory mortared_stone_brick satisfactory	Transverse crack in headwa no no	satisfactory -71.0651 42.72633
		,		,			Minor edge failure and transverse	acking				Low point in roadway swale. Minor surface rusting, and asphalt coating still present on			4 to 8 inch diameter trees growing
B - 130 B - 6 Drainage outlet B - 5 Located over town lin B - 7 Located on private B - 8 Located on private	12/7/2020 15:08	yes yes	paved	poor	20	33 no	no about pipe.	round roadway_aligned no_discernable_stream_c	nannel no_dry_bottom yes_<25%	downstream leaves,sediment perched corrugated_metal_pipe	12 12 0 0 good none	bottom of pipe. slope_flatter_than_2:1_,hea	1.5 poor mortared_stone_brick satisfactor	y Evidence of mortar repair tx no no	within outlet channel. fair 77.0551 42.72604 77.0598 42.73194 77.0598 42.73194 77.0598 42.73122 77.0651 42.73167 77.0657 42.73121
							Rutting and fatigue cracking, trans	rse cracking			culverthe	Minor flattening of pipe, areas of localized buckling at outlet. Up to 24 inches of scour beneath pipe. Appears to be beaver dam			Erosion of downstream banks. Heavy vegetation/branches
B - 11ob	12/7/2020 15:18 rain ajs	yes yes	paved	poor	17	35 no	no over pipe. Rutting and fatigue cracking, trans	round roadway_aligned skewed_<45°	flowing yes_75_100	% within_culvert branches perched corrugated_metal_pipe within_culvert.do_sediment.leaves.b	24 21 8 1 poor severe adwall culvert,he	15 midpipe. slope_steeper_than_2:1_,ht Minor surface rusting throughout. Dammed mid pipe.	3 failing dry_laid_stone_brick failing	Stone missing between pipcyes no	downstream. failing -71.0695 42.72814
B - 110a	12/7/2020 15:47 rain ajs	yes yes	paved	poor	17	35 no	no over pipe.	round roadway_aligned skewed_<45°	stagnant yes_75_100		24 18 19 0 satisfactory severe adwall	13 Minor pipe flattening. headwall_retaining_wall.slo Buckling/crimping at end of	3 poor dry_laid_stone_brick poor	Stones missing between pip yes no	3.3 feet between outlets. poor -71.0695 42.72822
B - 11ia	12/7/2020 15:59 rain ajs	yes yes	paved	poor	17	35 yes overhead_wires	no	round roadway_aligned no_discernable_stream_c	hannel stagnant yes_75_100	sediment,branche % within_culvert s at_stream_grade corrugated_metal_pipe	24 24 23 10 poor minor her	protruded pipe. Minor interior surface rust. Minor flattening. Appears to be darn within pipe. slope_flatter_than_2:1_ Minor surface rusting. minor tear	2.5 poor	Washout between pipes. Myes no	Beaver screen present but not functioning. poor -71.0697 42.72822
B - 11ib Could not locate culve		yes yes	paved	poor	17	35 yes overhead_wires	Rutting and fatigue cracking, trans no over pipe.	rse cracking round roadway_aligned no_discernable_stream_c	nannel stagnant yes_75_100	sediment,leaves,b % within_culvert ranches at_stream_grade corrugated_metal_pipe	24 24 22 16 satisfactory minor culvert	in top of pipe, evidence of daming within pipe. slope_flatter_than_2:1_	3 satisfactory	Erosion between pipes. 4.3 yes no	Some dry stone surrounding pipe. satisfactory -71.0697 42.72813
on either side of road B - 9 Evidence of potential		culvert_was_not_located_does_no													-71.0699 42.72937
B - 4i	12/7/2020 16:27 overcast ajs	yes yes	paved	good	20	28 no	no	round roadway_aligned no_discernable_stream_c	nannel no_dry_bottom no	at_stream_grade ductile_iron_pipe vegetation_growt	12 12 0 0 good none	Culvert protrudes approximately 2ft from bank. slope_steeper_than_2:1_	1 satisfactory	Minor erosion on side of piç no no	Minor vegetation surrounding pipe. satisfactory -71.0765 42.73389 Vegetation and trees surrounding
B - 4o Next to failed 12 inch	12/7/2020 16:41 overcast ajs	yes yes	paved	good	20	28 yes overhead_wires	no	round roadway_aligned no_discernable_stream_c		downstream h at_stream_grade ductile_iron_pipe	12 12 0 0 good none	Outlets into protected wetland. slope_steeper_than_2:1_	1 satisfactory	Minor roadway washout. Ri no no	outlet. satisfactory -71.0765 42.73384 12 inch failed RCP located adjacent
B - 3i RCP.	12/7/2020 16:49 rain ajs	yes yes	paved	satisfactory	20	28 no	no Depression noted over failed RCP.	round skewed_<45° no_discernable_stream_c	nannel no_dry_bottom yes_<25%	within_culvert sediment perched ductile_iron_pipe	12 12 0 0 good none	Protruding pipe. slope_flatter_than_2:1_	1 satisfactory	Moderate vegetation grow no no	to DI pipe. satisfactory -71.074 42.73451
B - 30 B - 1 Located over town lin Potentially	12/7/2020 16:56 overcast ajs 12/7/2020 17:06 overcast djn	yes yes	paved	satisfactory	20	28 no	no Depression noted over failed RCP	round skewed_<45° skewed_<45°	no_dry_bottom yes_<25%	within_culvert,do_sediment,leaves,v wnstream egetation_growth at_stream_grade ductile_iron_pipe	12 12 0 0 good none	Surface rusting. slope_flatter_than_2:1_	1 satisfactory	Minor vegetation, riprap in no no	satisfactory -71.0739 42.73448 -71.0841 42.73639
B - 2o buried/removed culvi B - 2i	rt. 12/7/2020 17:15 rain djn 12/7/2020 17:18 rain djn	culvert_was_not_located_does_no culvert_was_not_located_does_no										Concrete box culvert with open			-71.0832 42.7346 -71.0836 42.73429
B - 10i	12/7/2020 17:49 overcast ajs	yes yes	paved	good	24	35 no	yes guardrail good	bax skewed_>45" skewed_<45"	stagnant yes_25_50%	sediment,leaves,b within_culvert ranches at_stream_grade other	36 25 36 11 poor none	bottom dapper. Concrete box culver for first 8 feet, dry laid stone clapper after. Scaling to high water. Sporadic spalling in concrete. slope_steeper_than_2:1_	4 poor	Erosion down embankment no no	Beaver screen present but not attached. poor -71.0776 42.72917
8 - 10o	12/7/2020 18:08 overcast ajs	yes yes	paved	good	24	35 yes overhead_wires	yes guardrali satisfactory	open_botts:kewed_>45° stream_aligned	flowing no	at_stream_grade dv_laid_stone	36 24 36 3 poor minor culvert	36 inch concrete box header, 5 foot extension from disperer. Clapper and concrete have separation between them. Concrete extension undermining on both sidensii Scaling to bit 7 water in concrete extension. sidensi policy sidensii sidensii sidens	4 poer	Significant erosion in embarno no	Vegetation/overgrowth downstream. Sound riprap armoring downstream. 71.0775 42.72508



Appendix B Structures Assessment Report

STRUCTURES ASSESSMENT REPORT

BRIDGES:

B-19-001 (899) - Middleton Road over Fish Brook

B-19-002 (8RE) – Mill Road over Fish Brook

B-19-003 (2R3) – Lockwood Lane over Fish Brook

B-19-004 (89A) – Fuller Lane over Fish Brook

B-19-005 (89B) – Endicott Road over Fish Brook

B-19-018 (C68) – Brookview Road over Fish Brook

IN

BOXFORD, MA

DATE OF INSPECTIONS: NOVEMBER 2020 & FEBRUARY 2021 PREPARED FOR:

TOWN OF BOXFORD

PREPARED BY:



TEC, INC. 146 DASCOMB ROAD ANDOVER, MA 01810

BRIDGE NO. B-19-001 (899) – MIDDLETON ROAD OVER FISH BROOK General

The bridge carries Middleton Road over Fish Brook and is located just south of the intersection at Middleton Road and Main Street (Figure 1). The bridge is situated on an S-shape curve. The superstructure is comprised of prestressed concrete beams, a cast in place concrete deck slab, and a hot mixed asphalt (HMA) wearing surface. The abutments appear to be dry-laid stones with a cast in place concrete beam seat, bearing on the dry-laid stones. (Figure 2). Based on 2019 data, this bridge is not listed on the National Bridge Inventory (NBI) in Massachusetts, however there is routine underwater inspection report that was performed at this location by MassDOT on December 28, 2018 (provided in Attachment A).



Figure 1: South bridge roadway approach (looking north) - B-19-001 (899)



Observations

The wearing surface on the bridge and at both bridge approaches appears to be sound. The edge of pavement to edge of pavement was measured to be approximately 21 feet wide with two, 10-footwide travel lanes. The out to out was measured as approximately 25.25 feet. Membrane waterproofing is present under HMA wearing surface and exposed at both fascia. There is no curb present.

The guardrail is a non-standard steel W-beam guardrail located on the west and east sides of the bridge. There is no approach guardrail transition nor an end-treatment. On both fascia's, there is one guardrail post that is embedded into the bridge. There appears to be impact damage, scuffs and rusting on the eastern guardrail. The western guardrail has minor scuffs and rusting (Figure 3).

There is severe embankment erosion covered by leaves at all four corners of the bridge. The northwestern embankment has an exposed gas line, just behind the guardrail. Overhead wires are located above the western embankment. Vegetation growth/overgrowth is present at the upstream (east) and the downstream (west) channel (Figure 4).

The prestressed concrete beams are labeled 1-4 ranging east to west. Beams 1 and 4 overhang the abutments at all four corners. The overhang varies anywhere from approximately 5-20 inches. The underside of beam 1 is wet. The east fascia of beam 4 has a small wet spot, surrounded by minor efflorescence and punky concrete. The wet spots are likely due to leakage from the roadway surface above. There is minor, sporadic rust staining throughout the entire underside of the beams. Generally, the beams appear to be in fair condition, equating to a 5 per the MassDOT bridge inspection condition rating guide ranging from 0-9.

The freeboard at the midspan of the bridge is approximately 30". The water depth at the upstream east fascia was about chest height (channel inspection inaccessible with waders only). The downstream (west) channel was the access point for a wader only inspection by TEC. The Fish Brook flows east to west with a moderate flow velocity.

The abutments consist of dry-laid fieldstones with a cast in place concrete beam seat that bears on the stones. The breastwalls and wingwalls consist of dry-laid, irregular fieldstones. There appears



to be several voids between dry-laid fieldstones throughout. At the west fascia of the north abutment (corner of breastwall and wingwall) there is a void beneath beam 1 that is up to approximately 3 feet deep. There is moderate to severe erosion/washout behind both abutments ranging from 9-20 inches, at all four corners (Figure 5). There is fill spilling through voids and missing chinking stones at the northwest wingwall. Generally, the substructure elements appear to be in fair condition, equating to a 5 per the MassDOT bridge inspection condition rating guide. Refer to Attachment A in the appendix for additional information on the substructure, provided in MassDOT's routine underwater inspection report.

Recommendations

To improve motorist safety, TEC recommends that standard guardrail should be installed to replace the non-standard, damaged guardrail in place currently on both sides.

All embankment slopes should be treated, to replace the eroded material and for preventative maintenance.

Minor repairs should be made to the abutments and wingwalls including readjusting/adding field stones and filling any voids with mortar.

TEC concurs with the condition ratings from MassDOT routine underwater inspection report from December 2018.

The following estimate includes construction costs for recommended repairs noted above (See Table 1 (pg. 4) for costing breakdown; quantities are approximate):

- Embankment Slope Repairs
 - o MassDOT Item 151.3 GRAVEL BORROW FOR SLOPE TREATMENT (30cy)
- Guardrail Replacement
 - o MassDOT Item 620.13 GUARDRAIL, TL-3 (SINGLE FACED) (300ft)
 - MassDOT Item 627.73 GUARDRAIL TANGENT END TREATMENT, TL-3 (2 each)
- Wingwall/Abutment Repairs
 - o MassDOT Item 692 BALANCE STONE WALL (100ft)
 - o MassDOT Item 106.251 CEMENT MORTAR FOR PATCHING (50cy)



Table 1: Cost estimate for repairs – B-19-001

Middleton Road over Fish Brook (B-19-001)
Cost Estimate	
Construction Costs	\$113,000
Safety Improvements (guardrail replacement)	\$90,000
Maintenance (embankment slope and field stone repairs)	\$23,000
Engineering Cost (survey, design, permitting, const. oversight)	\$45,000
Total	\$158,000





Figure 2: Bearing at the southwest corner - B-19-001



Figure 3: Impact damage to guardrail at northeast corner - B-19-001





Figure 4: Typical vegetation growth at northwest corner - B-19-001



Figure 5: Erosion/washout behind the north abutment (west fascia) - B-19-001



BRIDGE NO. B-19-002 (8RE) – MILL ROAD OVER FISH BROOK

General

The bridge carries Mill Road over Fish Brook and is located just south of the intersection at Mill Road and Mill Run Road (Figure 6). Generally, the topside and the underside of the bridge had minor deficiencies but was in overall good condition. This bridge was built in 2004 and is listed on NBI in Massachusetts as of 2019 data. A routine bridge inspection report was performed at this location by MassDOT on May 16, 2019 (provided in Attachment B).



Figure 6: Bridge roadway approach- B-19-002



Observations.

There are leaves accumulated along both gutter lines for the length of the bridge. An area at the southeast corner of the roadway has material accumulation and appears to be a spot for water to puddle. There is vegetation growth present on the bridge rail, wingwalls, and under all approach guardrail (Figure 7). There is debris/ potential tree growth directly upstream of bridge, at the midspan of the channel mouth.

Generally, the HMA wearing surface is in good condition, equating to a 7 per the MassDOT bridge inspection condition rating guide. There are transverse cracks in the wearing surface (up to \sim 3/4" wide) at what appears to be the north and south abutment deck joints.

The bridge rail is an S3-TL4 type bridge railing with concrete transitions. There is thrie beam transition guardrail connected to all four concrete transitions as well as approach guardrail with ends buried. The S3-TL4 bridge railing has minor sporadic scrapes to the paint full length, both sides (Figure 8). The northwest approach guardrail end treatment has impact damage and is detached from one of the timber posts (Figure 9).

TEC field measured the curb reveal as approximately 7" (west) and 7½" (east), which is consistent with the design plans. There is minor sporadic spalling along both curbs. There is a spall in the roadway face of the east curb measuring (~18"x3"x½") with minor sporadic honeycombing along the top surface. There is similarly sized spalling at the top of the southeast and northwest concrete transitions (~10"x4"x2") (Figure 10).

The breastwalls and wingwalls are precast concrete. All four wingwalls are U-wingwalls and contain some vegetation growth. Generally, the breastwalls and wingwalls are in good condition, equating to a 7 per the MassDOT bridge inspection condition rating guide. There is water staining on the breastwalls at both abutments beneath the east and west fascia girders.

TEC concurs with the condition ratings from the routine bridge inspection report performed by MassDOT in May 2019.



Recommendations.

Reattach the approach guardrail to the timber posts at the northwest corner. Repair and/or replace the northwest corner approach guardrail.

Perform minor concrete patch repairs to the spalls at the concrete transitions and both curbs using a concrete patching material.

Clear the roadway surface, bridge railing, and guardrail of any vegetation growth/debris buildup (typical bridge maintenance)

All these repairs are minor and can be readily made by the Town of Boxford DPW. Therefore, no cost estimate was prepared.





Figure 7: Typical leaf accumulation and vegetation growth under all four guardrail approaches and transitions- B-19-002



Figure 8: Typical minor sporadic scrapes to S3-TL4 bridge railing - B-19-002





Figure 9: Impact damage to northwest approach guardrail - B-19-002



Figure 10: Spalling on southeast concrete transition - B-19-002



BRIDGE NO. B-19-003 (2R3) – LOCKWOOD LANE OVER FISH BROOK General

The bridge carries Lockwood Lane over Fish Brook and is located east of the intersection at Lockwood Lane and Middleton Road. This bridge was built in 1988 and is a box culvert consisting of two corrugated aluminum arches, two cast in place concrete headwalls, and four stone wingwalls. There are timber bridge railings traveling the bridge on both sides (Figure 11). The Fish Brook flows north to south under the roadway. This bridge is not listed on the NBI in Massachusetts as of 2019 data, but there was a routine bridge inspection report performed at this location by MassDOT on January 11, 2019 (provided in Attachment C).



Figure 11: Bridge roadway approach (looking west) - B-19-003



Observations.

There is a parking area near the southeast bridge approach and a pedestrian bridge at the southwest bridge approach. There is some minor longitudinal pavement cracking for the length of the bridge and transverse pavement cracking at the east and west bridge joints (Figure 12). The curbs were inaccessible as there was snow build up at the north/south gutter lines, and under the timber bridge rail. After a follow-up visit, it was determined that there are no curbs present along the bridge on either side. The pavement appeared to be in fair condition, equating to a 5 per the MassDOT bridge inspection condition rating guide.

There is no approach guardrail nor end treatments at any of the four corners of the bridge. The timber bridge railing and timber posts exhibit minor wood checking throughout (Figure 13). There are overhead wires on the north embankment and there is a utility pole located at the northwest side near the bridge rail. The entire structure is surrounded by vegetation with some embankment erosion upstream (north) and downstream (south).

Generally, both headwalls look sound from a visual inspection (Figure 14). The four wingwalls appear to be stacked stones with some minor voids and misalignment (Figure 15). Due to the water depth/stream flow rate, the corrugated aluminum arches were unable to be inspected by TEC.

For a condition assessment of this culvert, please refer to Attachment C of this report.

TEC concurs with the condition ratings from the culvert inspection report performed by MassDOT in Jan 2019.



Recommendations.

To improve motorist safety, replace the timber bridge rail with standard guardrail and endtreatments.

To improve the structural integrity of bridge, realign wingwall stones and treat the north/south embankment slopes with rip rap.

Monitor the vegetation surrounding the bridge and the channel mouths. At the time of the inspection, the vegetation was not restricting the flow of water, but as growth continues, preventative maintenance measures may be necessary.

The following estimate includes construction costs for recommended repairs noted above (See Table 3 below for costing breakdown; quantities are approximate):

- Guardrail Replacement
 - o MassDOT Item 620.13 GUARDRAIL, TL-3 (SINGLE FACED) (250 ft)
 - MassDOT Item 627.73 GUARDRAIL TANGENT END TREATMENT, TL-3 (2 each)
- Wingwall Repairs
 - o MassDOT Item 983.1 RIPRAP (10 TON)
 - o MassDOT Item 692 BALANCE STONE WALL (100 ft)

Table 3: Cost estimate for repairs – B-19-003

Lockwood Lane over Fish Brook (B-19-003	
Cost Estimate	
Construction Costs	\$120,000
Safety Improvements (guardrail replacement)	\$90,000
Maintenance (wingwall repairs)	\$30,000
Engineering Cost	\$8,000
Total	\$128,000





Figure 12: Transverse pavement cracking at west bridge joint (looking south) - B-19-003



Figure 13: Typical wood checking in timber bridge railing (looking south) - B-19-003





Figure 14: Southern head wall and channel mouth (looking north) - B-19-003



Figure 15: Typical stone wingwall - B-19-003



BRIDGE NO. B-19-004 (89A) – FULLER LANE OVER FISH BROOK

General

The bridge carries Fuller Lane over Fish Brook and is located east of the I95 underpass that spans Fuller Lane. The bridge is situated on the Topsfield/Boxford town line (Figure 16). When traveling east, Fuller lane turns into River Road. This bridge was built in 1985 and is precast concrete culvert consisting of six adjacent precast concrete beams and cast in place concrete headwalls. The west breastwall appears to be concrete and the east breastwall appears to be a cast in place concrete slab atop stone masonry. The Fish Brook flows North to South. There is W-beam guardrail over the bridge and at approaches both sides, with no defined transitions or end-treatments. This bridge is not listed on the NBI in Massachusetts as of 2019 data, but there was a routine bridge inspection report performed recently at this location by MassDOT on May 11, 2020 (provided in Attachment D).



Figure 16: West bridge approach (looking east) - B-19-004



Observations

There are overhead wires and utility poles on the southern embankment slope. There is a utility gas line spanning the bridge east to west at the north bridge fascia. At the east end of the gas line there is a utility support. This was the only utility support in place at the time of inspection (Figure 17).

There is new pavement that begins at the east approach near the Topsfield/Boxford town line. The adjacent, older pavement has random longitudinal cracking, predominantly at the gutter lines. A small pothole is present near the center of the roadway and connects to smaller longitudinal cracks. There is some minor transverse cracking. Generally, the wearing surface was in satisfactory condition (Figure 18), equating to a 6 per the MassDOT bridge inspection condition rating guide.

The guardrail is bolted to the concrete headwall over the bridge, on both sides. There is typical cracking, delamination, and sporadic areas of spalled concrete surrounding the guardrail baseplates. There is minor rust staining and impact damage to both guardrails. The south guardrail appears to be missing a post (Figure 19).

The south headwall was measured to be approximately, ~ 17 " wide by ~ 21 '-4" long. There are minor sporadic areas of spalls, cracks, and efflorescence along both headwalls. There is a spall in the south headwall that measures to be $\sim (4' \text{ long by 4" wide by 2" deep})$. There is a horizontal crack at the north headwall fascia measuring $\sim (5' \text{ long by } \frac{1}{4}\text{" wide})$. On the south headwall, there is a concrete spall and exposed rusted rebar at the west fascia (Figure 20).

A south channel inspection was inaccessible with waders due to the water depth. A north channel inspection was limited due to the water depth. There was apparent undermining at the southwest wingwall adjacent to the west abutment (~3' wide by 1' tall by 2' deep).

Please refer to the May 2020 MassDOT routine inspection report provided in Attachment D, for additional information.

TEC concurs with the condition ratings from the routine inspection report performed by MassDOT in May 2020.



Recommendations

Remove and replace the top 7 inches of the headwall on both sides with cast in place concrete. Much of the repair areas are within the top 7 inches of the headwall. This repair would need to in conjunction with the traffic safety feature improvements.

To improve motorist safety, remove and replace the existing guardrail with a standard guardrail and proper end-treatments.

Ensure the utility gas line is properly supported and install additional supports if needed.

The following estimate includes construction costs for recommended repairs noted above (See Table 4 below for costing breakdown; quantities are approximate):

- Guardrail Replacement
 - o MassDOT Item 620.13 GUARDRAIL, TL-3 (SINGLE FACED) (350 ft)
 - o MassDOT Item 627.73 GUARDRAIL TANGENT END TREATMENT, TL-3 (2 each)
- Concrete Headwall Replacement
 - o MassDOT Item 904.3 5000 PSI, 3/4", 685 HP CEMENT CONCRETE (2 cy)

Table 4: Cost estimate for repairs – B-19-004

Fuller Lane over Fish Brook (B-19-004)	
Cost Estimate	
Construction Costs	\$140,000
Safety Improvements (guardrail replacement)	\$90,000
Maintenance (headwall Replacement)	\$50,000
Engineering Cost (survey, design, permitting, const. oversight)	\$45,000
Total	\$185,000





Figure 17: Utility gas line spanning the north bridge fascia – B-19-004



Figure 18: Typical wearing surface – B-19-004





Figure 19: Typical rust staining and impact damage to guardrail – B-19-004



Figure 20: Typical horizontal crack on headwall at guardrail baseplates – B-19-004



BRIDGE NO. B-19-005 (89B) – ENDICOTT ROAD OVER FISH BROOK General

The bridge carries Endicott Road over Fish Brook and is located just north of the Masconomet Regional High School and Middle School. This bridge was built in 1850 and rebuilt in 1900. There are timber bridge railings traveling the bridge on both sides (Figure 21). This bridge is not listed on the NBI in Massachusetts as of 2019 data, but there was a routine bridge inspection report performed at this location by MassDOT on May 7, 2020 (provided in Attachment E).



Figure 21: North bridge approach (looking south) - B-19-005

Observations

The pavement is in fair condition with minor sporadic cracking. There appears to be more significant wear towards both gutter lines, though it was covered in snow. There are fire hydrants present at the north and south bridge approaches on the east embankment. There are overhead wires present on the east embankment. The sidewalks were covered in snow during the preliminary inspection but were uncovered during follow-up visit (Figure 22). TEC measured the curb reveal to be approximately 5½".



The east and west bridge railings consist of concrete posts with timber railings. Generally, there is heavy spalling all on all posts with exposed rusted rebar. The timber railings are not attached to the approach guardrail (concrete posts with wire rail). The approach guardrail has heavy spalling and exposed rusted rebar on the concrete posts with some missing posts. The wire rail is disconnected from the concrete posts and broken in some areas (Figure 23).

The west bridge elevation has heavy spalling with exposed rusted rebar along the full length of the west fascia and was covered in snow. There is a gas line spanning the west bridge fascia, but not connected to bridge. The east bridge elevation has a utility spanning the east fascia, above the wingwalls (Figure 24).

The Fish Brook was frozen over, and a wader inspection was inaccessible.

Please refer to the May 2020 MassDOT culvert inspection report provided in Attachment E, for additional information.

TEC concurs with the condition ratings from the routine inspection report performed by MassDOT in May 2020.

Recommendations

To improve motorist safety, remove and replace the existing guardrail with a standard guardrail (approaches, transitions, and end-treatments).

Ensure the utilities spanning the east and west bridge fasciae are properly supported. Provide any additional support if needed.

The following estimate includes construction costs for recommended repairs noted above (See Table 5 below for costing breakdown; quantities are approximate):

- Guardrail Replacement
 - o MassDOT Item 620.13 GUARDRAIL, TL-3 (SINGLE FACED) (300ft)
 - MassDOT Item 627.73 GUARDRAIL TANGENT END TREATMENT, TL-3 (2 each)



Table 5: Cost estimate for repairs – B-19-005

Endicott Road over Fish Brook (B-19-005)	
Cost Estimate	
Construction Costs	\$90,000
Safety Improvements (guardrail replacement)	\$90,000
Engineering Cost (survey, design, permitting, const. oversight)	\$45,000
Total	\$135,000



Figure 22: Pavement condition along west gutter line – B-19-005





Figure 23: Typical condition of concrete posts at bridge approaches - B-19-005



Figure 24: Utility gas line spanning the west bridge fascia - B-19-005



BRIDGE NO. B-19-018 (C68) – BROOKVIEW ROAD OVER FISH BROOK General

The bridge carries Brookview Road over Fish Brook and is located south of the intersection at Brookview Road and Lawrence Road (Figure 25). The bridge is situated on a severe skew in relation to the roadway. The bridge appears to be comprised of precast concrete deck panels, a prestressed concrete deck slab, and a hot mixed asphalt (HMA) wearing surface. The abutments appear to be dry-laid stones. The Fish Brook flows west to east. Based on 2019 data, this bridge is not listed on the National Bridge Inventory (NBI) in Massachusetts, however MassDOT has performed multiple inspections on this bridge including: a routine and special member inspection in 2019, a special member inspection in early 2020, and a routine underwater inspection in late 2020. The corresponding inspection reports are provided in Attachment F.

The special member inspections were required due to an undermining failure at the north and south breastwalls (Figures 30 & 31). These deficiencies were repaired shortly after the 2019 and 2020 special member inspections performed by MassDOT.

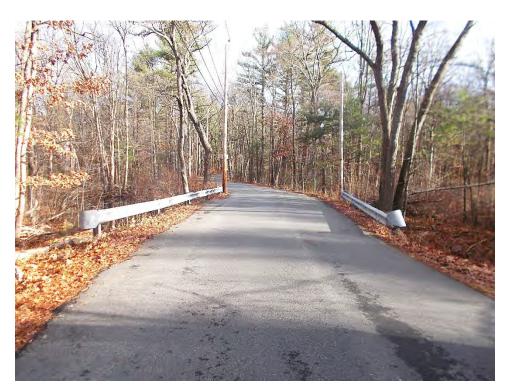


Figure 25: General bridge approach (looking north) - B-19-018



Observations

Generally, the wearing surface is sound and appears to be placed directly on top of the prestressed concrete slab. There is minor sporadic cracking in the wearing surface along the length of the bridge. There is embankment erosion at all four corners of the bridge, exhibiting the prestressed concrete slab ends to be visible (Figure 26).

The guardrail traveling the bridge is a steel W-beam guardrail embedded into the concrete slab. Both guardrails are non-standard. The approach guardrail is loose and can be moved with foot pressure (Figure 27).

The abutments and U-wingwalls consist of dry-laid stones with evidence of concrete repairs on both breastwalls. The repairs are irregular in nature and are bulged to form around stones. The U-wingwalls are irregular with multiple areas of fill spilling through stones. There are voids present between all dry-laid stones up to ~3" deep at abutments. The northwest wingwall is bulging ~2' out of plane (Figure 28). There is undermining at the northwest breastwall/wingwall measuring ~ (18" deep by 7' long). From foot probing, there appears to be multiple areas of scour present along both abutments. The Fish Brook is traveling with moderate velocity and the stream appears to get deeper as you move downstream.

There is debris build-up in the upstream (west) channel \sim (7' long by 2' tall by 2' wide) consisting of sticks, leaves, brush, etc. Generally, the underside of the beams is sound, and each beam is \sim 5' wide. The two fascia beams overhang the abutments at all four corners (up to \sim 20") (Figure 29). There is minor longitudinal cracking and delamination throughout the underside of the beams.

Please refer to Attachment F for more information on this bridge provided in MassDOT inspection reports.

TEC concurs with the condition ratings from the inspection reports performed by MassDOT in 2019 and 2020.



Recommendations

To improve motorist safety, TEC recommends removing and replacing the existing guardrail with standard guardrail (proper approaches, transitions, and end treatments).

Perform substructure/embankment repairs including realigning wingwalls, patching voids in stones, riprap treatment adjacent to channel mouths, and undermining repairs.

The following estimate includes construction costs for recommended repairs noted above (See Table 6 below for costing breakdown; quantities are approximate):

- Guardrail Replacement
 - o MassDOT Item 620.13 GUARDRAIL, TL-3 (SINGLE FACED) (300ft)
 - o MassDOT Item 627.73 GUARDRAIL TANGENT END TREATMENT, TL-3 (2 each)
- Substructure/embankment Repairs
 - o MassDOT Item 692 BALANCE STONE WALL (100ft)
 - o MassDOT Item 151.3 GRAVEL BORROW FOR SLOPE TREATMENT (50cy)
 - o MassDOT Item 106.251 CEMENT MORTAR FOR PATCHING (50cy)
 - o MassDOT Item 983.1 RIPRAP (15 TON)

Table 6: Cost estimate for repairs – B-19-018

Brookview Road over Fish Brook (B-19-018	3)
Cost Estimate	
Construction Costs	\$146,500
Safety Improvements (guardrail replacement)	\$90,000
Maintenance (substructure/embankment repairs)	\$56,500
Engineering Cost (survey, design, permitting, const. oversight)	\$65,000
Total	\$211,500





Figure 26: Typical exposed prestressed slab at northeast corner – B-19-018



Figure 27: Typical guardrail over bridge (looking south) – B-19-018





Figure 28: Northwest wingwall bulging out of plane – B-19-018



Figure 29: Beam overhang at north abutment – B-19-018





Figure 30: Typical breastwall undermining repair – B-19-018



Figure 31: Typical breastwall undermining repair – B-19-018



CONCLUSIONS

Overall, the bridges in Boxford are in fair condition. Most of the bridges do not have traffic safety features that meet the current crash-tested standards. TEC recommends continued routine maintenance and inspection at these bridges. Below, TEC has outlined approximate programmatic costs for the maintenance repairs of these structures. The traffic safety feature improvements are not considered below but should be pursued at the Town's discretion.

Boxford Bridge Asset Management	
Construction Cost Estimate	
Repairs within the next 5 years	\$53,000
Middleton Road Maintenance Repairs	\$23,000
Brookview Road Maintenance Repairs	\$30,000
Repairs within 5-10 years	\$175,000
Lockwood Lane Maintenance Repairs	\$30,000
Fuller Lane Repairs and Traffic Safety Features	\$140,000
Repairs within 15-20 years	
Replacement of Brookview Rd over Fish Brook	\$1,500,000



ATTACHMENT A: MIDDLETON ROAD OVER FISH BROOK (B-19-001)
 ROUTINE UNDERWATER INSPECTION REPORT PERFORMED BY MASSDOT ON DECEMBER 28, 2018

2-DIST **04**

B.I.N. **899**

UNDERWATER OPERATIONS TEAM ROUTINE UNDERWATER INSPECTION REPORT

BR. DEPT. NO.
B-19-001

CITY/TOWN LEVEL OF INSPECTION 93B-DATE INSPECTED 8-STRUCTURE NO BOXFORD B19001-899-MUN-CUL **DEC 28, 2018** Ш 7-FACILITY CARRIED ACCESS TO BRIDGE UNDERWATER OPERATIONS ENGINEER **HWY MIDDLETON RD** RANDI E. BONICA **EMBANKMENT** 06-FEATURES INTERSECTED DEPTH VISIBILITY TEAM LEADER (DIVE MASTER) Report submitted by: WILLIAM J. COLLERAN WATER FISH BROOK 1.2 m 1 m TEAM MEMBERS BOTTOM CONDITION CURRENT R. E. BONICA, K. CARR **GRAVEL, COBBLES, BOULDERS** MODERATE ITEM 60 **ITEM 61** ITEM 62 CHANNEL & 5 5 N SUBSTRUCTURE **CULVERTS CHANNEL PROTECTION** DEF DEF DFF 1. Abutments 5 1. Channel Scour 1. Roof N a. Pedestals Ν 2. Embankment Erosion 5 2. Floor N 5 M-P 3. Walls Ν b. Bridge Seats 3. Debris Ν M-P c. Backwalls 6 Ν 4. Vegetation 4. Headwall 5 M-P d. Breastwalls 7 Ν 5. Utilities 5. Wingwall 6 e. Wingwalls 6. Rip-Rap/Slope Protection Ν N 6. Pipe Ν f. Slope Paving/Rip-Rap 7 Ν 7. Aggradation 7. Protective Coating Ν g. Pointing Ν Ν 8. Fender System 8. Embankment 7 h. Footings/Cribbing Ν N 9. Wearing Surface a. Piles X i. Piles N N b. Diagonal Bracing 10. Railing 5 j. Scour Ν Ν c. Horizontal Bracing 11. Sidewalks 5 M-P k. Settlement Ν Ν d. Wales 12. Utilities Ν Ν Ν e. Fasteners 13. Member Alignment 2. Piers or Bents Ν Ν Ν f. Ladders 14. Deformation Ν -Ν a. Pedestals Ν 15 Scour N b. Caps N ITEM 59 SUPERSTRUCTURE 16. Settlement DFF Ν N c. Columns 17. Ν Ν d. Stems/Webs/Pierwalls 18. Ν Ν e. Pointing N UNDERMINING (Y/N) Ν f. Footing **DEFICIENCY REPORTING GUIDE** N g. Piles DEFICIENCY: A defect in a structure that requires corrective action. Ν h. Scour CATEGORIES OF DEFICIENCIES: Ν i. Settlement M= Minor Deficiency- - Deficiencies which are minor in nature, generally do not impact the structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor scouring, etc. Ν S= Severe/Major DeficiencyDeficiencies which are more extensive in nature and need more planning and effort to repair. Examples include but are not limited to: Moderate to major deterioration in concrete, Exposed and corrording rebars, Deteriorated timber piles, Considerable settlement, Considerable scouring or undermining, etc. N 3. Pile Bents Ν A deficiency in a structural element of a bridge that poses an extreme unsafe condition due to the failure or imminent failure of the element which will affect the structural integrity of C-S= Critical Structural Deficiencya. Pile Caps Ν the bridge. A deficiency in a component or element of a bridge that poses an extreme hazard or unsafe condition to the public, but does not impair the structural integrity of the bridge. Examples include but are not limited to: Any part of piles or fender system which are projecting outward and may become a safety hazard for the navigational traffic, etc. C-H= Critical Hazard Deficiencyb. Piles N Ν c. Diagonal Bracing URGENCY OF REPAIR: N d. Horizontal Bracing [Inspector(s) immediately contact District Bridge Inspection Engineer (DBIE) to report the Deficiency and to receive further instruction from him/her.] I=Immediate-Ν e. Fasteners [Action/Repair should be initiated by District Maintenance Engineer or the responsible party (if not a State owned bridge) upon receipt of the Inspection Report.] A=ASAP-[Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs UNDERMINING (Y/N) P=Prioritize
 CITY/TOWN
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 B-19-001
 B19001-899-MUN-CUL
 DEC 28, 2018

REMARKS

GENERAL REMARKS

This bridge is a single span structure with dry-laid fieldstone abutments with concrete caps for bridge seats and concrete deck beams for a bridge deck.

Orientation:

Abutments are labeled left and right when facing downstream. Sta 10+00 is at the downstream end. The upstream end is Sta 10+27.

ITEM 60 - SUBSTRUCTURE

<u>Item 60.1 - Abutments</u>

Item 60.1.d - Breastwalls

Left Abutment: The breastwall consists of dry-laid, irregular fieldstones. There are random loose and missing chinking stones in the breastwall. Maximum penetration into open joints is 3'+. There are random cracked stones in the breastwall.

There is an area of bulging stones at the approximate mid-length of the breastwall. The area is approximately 8' long and extends from the mudline to the concrete bridge seat. The stones are bulging approximately 1'. This condition is probably from the original construction.

There is a void at Sta 10+25 to 10+27 at the top of the breastwall measuring 2.0' long, 1.0' high and 3.0' penetration.

Right Abutment: The breastwall consists of dry-laid, irregular fieldstones. There are random loose and missing chinking stones in the breastwall. Maximum penetration into open joints is 3'+.

There is a 1/2" wide crack at Sta 10+06 in the stone 2.5' below the deck. There are random cracked stones up to 1/8" wide in the breastwall.

There is an area of bulging stones at the approximate mid-length of the breastwall. The area is approximately 6' long and 3' high and begins approximately 3' below the top of the breastwall. The stones are bulging approximately 1'. This condition is probably from the original construction.

Item 60.1.e - Wingwalls

Wingwalls have random cracked stones. Maximum penetration is 3' between stones.

Left Abutment: The downstream left wingwall, approximately 3' downstream of the bridge, has an area of collapsed fieldstones measuring 4' long, 2' high and 2' penetration. This condition appears to be caused by roadway runoff.

Item 60.1.h - Footings/Cribbing

Right Abutment: Timber cribbing is exposed from Sta 10+16 to 10+28. Height of timber is 1'. There are isolated penetrations under the timber of 0.5'.

Item 60.1.j - Scour

Right Abutment: Timber cribbing is exposed from Sta 10+16 to 10+28. Height of timber is 1'. There are isolated penetrations under the timber of 0.5'.

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REMARKS

Item 60.1.k - Settlement

Left Abutment: The breastwall consists of dry-laid, irregular fieldstones. There are random loose and missing chinking stones in the breastwall. Maximum penetration into open joints is 3'+. There are random cracked stones in the breastwall.

There is an area of bulging stones at the approximate mid-length of the breastwall. The area is approximately 8' long and extends from the mudline to the concrete bridge seat. The stones are bulging approximately 1'. This condition is probably from the original construction.

There is a void at Sta 10+25 to 10+27 at the top of the breastwall measuring 2.0' long, 1.0' high and 3.0' penetration.

Right Abutment: The breastwall consists of dry-laid, irregular fieldstones. There are random loose and missing chinking stones in the breastwall. Maximum penetration into open joints is 3'+.

There is a 1/2" wide crack at Sta 10+06 in the stone 2.5' below the deck. There are random cracked stones up to 1/8" wide in the breastwall.

There is an area of bulging stones at the approximate mid-length of the breastwall. The area is approximately 6' long and 3' high and begins approximately 3' below the top of the breastwall. The stones are bulging approximately 1'. This condition is probably from the original construction.

Left Abutment Wingwall: The downstream left wingwall, approximately 3' downstream of the bridge, has an area of collapsed fieldstones measuring 4' long, 2' high and 2' penetration. This condition appears to be caused by roadway runoff.

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

Right Abutment: Timber cribbing is exposed from Sta 10+16 to 10+28. Height of timber is 1'. There are isolated penetrations under the timber of 0.5'.

Item 61.2 - Embankment Erosion

The upstream and downstream embankments are eroded.

Item 61.3 - Debris

There are several trees up to 1.5' in diameter across the channel at the upstream end. There is recent beaver activity at the bridge.

<u>Item 61.4 - Vegetation</u>

There are vines at the upstream end hanging into the water.

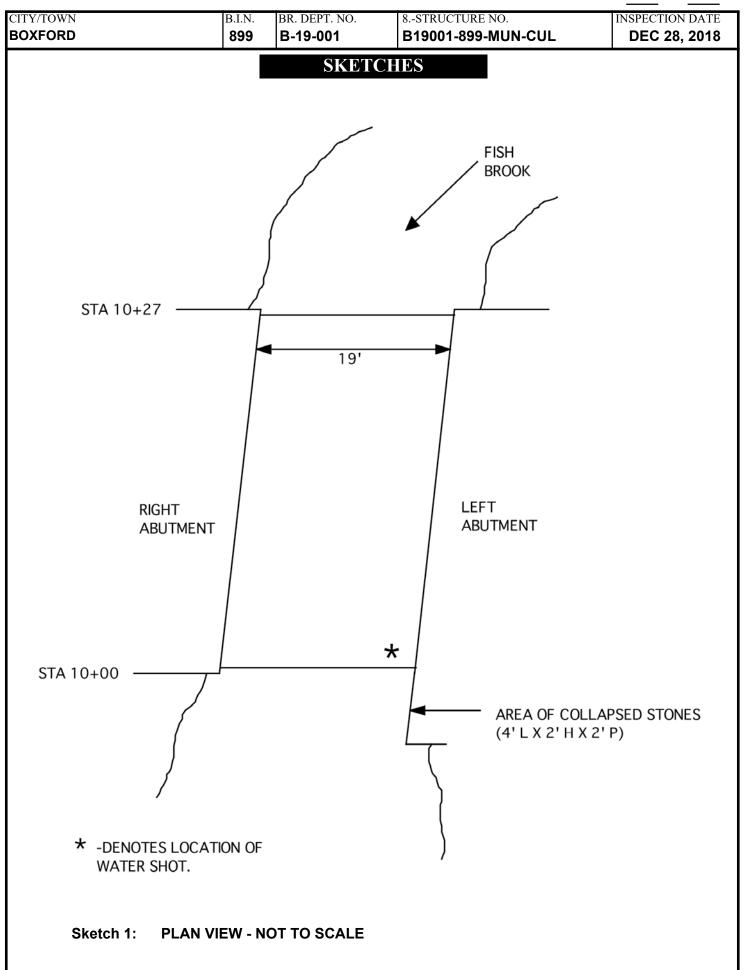
<u>Item 61.5 - Utilities</u>

There is a 6" diameter Boxford Fire Department intake pipe which extends into the center of the stream at the downstream left embankment. The end of the pipe is free of debris.

Sketch / Chart Log

Sketch 1: PLAN VIEW - NOT TO SCALE

Chart 1: SCOUR MONITORING CHART AT DOWNSTREAM END



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 DEC 28, 2018

CHARTS

	3/11/03	3/10/06	5/6/09	12/28/12	12/15/15	12/28/18	
LEFT ABUTMENT	2.3'	2.8'	3.1'	3.1'	3.1'	3.1	
C/L SPAN	3.6'	3.8'	3.8'	3.7'	3.7'	3.9	
RIGHT ABUTMENT	2,3'	2.9'	3.4'	3.0'	3.0'	3.1	
Υ	2.6'	3.7'	3.3'	2.0'	4.0'	3.2	
CORRECTION FACTOR	in out	+1.1'	+0.7'	-0.6'	+1.4'	+0.6'	

NOTES:

- 1. Y =WATERLINE TO BOTTOM OF BRIDGE DECK AT DOWNSTREAM END, LEFT ABUTMENT.
- 2. SOUNDINGS ADJUSTED TO 3/11/03 WATERLINE WITH CORRECTION FACTOR.
- 3. STA 10+00 IS AT THE DOWNSTREAM END.

•	er 20, 2020 State Information	1			Classification		Code
BDEPT#= B19001		Agency Br.No.		(112) NBIS Bridge Length			N
Town= Boxford		L	.0.	(104) Highway System			N
B.I.N= 899			TO= 029.0	(20) i dilottorial oldoo	Urban Local		19
RANK= 0 H.I.=	0 _ Identification _	FHWA Select List= I	N (6/21/2017)	(100) Defense Highway			1
(8) Structure Number		B19001	899MUNCUL	(101) Parallel Structure			?
(5) Inventory Route				(102) Direction of Traffic -			?
(2) State Highway Department Dis			04				N
(b) County Couc	(4) Place code	WATER 5	07420	()			0
(6) Features Intersected				(110) Designated National N	etwork		V
(7) Facility Carried (9) Location		HVVT WIID	DLETON RD	,	own Agency		03
(11) Kilometerpoint			0000.000	• •	wn Agency		03
(12) Base Highway Network			N	,	undeterr	mined	00
(13) LRS Inventory Route & Subro	ute	00000000000	.,	(67) Theteriodi digrilliodi	Condition		Code
(16) Latitude		42 DEG 38 MIN	46.44 SEC	(58) Deck			N
(17) Longitude		70 DEG 59 MIN	45.60 SEC	(FO) C			N
(98) Border Bridge State Code		Sha	re %	(60) Substructure			5
(99) Border Bridge Structure No.	#			(61) Channel & Channel Prot	ection		5
Struc	ture Type and M	aterial		(62) Culverts	Load Rating and Po	netina	Code
(43) Structure Type Main:		Code	???	(31) Design Load -	_ Load Naming and Po		code ?
	Jointles	s bridge type: Not a	pplicable	(63) Operating Rating Method	d - Allowable Stres	ss (AS)	2
(44) Structure Type Appr:				(64) Operating Rating		. ,	00.0
		Code	???	(65) Inventory Rating Method	- Allowable Stres	ss (AS)	2
(45) Number of spans in main unit			001	(66) Inventory Rating			00.0
(46) Number of approach spans			0000	(70) Bridge Posting			0
(107) Deck Structure Type -	_		Code ?	(41) Structure - Op	oen Appraisal _		Code
(108) Wearing Surface / Protective	System:			(67) Structural Evaluation	Appraisar _		code
A) Type of wearing surface -			Code ?	(68) Deck Geometry			<u> </u>
B) Type of membrane -			Code ?	(69) Underclearances, vert. a	nd horiz.		N
C) Type of deck protection -	Age and Consider		Code ?	(71) Waterway adequacy			
(07) \	Age and Service		4050	(72) Approach Roadway Aligi	nment		
(27) Year Built			1850 1900	(36) Traffic Safety Features			
(106) Year Reconstructed (42) Type of Service: On -			1900	(113) Scour Critical Bridges	Inspections		N
Under -			Code	(90) Inspection Date 0	0/00/00	(91) Freque	ncy 24 M
(28) Lanes: On Structure	00	Under structure	00	(92) Critical Feature Inspection		(* .)	(93) CFI DATI
(29) Average Daily Traffic	00	Chaor Structure	000000	(A) Fracture Critical Detail	N	00 MO A	
	000 (109) Truck	ADT	00 %	(B) Underwater Inspection	Υ	36 MO E	3) 12/28/ ²
(19) Bypass, detour length	(100) 11111		000 KM	(C) Other Special Inspection	n N	00 MO C	00/00/0
	Geometric Data			(*) Other Inspection ()	N	00 MO*	00/00/0
(48) Length of maximum span			0001.2 M	(*) Closed Bridge	N	00 MO*	00/00/0
(49) Structure Length			00004.9 M	(*) UW Special Inspection	N	00 MO *	00/00/0
(50) Curb or sidewalk:	Left 00	.0 M Righ		(*) Damage Inspection	Dating Loads	MO *	00/00/0
(51) Bridge Roadway Width Curb t	o Curb		000.0 M	Report Date 00/00/00	Rating Loads H20		rpe 3S2 Type HS
(52) Deck Width Out to Out			000.0 M	Operating	0.0	0.0	0.0 0.0
(32) Approach Roadway Width (w/	shoulders)	_	000.0 M	Inventory	0.0	0.0	0.0 0.0
(33) Bridge Median -	(0=) 0:	Cod			Field Posting		
(34) Skew 00 DEG	(35) Structu	re Flared	N	Status		Posting Date	00/00/00
(10) Inventory Route MIN Vert Cle			00.00 M	2 Axle	3 Axle	5 Axle	Single
(47) Inventory Route Total Horiz C			00.0M	Actual Recommended			
(53) Min Vert Clear Over Bridge R	-	1	M 00.00	Missing Signs N			
(54) Min Vert Underclear ref	F		M 00.00		Misc.		
(55) Min Lat Underclear RT ref	F	1	M 0.00	Bridge Name			
(56) Min Lat Underclear LT	Navigation Data		00.0 M	N Anti-missile fence	N Acrow Panel	N Jo	ointless Bridge
(38) Navigation Control -			Code ?	Freeze/Thaw N : Not Applie		4/11co4/	
(111) Pier Protection			Code ?	•	Accessibility (Needed		
(39) Navigation Vertical Clearance			000.0 M	N / N Liftbucket	N / N Rigging	N	I / N Other
(116) Vert-lift Bridge Nav Min Vert	Clear		М	N / N Ladder N / N Boat	N / N Staging N / N Traffic Cor	ntrol	
· · ·			0000.0 M				Inspection
(40) Navigation Horizontal Clearan	56		0000.01	N / N Wader	N / N RR Flagpe	erson	Hours: 00

ATTACHMENT B: MILL ROAD OVER FISH BROOK (B-19-002)

- ROUTINE BRIDGE INSPECTION REPORT PERFORMED BY MASSDOT ON MAY 16, 2019
- EXISTING BRIDGE PLANS, DATED: APRIL 2002

[2-DIST] B.I.N. STRUCTURES INSPECTION FIELD REPORT BR. DEPT. NO.

04 8RE ROUTINE INSPECTION												3-19	-00	2			
CITY/TOWN			8ST	RUCTURE NO.			11-	-Kilo	. POINT	41-STATUS	90-ROUTINE INSP. DATE						
BOXFORD				B19002-8F	RE-MU	N-NBI		000	0.660	A:OPEN	M	ΙΑΥ	16,	2019			
07-FACILITY CARRIED				MEMORIAL NAM	E/LOCAL	NAME		27-Y	R BUILT	106-YR REBUILT	YR F	EHAE	3'D (N	ON 106)			
HWY MILL ROAD								2	2004	0000		(0000)			
06-FEATURES INTERSECTED			26-FUNCTIONAL CLASS DIST. BRIDGE						EINSPECTI	ON ENGINEER	J. Did	eo					
WATER FISH BRO	OK			Rural Loca	ıl												
43-STRUCTURE TYPE				22-OWNER 21-MAINTAINER TEAM LEAD													
501 : Prestressed (Town Agency Agency															
107-DECK TYPE		WEATHER	TEMP. (a		TEAM MI												
2 : Concrete Preca	st Pa	nels		Clear	13	°C	O. MO	บร	STAFA								
ITEM 58	7		m	ZM 59		7	1		ITEM	60		8					
DECK	SUP	ERSTRUCTU	RE	<u>'</u>			SUBST	RUCTURE	L		DEF						
1.Wearing surface	7	M-P	1.Stri	ingers		N] -		1. Abut	ments	Dive	Cur	8				
2.Deck Condition	7	_	-	orbeams		N	1 -		a. Pedes	tals	N	N		-			
	N		3.Flo	or System Braci	na	N	_		b. Bridge		N	Н		-			
3.Stay in Place Forms				ders or Beams	9	7			c. Backw		N	H 8		-			
4.Curbs	7	-		sses - General		N	-	-	e. Wingw		N	7					
5.Median	N	-		Upper Chords		V 14			f. Slope	Paving/Rip-Rap	N	8		-			
6.Sidewalks	N	-					-	$-\ $	g. Pointii		N	N		-			
7.Parapets	7	-		Lower Chords N			-	$-\ $	h. Footin	gs	N N	H		-			
8.Railing	7	-		Web Members		N	-		j. Scour		N	8		-			
9.Anti Missile Fence	N	-		Lateral Bracing		N	-	_	k. Settler	ment	N	8		-			
10.Drainage System	N	_	e. Sway Bracings N				-		m.		N N	N N		-			
11.Lighting Standards	N	_	f. Portals N			_	-	_	1	or Bents			N				
	N		<u> </u>	End Posts		N	- -	_	a. Pedes	tals	N	N		-			
12.Utilities		-		& Hangers		N	-		b. Caps		N	N		-			
13.Deck Joints	7	M-P		nn Pit's, Gusset	s & Angl		-		c. Colum	ns /Webs/Pierwalls	N	N					
14.	N	-		ver Plates		N	-		e. Pointii		N	N		-			
15.	N	-	9.Bea	aring Devices		Н	-		f. Footin	<u> </u>	N	N		-			
16.	N	-	10. Di	aphragms/Cros	s Frame	s N	-		g. Piles		N	N		-			
			11.Ri	vets & Bolts		N	-		h. Scour		N	N		-			
OUDD DEVEAL	<u> </u>	W	12.W	elds		N	-		j.		N	N		-			
(In millimeters)	50	250	13. M	ember Alignmer	nt	8	-		k.		N	N		-			
APPROACHES		DEF	14.Pa	aint/Coating		N	-		3. Pile I		N	N	N				
a. Appr. pavement condition	7		15.			N	_		b. Piles	ips	N	N					
	-	-	Voar	Painted	N				-	nal Bracing	N	N		-			
b. Appr. Roadway Settlement	8	-	ı eal	i annou		•		_[ntal Bracing	N N			-			
c. Appr. Sidewalk Settlement	N	-		ISION DAMAGE:		•		╽	e. Faster	iers	N	N	J l	-			
d.	N	-		None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain					UNDERMINING (Y/N) If YES please explain				N				
OVERHEAD SIGNS	Y/N)	N			Moderat	•	evere ()	∐		ON DAMAGE:							
(Attached to bridge)		DEE	LOAD	VIBRATION:		None (X) Minor () Moderate () Severe (

DEF

Ν

N

) Severe (

Ν

SCOUR: Please explain

93B-U/W (DIVE) Insp

Ν

None (X) Minor (

I-60 (Dive Report):

None (X) Minor () Moderate (

Any Cracks: (Y/N)

Any Fracture Critical Member: (Y/N)

00/00/0000

8

) Moderate () Severe (

I-60 (This Report):

a. Condition of Welds

b. Condition of Bolts

c. Condition of Signs

CITY/	TOW1	V			B.I.I	N. BR. DI	EPT. NO. 8	INSPECTION DATE						
	FOR				8R	l l				E-MUN-NI		MAY 1		
		_				_								
ITR	EM 61				7	ITEM 36	TRAFFIC SAF	ETY 36	COND	DEF	ACCESSIE	BILITY	(Y/N	(/P)
СНА	NNE	L &		ı		A. Bridge R	ailing	1	7	-		N	leeded	
СНА	NNE	L PROTECTIO	V			B. Transitio		1	8	_	Lift Bucket		N	N
				_				1	6	M-P	Ladder		N	N
			Dive		DEF	C. Approac					Boat		N	N
1.Ch	annel	Scour	N	7	-	D. Approac	n Guardrail Ends	0	6	M-P	Waders		Υ	Υ
2.Em	nbankn	nent Erosion	N	7	-	WEIGHT P	OSTING	Not Ap	plicabl	le X	Inspector 50		N	N
3.Del	bris		N	8	-		_Н_			ngle	Rigging		N	N
4 Vo	getatio	<u> </u>	N	7		Actual Pos	tina N	N	N	N	Staging	N	N	
<u> </u>)II			-			N	N		Traffic Conti	rol	N	N
5.Uti	lities		N	N	-	Recommer	ded Posting N	N	RR Flagger		N	N		
6.Rip	-Rap/S	Slope Protection	N	8	-	Waived Date	: 00/00/0000 EJ	0/00/0000	Police		N	N		
7.Aq	gradat	ion	N	8	-		At bridge	Advance	Other:					
	nder S		N	N		Signs In Place	e N	S	N	S	Other.		N	N
0.1 61	iluei 3	ystein	IN	IN		(Y=Yes,N=No NR=NotRegu							14	IN
						Legibility/	"cu)			1 /	TOTAL HO	OURS		8
						Visibility			0 0 0 0 0 0		0			
			V	PLANS	(Y/N): [Υ							
Not X ft in ft in meter												(.,.	,	•
STREAM FLOW VELOCITY: Actual Field Measurement O (V.C.R.) (Y/N): N														
STREAM FLOW VELOCITY: Posted Clearance 0 0												(1/14).	IN	
Tidal () High () Moderate () Low (X) None () At bridge Advance TAPE#:														
ITEM 61 (Dive Report): N ITEM 61 (This Report): 7 Signs In Place (Y=Yes,N=No,														
	•	. ,	•		, L-	NR=Not Req		<u> </u>		 	List of field tes	sts performed:		
93b-U	U/W IN	SP. DATE : 00	0/00/	0000)	Legibility/		/						
D A TEX	INC					Visibility								
RATI		rt (Y/N): Y								.	S please give pi		_	
Rating	ј керо	rt (Y/N): Y				Recommend	for Rating or Rerat	ing (Y/N	1):	N HIG	SH() MEDIUM	() LOW ()	
Date:	.	12/01/2005				REASON:								
In	nspectio	on data at time of e	existir	na rat	ina	KLASON.								
		: 9 160: 9 Da				-								
				0,00	2000									
						С	<u>ONDITION RA</u>	<u>TING</u>	<u>GUID</u>	E (For	Items 58, 59, 60	and 61)		
	CODE	CONDITION					DEFECTS							
	N	NOT APPLICABLE												
G	9	EXCELLENT	E	xcellen	t condition.									
G	8	VERY GOOD	N	o probl	em noted.									
					inor probler	26								
G -	7	GOOD	_		'		1-4							
F	6	SATISFACTORY	_			show some minor								
F	5	FAIR	Al	ll prima	ry structura	l elements are sou	nd but may have minor sect	ion loss, ci	racking, s	spalling or scour.				
Р	4	POOR	_			ss, deterioration, s					16.3	31 50		
Р	3	SERIOUS				erioration, spalling cks in concrete ma	or scour have seriously affe y be present.	cted prima	ry structu	iral components. L	₋ocal failures are po	ssible. Fatigue cra	acks	
		ODITIOAL					ctural elements. Fatigue cra							
С	2	CRITICAL					closely monitored it may be						ma.	
С	1	"IMMINENT" FAILURE					ent in critical structural com action may put it back in lig			vertical or norizon	tai movement affect	ing structure stab	ility.	
		EALL ED	0	ut of se	ervice - hev	and corrective action	n							
	0 FAILED Out of service - beyond corrective action.													
DEFICIENCY REPORTING GUIDE														
DEFI	CIENC	Y: A defect in a str	ucture	that re	quires corre		ICIENCY REPO	<u>DRTIN</u>	IG G	UIDE				
	CIENC	Y: A defect in a stru			quires corre		ICIENCY REPO	DRTIN	IG G	UIDE				
CATE	CIENC	ES OF DEFICIENC	CIES	;	<u> </u>	ctive action.					de but are not limited to:	Spalled concrete, Mi	nor pot	
CATE M= M	CIENC EGORI Minor D	ES OF DEFICIENC Deficiency - Deficiencies holes, Minor	Vertical a	re minor	in nature, ge	nerally do not impact thring, Clogged drainage	e structural integrity of the bridge , etc.	and could ea	asily be rep	aired. Examples includ				and
CATE M= M	CIENC EGORI Minor D	ES OF DEFICIENC Deficiency - Deficiencies holes, Minor	Vertical a	re minor on of ster ies which rebars, C	in nature, ge el, Minor scou n are more ex Considerable s	ctive action. nerally do not impact thring, Clogged drainage ensive in nature and nettlement, Considerab	e structural integrity of the bridge , etc. eed more planning and effort to re e scouring or undermining, Mode	and could ea epair. Exampl rate to exten	asily be reposed include sive corros	aired. Examples included but are not limited to: lion to structural steel w	Moderate to major deter with measurable loss of	rioration in concrete, I section, etc.	Exposed	
CATE M= M S= Se	CIENC EGORI Minor D	ES OF DEFICIENC Deficiency - Deficiencies holes, Minor	which a corrosid eficienci	are minor on of steries which rebars, C	in nature, ge el, Minor scou n are more ex Considerable s ciency in a str	ctive action. nerally do not impact thring, Clogged drainage ensive in nature and nettlement, Considerab	e structural integrity of the bridge , etc.	and could ea epair. Exampl rate to exten	asily be reposed include sive corros	aired. Examples included but are not limited to: lion to structural steel w	Moderate to major deter with measurable loss of	rioration in concrete, I section, etc.	Exposed	
CATE M= N S= Se C-S=	CIENC EGORI Minor D evere/M Critica	ES OF DEFICIENC Deficiency - Deficiencies holes, Minor Lajor Deficiency - Deficiency	which a corrosid efficiencion proded in the corrosided in the corr	re minor on of steries which rebars, C A defi the br	in nature, ge el, Minor scou n are more exi considerable s ciency in a str idge.	ctive action. nerally do not impact the ring, Clogged drainage ensive in nature and nettlement, Considerab uctural element of a bronent or element of a legistration.	e structural integrity of the bridge , etc. eed more planning and effort to re e scouring or undermining, Mode dge that poses an extreme unsaf oridge that poses an extreme haz	and could ea epair. Exampl rate to exten e condition d ard or unsafe	asily be repailes include sive corrostue to the face condition to	aired. Examples included to: but are not limited to: ion to structural steel vailure or imminent failuto to the public, but does	Moderate to major deter with measurable loss of re of the element which not impair the structura	rioration in concrete, I section, etc. will affect the structur I integrity of the bridge	Exposed ral integri e. Examp	ty of oles
CATE M= N S= Se C-S=	CIENC EGORI Minor D evere/M Critica	Deficiency - Deficiencies holes, Minor Deficiency - Defic	which a corroside efficiencial product of the corroside of the corroded of the	re minor on of steries which rebars, C A defi the br	in nature, ge el, Minor scou n are more exi considerable s ciency in a str idge.	ctive action. nerally do not impact the ring, Clogged drainage ensive in nature and nettlement, Considerab uctural element of a bronent or element of a legistration.	e structural integrity of the bridge, etc. sed more planning and effort to re e scouring or undermining, Mode dge that poses an extreme unsaf	and could ea epair. Exampl rate to exten e condition d ard or unsafe	asily be repailes include sive corrostue to the face condition to	aired. Examples included to: but are not limited to: ion to structural steel vailure or imminent failuto to the public, but does	Moderate to major deter with measurable loss of re of the element which not impair the structura	rioration in concrete, I section, etc. will affect the structur I integrity of the bridge	Exposed ral integri e. Examp	ty of oles
CATE M= N S= Se C-S= C-H=	CIENC EGORI Minor D evere/M Critica Critica	Deficiency - Deficiencies holes, Minor Deficiency - Defic	which a corroside efficiencial product of the corroside of the corroded of the	ire minor on of steries which rebars, C A defi the br A deficie	in nature, ge el, Minor scou n are more exi considerable s ciency in a str idge.	ctive action. nerally do not impact the ring, Clogged drainage ensive in nature and nettlement, Considerab uctural element of a bronent or element of a legistration.	e structural integrity of the bridge , etc. eed more planning and effort to re e scouring or undermining, Mode dge that poses an extreme unsaf oridge that poses an extreme haz	and could ea epair. Exampl rate to exten e condition d ard or unsafe	asily be repailes include sive corrostue to the face condition to	aired. Examples included to: but are not limited to: ion to structural steel vailure or imminent failuto to the public, but does	Moderate to major deter with measurable loss of re of the element which not impair the structura	rioration in concrete, I section, etc. will affect the structur I integrity of the bridge	Exposed ral integri e. Examp	ty of oles
CATE M= N S= Se C-S= C-H=	CIENCE EGORI Minor Devere/M Critica Critica ENCY	Deficiency - Deficiencies holes, Minor Deficiency - Defic	which a corrosic efficienci orroded in the corrosic efficienci orroded in the corroded in the corrosic effects and the corrosic effects are correctly as a corroded in the corrosic effects are correctly as a corrosic efficiency and the corrosic efficiency and the corrosic efficiency and the corrosic efficiency are correctly as a corrosic efficiency and the corrosic efficiency are correctly as a corrosic efficiency and the corroded in the correctly and the correctly effect in the correctly e	re minor on of steries which rebars, C A defi- the br A deficie include betc.	in nature, ge el, Minor scou n are more exi Considerable s ciency in a str idge. ncy in a comp aut are not lim	ctive action. nerally do not impact the ring, Clogged drainage ensive in nature and nettlement, Considerab uctural element of a bronnent or element of a leted to: Loose concrete	e structural integrity of the bridge , etc. eed more planning and effort to re e scouring or undermining, Mode dge that poses an extreme unsaf oridge that poses an extreme haz	and could exepair. Example rate to exten e condition d ard or unsafestrians, A horreceive furth.	asily be repaired in the sinclude isive corrossitute to the face condition to lee in a side	aired. Examples included to a control to the control to the control to the public, but does swalk that may cause it on from him/her].	Moderate to major deter with measurable loss of re of the element which not impair the structura njuries to pedestrians, N	rioration in concrete, I section, etc. will affect the structur I integrity of the bridge	Exposed ral integri e. Examp	ty of oles

 CITY/TOWN
 B.I.N.
 BR. DEPT. NO.
 8.-STRUCTURE NO.
 INSPECTION DATE

 BOXFORD
 8RE
 B-19-002
 B19002-8RE-MUN-NBI
 MAY 16, 2019

REMARKS

BRIDGE ORIENTATION

Boxford Bridge B-19-002 carries Mill Road over Fish Brook. The approaches and abutments are labeled north and south according to the bridge plans. Fish Brook flows from west to east.

ITEM 58 - DECK

Item 58.1 - Wearing surface

Near mid-span there is a 5' long hairline longitudinal crack at the crown of the roadway. See Photo 1.

<u>Item 58.2 - Deck Condition</u>

This item is rated based on the condition of prestressed deck units. See item 59.4, "Girders or Beams".

Item 58.7 - Parapets

The east bridge railing base has a vertical hairline crack at post # 6, from the south.

Item 58.8 - Railing

Type S3-TL4 bridge railing.

The southeast concrete end post has honeycombing or minor collision damage to the top edge. **See Photo 2.**

There are scrapes to the coating system of the horizontal railing components on both bridge railings. **See Photo 3.**

Item 58.13 - Deck Joints

According to the design plans, the joints should be a saw-cut pavement with pourable joint sealer, however, the joints in the field are paved over.

At both north and south abutment joints the pavement is cracked, up to 3/4" wide at the north abutment. **See Photos 4 and 5.**

ITEM 59 - SUPERSTRUCTURE

Item 59.4 - Girders or Beams

Girder 1 has isolated vertical hairline cracks adjacent to the south abutment up to 12 inches high with light efflorescence. **See Photo 6.**

<u>Item 59.13 - Member Alignment</u>

See item 59.4, Girders or Beams."

ITEM 60 - SUBSTRUCTURE

Item 60.1 - Abutments

Item 60.1.e - Wingwalls

The wingwalls have isolated vertical hairline cracks up to full height.

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

The channel consists of cobbles and large 4-8 inch diameter stones.

 CITY/TOWN
 B.I.N.
 BR. DEPT. NO.
 8.-STRUCTURE NO.
 INSPECTION DATE

 BOXFORD
 8RE
 B-19-002
 B19002-8RE-MUN-NBI
 MAY 16, 2019

REMARKS

<u>Item 61.2 - Embankment Erosion</u>

There are spotty areas of vegetation along the shore with exposed roots.

<u>Item 61.4 - Vegetation</u>

There is light vegetation located at the upstream and downstream.

TRAFFIC SAFETY

Item 36a - Bridge Railing

See Item 58.8

Item 36b - Transitions

Thrie beam guardrail at all four transitions attached to end posts. Post spacing is properly spaced.

<u>Item 36c - Approach Guardrail</u>

Both Southwest & Northwest approach guardrail have minor collision damage to the guardrail. At the southwest approach, two spacer blocks are displaced and one spacer block is missing at post 2. **See photos 7 and 8.**

<u>Item 36d - Approach Guardrail Ends</u>

Approach guardrail ends are buried but are not flared away from bridge.

The northwest guardrail end has collision damage adjacent to the buried end. See Photo 8.

Photo Log

Photo 1: Wearing surface has hairline crack near mid-span at the crown of the roadway

Photo 2: Southeast end-post has minor honeycombing or collision damage.

Photo 3: East bridge railing has minor scrapes (typical at the west bridge railing).

Photo 4: South deck end has a minor crack in the pavement at the joint.

Photo 5: North deck end has a full length transverse crack, up to 3/4" wide.

Photo 6: Beam 1. west face near the south abutment has a 12" vertical hairline crack.

Photo 7: Northwest approach guardrail has collision damage and displaced or missing spacer blocks.

Photo 8: Northwest approach guardrail-Minor collision damage.

CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE BOXFORD 8RE B-19-002 B19002-8RE-MUN-NBI MAY 16, 2019



Photo 1: Wearing surface has hairline crack near mid-span at the crown of the roadway



Photo 2: Southeast end-post has minor honeycombing or collision damage.

CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE BOXFORD 8RE B-19-002 B19002-8RE-MUN-NBI MAY 16, 2019



Photo 3: East bridge railing has minor scrapes (typical at the west bridge railing).



Photo 4: South deck end has a minor crack in the pavement at the joint.

CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE
BOXFORD 8RE B-19-002 B19002-8RE-MUN-NBI MAY 16, 2019



Photo 5: North deck end has a full length transverse crack, up to 3/4" wide.



Photo 6: Beam 1, west face near the south abutment has a 12" vertical hairline crack.

CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE
BOXFORD 8RE B-19-002 B19002-8RE-MUN-NBI MAY 16, 2019



Photo 7: Northwest approach guardrail has collision damage and displaced or missing spacer blocks.



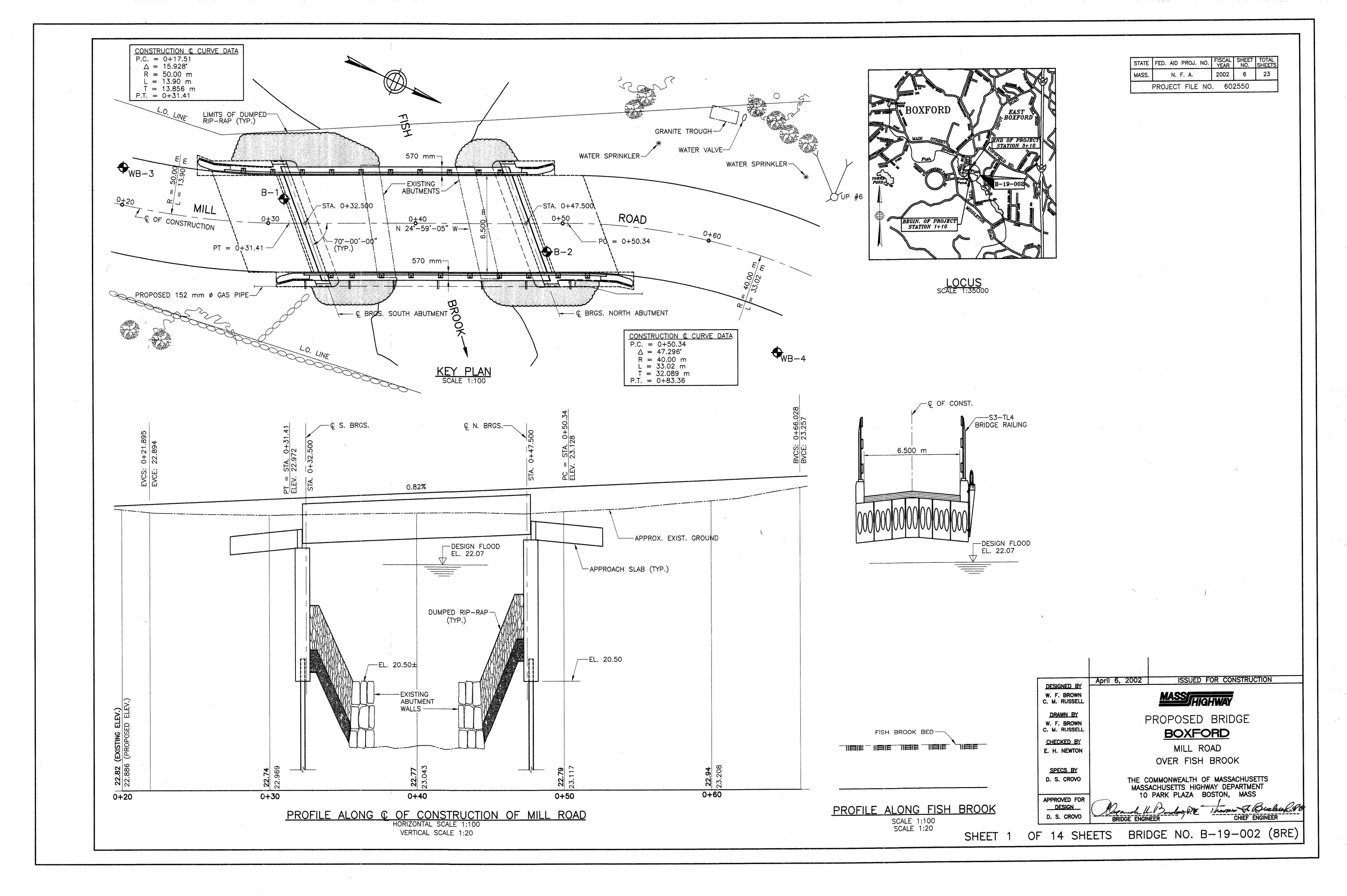
Photo 8: Northwest approach guardrail-Minor collision damage.

State Information			Classification	Code
BDEPT#= B19002	Agency Br.No.		(112) NBIS Bridge Length	Υ
Town= Boxford	L.	Ο.	(104) Highway System	Ν
B.I.N= 8RE		TO= 078.8	(20) 1 41101101141 01400	09
RANK= 4467 H.I.= 99.9 % Identification	FHWA Select List= \	Y (6/21/2017)	(100) Defense Highway	(
(8) Structure Number		BREMUNNBI	(101) Parallel Structure	1
(5) Inventory Route		151000000	(102) Direction of Traffic - 2-way traffic	2
(2) State Highway Department District		04		١
(3) County Code 009 (4) Place code		07420	. ,	(
(6) Features Intersected		ISH BROOK	. , ,	١
(7) Facility Carried		MILL ROAD		
(9) Location	1.1 MILE VV	EST OF I 95		03
(11) Kilometerpoint		0000.660 N		03
(12) Base Highway Network (13) LRS Inventory Route & Subroute	00000000000	IN	. ,	Code
(16) Latitude	42 DEG 39 MIN	18.77 SEC	(FO) D I	7
(17) Longitude	70 DEG 59 MIN	57.92 SEC	(EQ) Superatrusture	7
(98) Border Bridge State Code	Shar		(60) Substructure	8
(99) Border Bridge Structure No. #			(61) Channel & Channel Protection	7
Structure Type and Ma	aterial		(62) Culverts	1 2000
(43) Structure Type Main: Prestressed Conc		501		Code
Slab Jointless	s bridge type: Not a	pplicable	(31) Design Load - HS 20=MS 18 (63) Operating Rating Method - Load Factor (LF)	5
(44) Structure Type Appr:			(64) Operating Rating (64) Operating Rating	81.
Other	Code	000	(65) Inventory Rating Method - Load Factor (LF)	1
(45) Number of spans in main unit		001	(66) Inventory Rating	48.9
(46) Number of approach spans		0000	(70) Bridge Posting	5
(107) Deck Structure Type - Concrete Preca	st Panels	Code 2	(41) Structure - Open	. A
(108) Wearing Surface / Protective System:			Appraisal(67) Structural Evaluation	Code 7
A) Type of wearing surface - Bituminous		Code 6	(68) Deck Geometry	2
B) Type of membrane - Built-up		Code 1	(69) Underclearances, vert. and horiz.	١
C) Type of deck protection - Epoxy Coated	=	Code 1	(71) Waterway adequacy	ç
Age and Service		2004	(72) Approach Roadway Alignment	4
(27) Year Built		0000	(36) Traffic Safety Features 1 1	1 0
(106) Year Reconstructed(42) Type of Service: On - Highway		0000	(113) Scour Critical Bridges Inspections	8
Under - Waterway	(Code 15	(90) Inspection Date 05/16/19 (91) Frequency 24	1 N
(28) Lanes: On Structure 02	Under structure	00	(92) Critical Feature Inspection: (93) CFI	
(29) Average Daily Traffic		001200	(A) Fracture Critical Detail N 00 MO A) 00)/00/
(30) Year of ADT 2017 (109) Truck	ADT	11 %	(B) Underwater Inspection N 00 MO B) 00)/00/
(19) Bypass, detour length		003 KM	(C) Other Special Inspection N 00 MO C) 00)/00/
Geometric Data			(*) Other Inspection (FLOOD) N 00 MO *)	5/23/
(48) Length of maximum span		0014.3 M	(*) Closed Bridge N 00 MO *) 00)/00/
(49) Structure Length		00015.2M	,)/00/
• •	0 M Right		(*) Damage Inspection MO *) 00)/00/
(51) Bridge Roadway Width Curb to Curb		006.4 M	Report Date 12/01/05 H20 Type 3 Type 3S2 Type	HS
(52) Deck Width Out to Out		007.6 M	Operating 70.0 81.0 99.0 90	.0
(32) Approach Roadway Width (w/shoulders)	Code	006.8 M e 0	Inventory 42.0 50.0 78.0 54	.0
(33) Bridge Median - No median (34) Skew 09 DEG (35) Structur			Field Posting	
(34) Skew 09 DEG (35) Structur (10) Inventory Route MIN Vert Clear	e i iaieu	N 99.99 M	Status LEGAL Posting Date 03/21/06 2 Axle 3 Axle 5 Axle Single	
(47) Inventory Route Min Vert Clear		06.6 M	2 Axle 3 Axle 5 Axle Single Actual	:
(53) Min Vert Clear Over Bridge Rdwy		99.99 M	Recommended	
(54) Min Vert Underclear ref N		00.00 M	Missing Signs N	
(55) Min Lat Underclear RT ref		00.0 M	Misc	
(56) Min Lat Underclear LT		00.0 M	Bridge Name	
Navigation Data			N Anti-missile fence N Acrow Panel N Jointless Bridge Freeze/Thaw N: Not Applicable	
(38) Navigation Control - No navigation control of	on waterway	Code 0	Accessibility (Needed/Used)	
(111) Pier Protection		Code 1	N / N Liftbucket N / N Rigging N / N Other	
(39) Navigation Vertical Clearance		000.0 M	N / N Ladder N / N Staging	
(116) Vert-lift Bridge Nav Min Vert Clear		M	N / N Boat N / N Traffic Control	
(40) Navigation Horizontal Clearance		0000.0 M	Y / Y Wader N / N RR Flagperson Inspection	_
			N / N Inspector 50 N / N Police Hours:	00

National Bridge Element Inspection

BDEPT# B-19-002 Date 05/16/2019 District Bridge Inspection Eng'r B.I.N. 8RE Joseph Dideo Item 8 B19002-8RE-MUN-NBI Inspecting Agency Mass. Highway Dept. Span Group 1 Team Leader **Patrick Burke** Town Boxford Team Osama Moustafa Member(s) District 4

			_						
El#	Element Name	Units	Env.	Total Q.	% or Q	State 1	State 2	State 3	State 4
12	Re Concrete Deck	sq feet	2	1,232.910	\ \ \ \ \ \ \ \ \ \ \ \ \ \	1,232.910			
Notes:	•	•	•						
> 510	Wearing Surfaces	sq feet	2	1,048.940	%	1,027.940	21.000		
Notes:	•	•		•	•			•	
> > 3220	Crack (Wearing Surface)	sq feet	2	21.000	%		21.000		
Notes:	1								
104	Pre Clsd Box Girder	feet	2	285.420	\ \ \ \ \ \ \ \ \ \ \ \ \ \	285.420			
Notes:	•	•	•		•				
104	Pre Clsd Box Girder	feet	3	70.000	%	70.000			
Notes:	•	•	•						
215	Re Conc Abutment	feet	2	123.200	%	123.200			
Notes:									
301	Pourable Joint Seal	feet	2	64.800	%	64.800			
Notes:	•	•		•	•			•	
330	Metal Bridge Railing	feet	2	129.910	%	118.910	11.000		
Notes:	•	•							
> 1900	Distortion	feet	2	11.000			11.000		
Notes:	•	•							
> 7000	Damage	feet	2	11.000			11.000		
Notes:	•	•	•		•				
> 515	Steel Protective Coating	sq feet	2	454.680	%	416.180	38.500		
Notes:	•	•							
> > 3440	Eff (Stl Protect Coat)	sq feet	2	38.500			38.500		
Notes:	•		•	•	•			•	



GENERAL NOTES

DESIGN:

IN ACCORDANCE WITH THE 1996 STANDARD SPECIFICATIONS OF THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS WITH INTERIM SPECIFICATIONS THROUGH 1998, FOR MS22.5 LOADING.

EXISTING CONDITIONS:

DATE:

DATE TO BE PLACED ON THE THE INSIDE FACE OF THE NORTHWESTERLY AND SOUTHEASTERLY TRANSITIONS AS SHOWN ON THE PLANS. THE YEAR USED SHALL BE THE LATEST CONTRACT COMPLETION DATE IN EFFECT WHEN THE FIRST TRANSITION IS CAST. BOTH TRANSITIONS SHALL FEATURE THE SAME DATE.

SURVEY NOTEBOOKS:

NOTE BOOK No. : 28690

ELEVATIONS & BENCH MARK DATUM:

ALL ELEVATIONS BASED ON THE NORTH AMERICAN VERTICAL DATUM (NAVD) OF 1988, UNLESS OTHERWISE NOTED.

GPS #2 N933991.740, E241033.138 ELEV. = 23.181 GPS #3 N933942.953, E241055.031 ELEV. = 22.980

SCALES:

SCALES NOTED ON THE PLANS ARE NOT APPLICABLE TO REDUCED SIZE PRINTS, DIVIDE SCALES BY 2 FOR HALF-SIZE PRINTS.

UNSUITABLE MATERIAL:

ALL UNSUITABLE MATERIAL SHALL BE REMOVED WITHIN THE LIMITS OF THE FOUNDATIONS OF THE STRUCTURE, AS DIRECTED BY THE ENGINEER.

UTILITIES:

THE CONTRACTOR SHALL LOCATE AND PROTECT FROM DAMAGE ALL EXISTING UTILITIES DURING DEMOLITION AND CONSTRUCTION.

REINFORCEMENT:

ALL REINFORCING STEEL SHALL BE COATED DEFORMED BARS CONFORMING TO THE REQUIREMENTS OF AASHTO M31M GRADE 420. UNLESS OTHERWISE NOTED ON PLANS, ALL BARS SHALL BE LAPPED AS FOLLOWS:

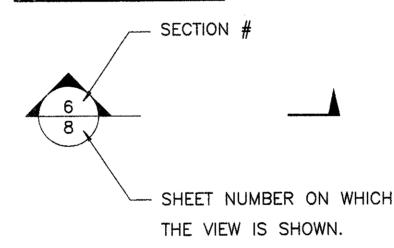
MC	DIFICATION CONDITION	#13 BARS	#16 BARS
1.	COATED BARS.	610 mm	760 mm
2.	300 mm OF	850 mm	1070 mn
	CONC. BELOW BAR		
3.	COATED BARS, COVER $< 3d_b$, OR	790 mm	990 mm
	CLEAR SPACING < 6db		
4.	CONDITION 3 WITH 300 mm OF	900 mm	1120 mr
	CONC. BELOW BAR		

IF THE ABOVE BARS ARE SPACED 150 MILLIMETERS OR MORE ON CENTER, THE LAP LENGTH SHALL BE 80% OF THE LAP LENGTH GIVEN ABOVE. ALL OTHER BARS SHALL BE LAPPED AS SHOWN ON THE PLANS.

FOUNDATIONS

FOUNDATIONS MAY BE ALTERED, IF NECESSARY TO SUIT CONDITIONS ENCOUNTERED DURING CONSTRUCTION, WITH THE APPROVAL OF THE ENGINEER.

SECTION MARK



CAST-IN-PLACE CONCRETE

- 1. CONCRETE FOR THE ABUTMENTS AND WINGWALLS BELOW THE BRIDGE SEAT CONSTRUCTION JOINT AND THE APPROACH SLAB SHALL BE 30 MPa- 40 mm- 335 kg CEMENT CONCRETE MASONRY.
- 2. CONCRETE FOR THE ABUTMENTS AND WINGWALLS ABOVE THE BRIDGE SEAT CONSTRUCTION JOINT AND THE HIGHWAY GUARDRAIL TRANSITIONS SHALL BE 35 MPa- 20 mm- 405 kg SILICA FUME MODIFIED CEMENT CONCRETE MASONRY AND SHALL BE PLACED AFTER ALL BEAMS HAVE BEEN ERECTED.
- 3. CONCRETE FOR THE BACKWALL SHALL BE 30 MPa- 20 mm- 390 kg CEMENT CONCRETE MASONRY (UNLESS PLACED AS PART OF THE APPROACH SLAB).

STRUCTURAL STEEL

ALL STRUCTURAL STEEL FOR THE UTILITY SUPPORTS SHALL CONFORM TO AASHTO M270 GRADE 250, AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M111 AND M232.

STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
MASS.	N. F. A.	2002	7	23
	PROJECT FILE NO	. 60	2550	

ESTIMATED QUANTITIES: (NOT GUARANTEED)		
DEMOLITION OF SUPERSTRUCTURE OF BRIDGE NO. B-19-002	_ 1	LS
BRIDGE EXCAVATION		CM
CLASS B ROCK EXCAVATION		СМ
GRAVEL BORROW FOR BACKFILLING STRUCTURES AND PIPES		СМ
CRUSHED STONE FOR DRAINAGE, REVETMENT, AND/OR WATERWORKS FOUNDATIONS	_ 90	MG
CLASS I BITUMINOUS CONCRETE PAVEMENT TYPE I-1		MG
CLASS I DENSE BINDER COURSE FOR BRIDGES	_ 10	MG
GEOTEXTILE FABRIC FOR PERMANENT EROSION CONTROL	_ 150	SM
STEEL PILE HP 310 x 94	_ 90	М
PRE-DRILLING FOR PILES	_ 90	М
PILE SHOES	_ 18	EA.
DUMPED RIPRAP	_ 140	MG
CONTROL OF WATER - STRUCTURE NO. B-19-002		LS
BRIDGE STRUCTURE, BRIDGE NO. B-19-002	_ 1	LS

HYDRAULIC DATA 33.05 SQUARE KILOMETERS DRAINAGE AREA: 12.744 CUBIC METERS PER SECOND DESIGN DISCHARGE: 10 YEARS DESIGN FREQUENCY: 22.07 METERS (NAVD) DESIGN FLOOD STAGE: 0.40 METERS PER SECOND DESIGN VELOCITY: BASIC FLOOD DATA 23.51 CUBIC METERS PER SECOND 100 YEAR FLOOD DISCHARGE: 22.52 METERS (NAVD) 100 YEAR FLOOD STAGE: FLOOD OF RECORD UNKNOWN DISCHARGE: UNKNOWN STAGE: 1976, 1979, 1987 DATE:

HISTORY OF ICE FLOES: NONE DOCUMENTED IN NBIS FILES

EVIDENCE OF SCOUR OR EROSION: THE USGS REPORTS OF OCTOBER 1994 INDICATES THAT THE CHANNEL IS VERTICALLY AND LATERALLY UNSTABLE DUE TO SCOUR, MEANDER IMPACTS, AND HEAVY BANK EROSION. THE CENTER CHANNEL DEEPENS FROM 0.61 METERS TO 0.914 METERS UNDER THE BRIDGE.

April 6, 2002 ISSUED FOR CONSTRUCTION

DATE DESCRIPTION

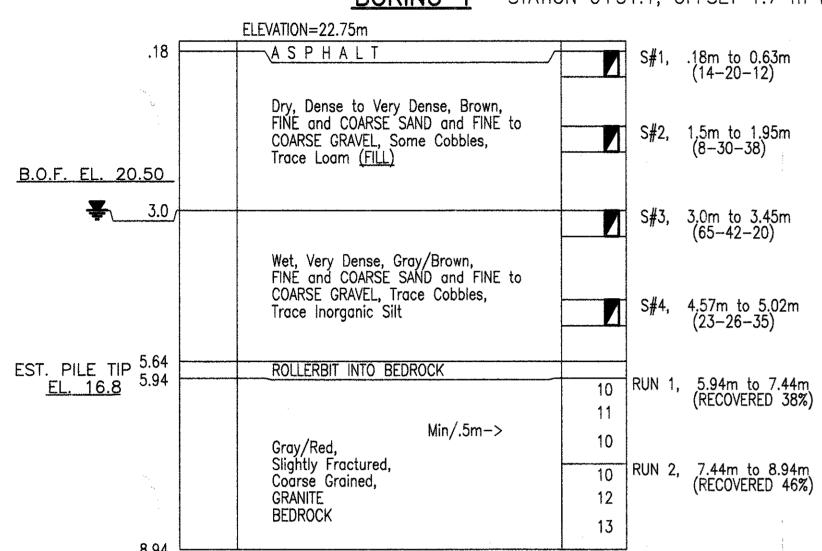
USE ONLY PRINTS OF LATEST DATE

SHEET 2 OF 14 SHEETS BRIDGE NO. B-19-002 (8RE)

CARR-DEE CORP.

37 LINDEN STREET P.O. BOX 67 MEDFORD, MA 02155-0001 Telephone (617) 391-4500 To: MASS. HIGHWAY DEPT. 10 PARK PLAZA, BOSTON, MA Date: 6-23-99 Job No.: 98120Q Location: BOXFORD-MILL RD/FISH BROOK, BRIDGE B-19-2

BORING 1 * STATION 0+31.1, OFFSET 1.7 m LT.



WATER LEVEL 3 m.
SIZE OF CASING HW, LENGTH 5.64m
SIZE OF ROCK CORE NX, LENGTH DRILLED: 3.0m
DRILLER: R. DESIMONE, INSPECTOR: CRAIG BATCHELDER
DATE STARTED & COMPLETED: 6-16-99(8:00am)-6-17-99(8:00am)
TOTAL HOURS WORKED: 8 HOURS

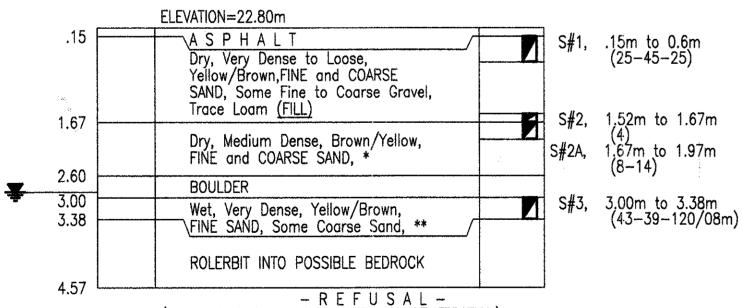
NOTE: "CHANGED LOCATION" FROM STATION 1+72>8, OFFSET 0.4m LT. TO STATION 1+74.3, OFFSET 0.4m LT. AS DIRECTED BY ENGINEER.

All samples have been visually classified by DRILLER. Unless otherwise specified, water levels noted were observed at completion of borings, and do not necessarily represent permanent ground water levels. Figures in parenthesis indicate the number of blows required to drive 5 cm. Split Sampler 15 cms. using 64 kg. weight falling 76 cms.(±). Figures in column to left (if noted) indicate number of blows to drive casing 15 cms., using 137 kg. weight falling 61 cms.(±).

CARR-DEE CORP.

37 LINDEN STREET P.O. BOX 67 MEDFORD, MA 02155-0001 Telephone (617) 391-4500 To: MASS. HIGHWAY DEPT. 10 PARK PLAZA, BOSTON, MA Date: 6-23-99 Job No.: 98120Q Location: BOXFORD-MILL RD/FISH BROOK, BRIDGE B-19-2

BORING WB-3 * STATION 0+19.5, OFFSET 2.5 m LT.



(120 BLOWS S.S., 140LB. WGT., NO PENETRATION)

WATER LEVEL 2.9m m.
SIZE OF CASING NW, LENGTH 3.3m
DRILLER: R. DESIMONE, INSPECTOR: CRAIG BATCHELDER
DATE STARTED & COMPLETED: 6-21-99(7:30am-11:30am)
TOTAL HOURS WORKED: 4 HOURS

* Some Fine and Medium Gravel, Trace Inorganic Silt

** Fine to Coarse Gravel, Some Inorganic Silt

NOTE: "CHANGED LOCATION" FROM STA. 1+63.2, OFFSET 1.4m LT. TO

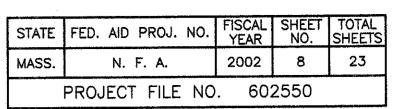
STA. 1+63.2, OFFSET .03m LT. DUE TO OVERHEAD BRANCHES.

All samples have been visually classified by DRILLER. Unless otherwise specified, water levels noted were observed at completion of borings, and do not necessarily represent permanent ground water levels. Figures in parenthesis indicate the number of blows required to drive 5 cm. Split Sampler 15 cms. using 64 kg. weight falling 76 cms.(±). Figures in column to left (if noted) indicate number of blows to drive casing 15 cms., using 137 kg. weight falling 61 cms.(±).

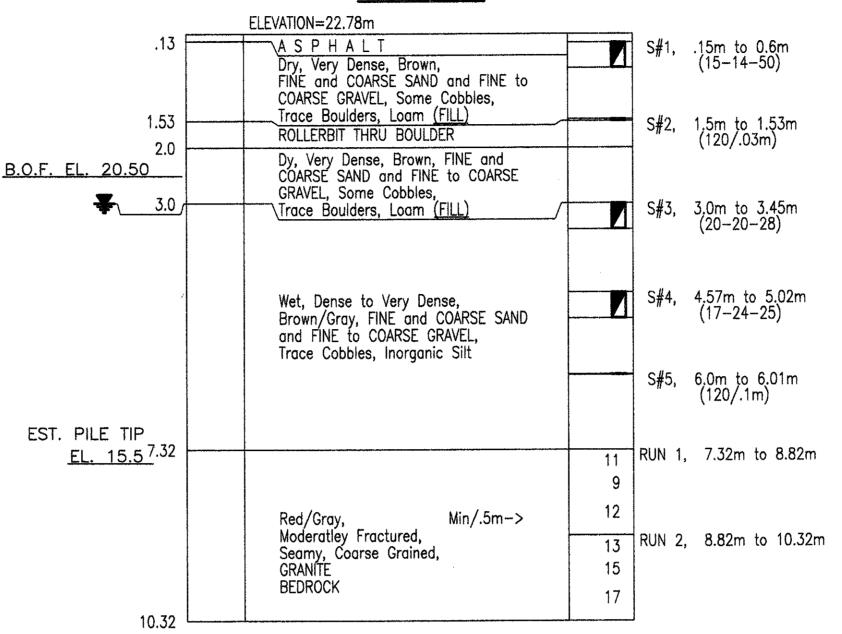
CARR-DEE CORP.

37 LINDEN STREET P.O. BOX 67 MEDFORD, MA 02155-0001 Telephone (617) 391-4500 To: MASS. HIGHWAY DEPT. 10 PARK PLAZA, BOSTON, MA Date: 6-23-99 Job No.: 98120Q Location: BOXFORD-MILL RD/FISH BROOK, BRIDGE B-19-2

MILE NOT TOTAL BINDOL D. TO Z







WATER LEVEL 3 m.
SIZE OF CASING HW, LENGTH 7.32m
SIZE OF ROCK CORE NX, LENGTH DRILLED: 3m
DRILLER: R. DESIMONE, INSPECTOR: CRAIG BATCHELDER
DATE STARTED & COMPLETED: 6-17-99(8:30am),6-18-99(10:00am)
TOTAL HOURS WORKED: 7.5 HOURS

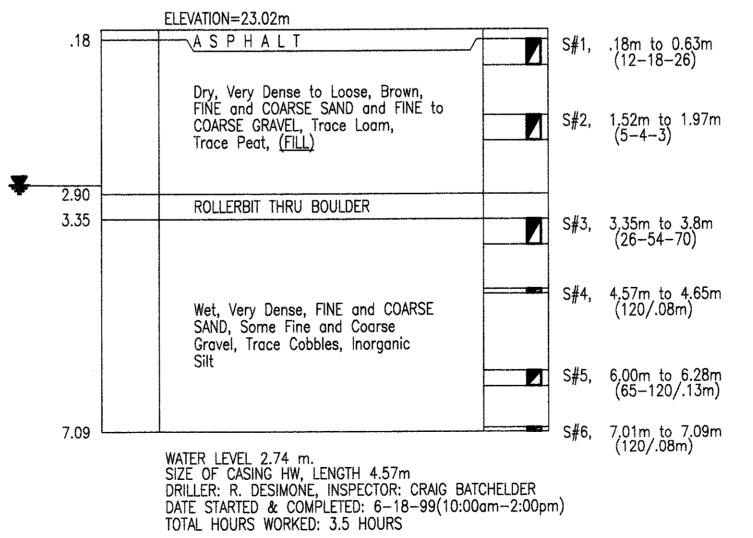
NOTE: "CHANGED LOCATION" FROM STATION 1+95.5, OFFSET 0.4m LT. TO STATION 1+92.5, OFFSET 0.4m LT. AS DIRECTED BY ENGINEER.

All samples have been visually classified by DRILLER. Unless otherwise specified, water levels noted were observed at completion of borings, and do not necessarily represent permanent ground water levels. Figures in parenthesis indicate the number of blows required to drive 5 cm. Split Sampler 15 cms. using 64 kg. weight falling 76 cms.(±). Figures in column to left (if noted) indicate number of blows to drive casing 15 cms., using 137 kg. weight falling 61 cms.(±).

CARR-DEE CORP.

37 LINDEN STREET P.O. BOX 67 MEDFORD, MA 02155-0001 Telephone (617) 391-4500
To: MASS. HIGHWAY DEPT. 10 PARK PLAZA, BOSTON, MA Date: 6-23-99 Job No.: 981200
Location: BOXFORD-MILL RD/FISH BROOK, BRIDGE B-19-2

BORING WB-4 * STATION 0+67.4, OFFSET 5.7 m RT.



NOTE: "CHANGED LOCATION" FROM STA. 2+09.7, OFFSET 0.7m RT. TO STA. 2+09.7, OFFSET 1.9m LT. DUE TO STEEP SLOPING CONDITION.

All samples have been visually classified by DRILLER. Unless otherwise specified, water levels noted were observed at completion of borings, and do not necessarily represent permanent ground water levels. Figures in parenthesis indicate the number of blows required to drive 5 cm. Split Sampler 15 cms. using 64 kg. weight falling 76 cms.(±). Figures in column to left (if noted) indicate number of blows to drive casing 15 cms., using 137 kg. weight falling 61 cms.(±).

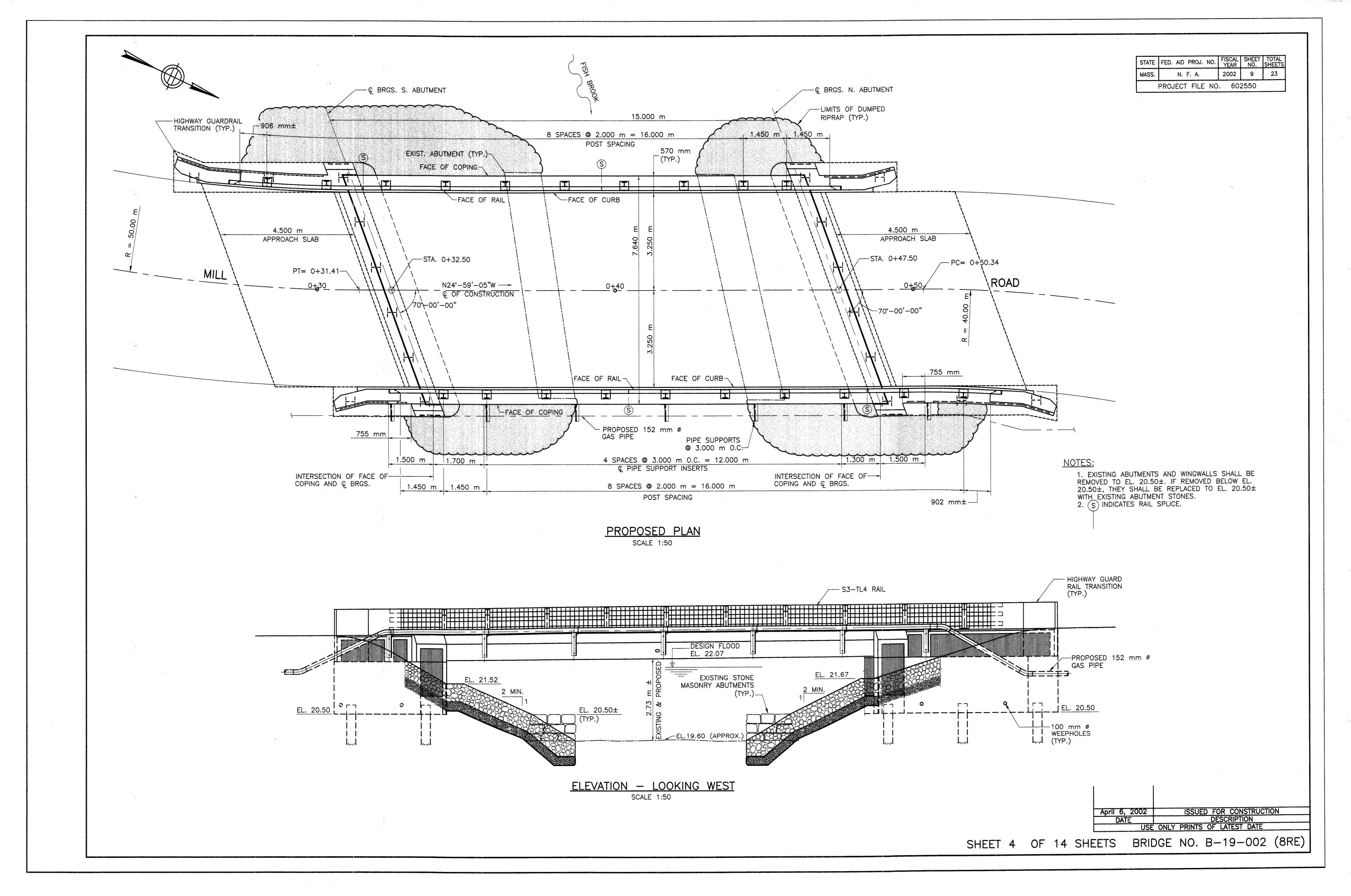
BORING NOTES:

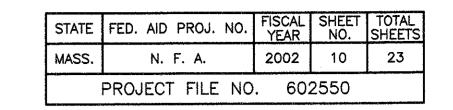
- 1. LOCATION OF BORINGS ON KEY PLAN ARE SHOWN THUS: \bigoplus B-1, ON SHEET 1.
- 2. BORINGS ARE TAKEN FOR THE PURPOSE OF DESIGN AND SHOW CONDITIONS AT BORING POINTS ONLY, BUT DO NOT NECESSARILY SHOW THE NATURE OF THE MATERIALS TO BE ENCOUNTERED DURING CONSTRUCTION.
- WATER LEVELS SHOWN ON THE BORING LOGS WERE OBSERVED AT THE TIME OF TAKING BORINGS AND DO NOT NECESSARILY SHOW THE TRUE GROUND WATER LEVEL.
- BORINGS WERE MADE BY CARR-DEE CO., 37 LINDEN ST., P.O. BOX 67, MEDFORD MA. 02155-0001.
- 5. BORINGS SAMPLES ARE STORED AT A STORAGE FACILITY LOCATED ON ROUTE 114 IN LAWRENCE, MA.. THE CONTRACTOR MAY EXAMINE THE SOIL AND ROCK SAMPLES BY CONTACTING THE MASSACHUSETTS HIGHWAY DEPARTMENT, GEOTECHNICAL SECTION AT 10 PARK PLAZA, ROOM 6210, BOSTON. MA. 02116 TELEPHONE (617)-973-8836.
- 6. ALL ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD-88).
- 7. LOCATION OF BORINGS ARE BASED ON THE TOPOGRAPHIC BASE PLAN DATED AUGUST 1998. LOCATIONS MARKED THUS: (*) HAVE BEEN PROVIDED BY THE ENGINEER AND INDICATE STATIONS AND OFFSETS BASED ON THE PROPOSED ALIGNMENT.

April 6, 2002 ISSUED FOR CONSTRUCTION

DATE DESCRIPTION

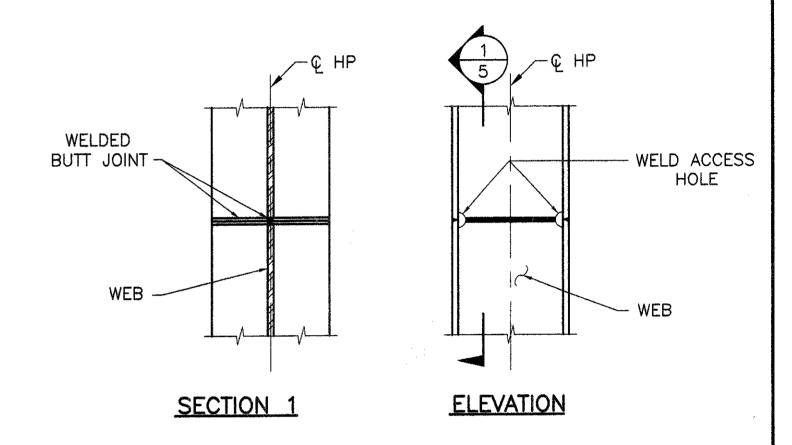
USE ONLY PRINTS OF LATEST DATE





PILE NOTES:

- 1. PILES SHALL BE DRIVEN TO PRACTICAL REFUSAL AT THE TOP OF BEDROCK. THE APPROXIMATE ELEVATION AT BEDROCK IS ELEV. 16.8 METERS AT THE SOUTH ABUTMENT AND ELEV. 15.5 METERS AT THE NORTH ABUTMENT.
- 2. a. THE FACTORED AXIAL DESIGN LOAD PER PILE IS 517 KN (GROUP I LOAD)
 - b. THE STRUCTURAL CAPACITY PER PILE IS 1972 kN
- 3. THE MINIMUM NOMINAL AXIAL RESISTANCE TO BE USED IN THE WEAP ANALYSIS IS 862 kN (517 kN/0.6).
- 4. HEAVY DUTY PILE SHOES SHALL BE INSTALLED ON THE TIPS OF ALL PILES. DETAILS OF THE PREFABRICATED PILE SHOES AND THE REQUIRED WELDING PROCEDURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

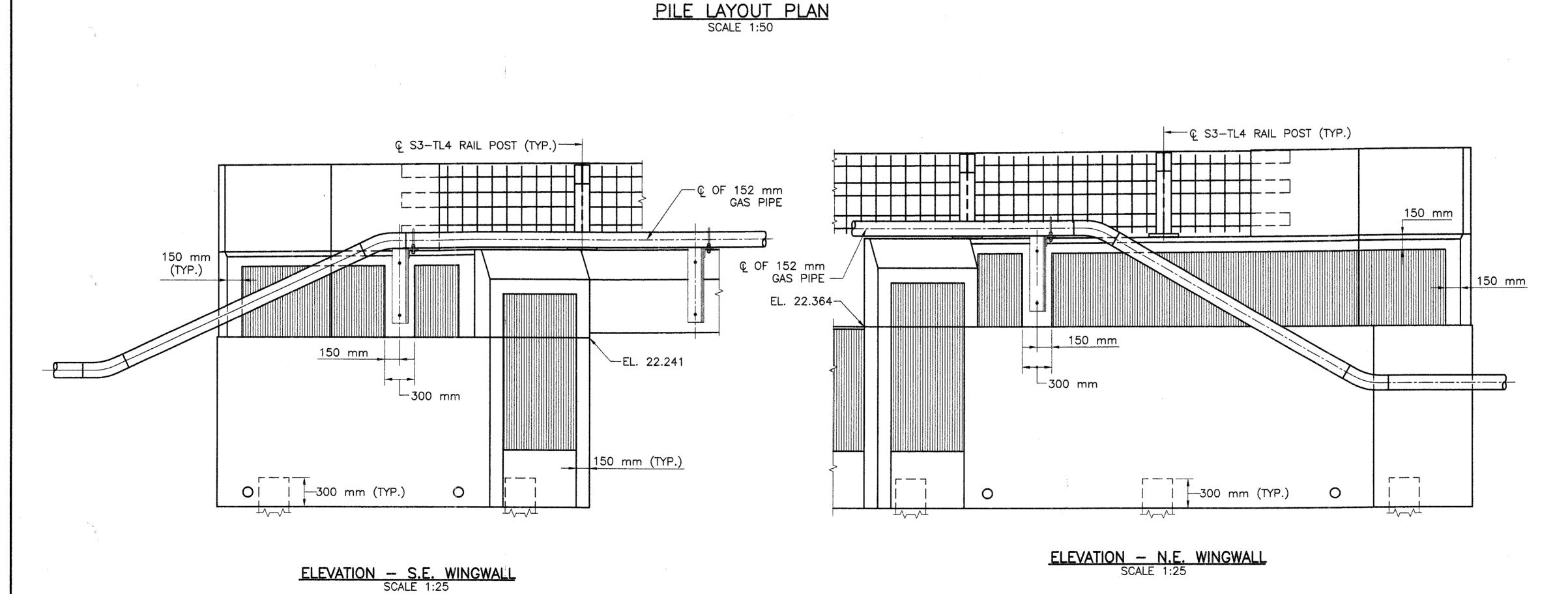


NOTES:

- 1. ALL WELDS SHALL BE COMPLETE PENETRATION AND SHALL CONFORM TO THE AASHTO/AWS BRIDGE WELDING CODE.
- WELDING PROCEDURES MUST BE APPROVED BY THE ENGINEER PRIOR TO WELDING.
- 3. WHENEVER POSSIBLE ALL PILES SHALL BE SPLICED ON THE GROUND IN THE FLAT POSITION.
- WEB SHALL BE COPED TO ALLOW FOR COMPLETE PENETRATION WELDING OF FLANGES.

H-PILE SPLICE DETAILS

SCALE 1: 10



√ C PILES

STA. 0+32.50-

2500 mm

500 mm-

500 mm-

2500 mm

_—Ç S. BRGS.

15.000 m

2.500 m

TYP. @ ALL WINGWALLS

253 mm

┌─Ç S. BRGS.

253 mm

225 mm (TYP.)—

---791 mm

—791 mm

−Ç PILES

2.500 m

-238 mm (TYP.)

-STA. 0+47.50

Q OF CONST.

N 24°-59'-05" W

-70°-00'-00" (TYP.)

2.500 m

TYP. ALL PILES
HP 310x94

2.500 m

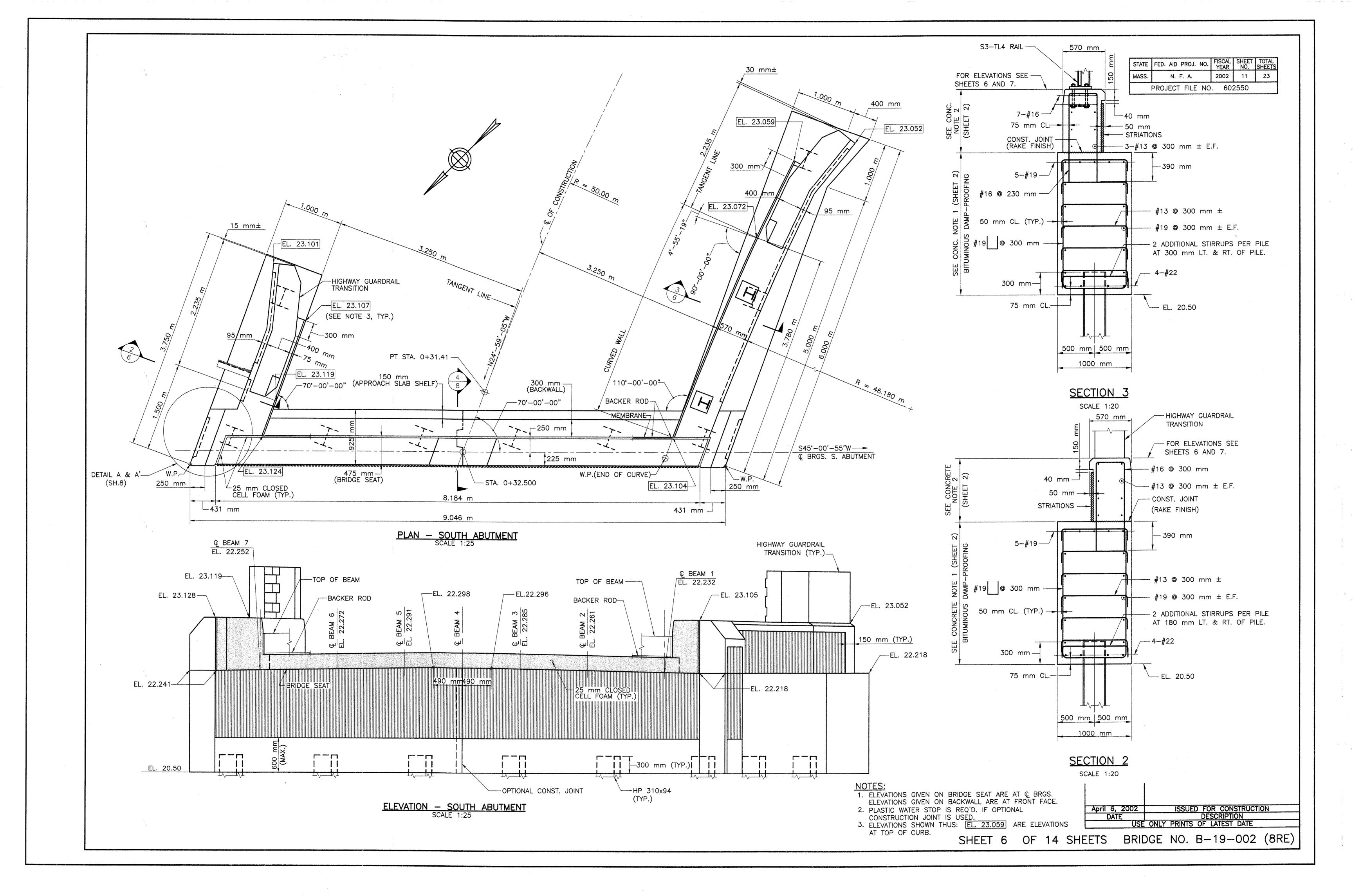
-Ç PILES

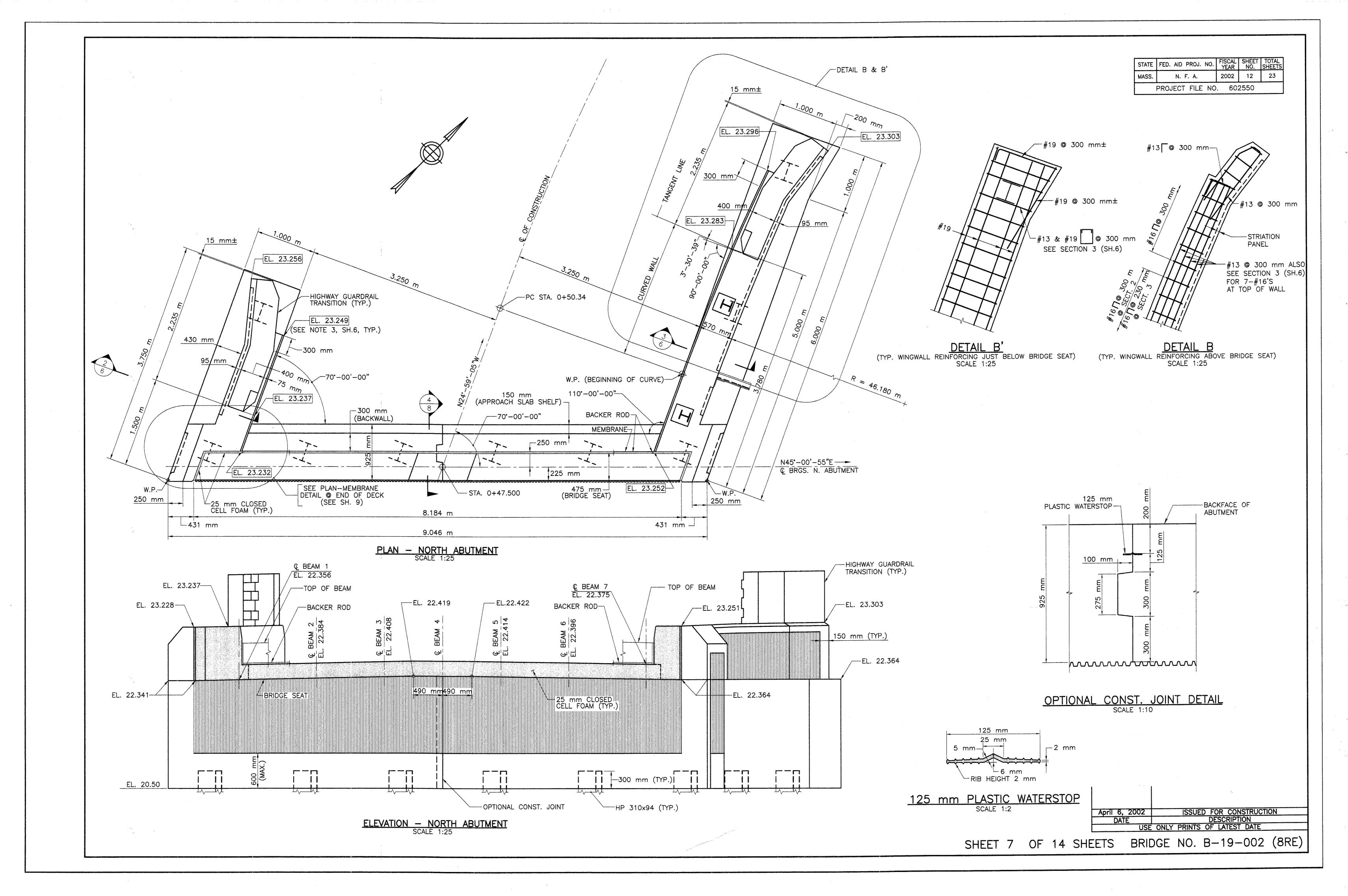
−Ç PILES

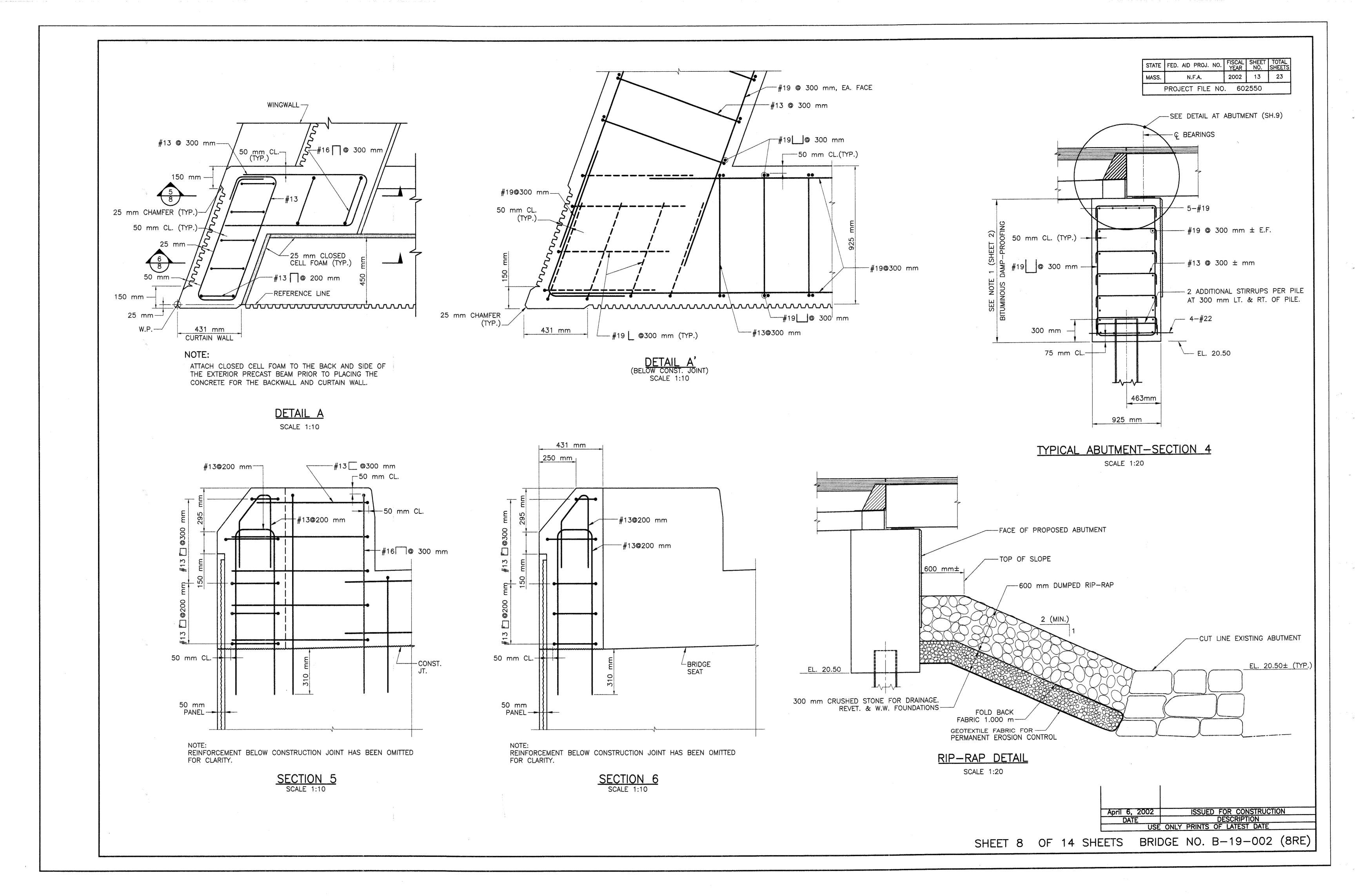
April 6, 2002 ISSUED FOR CONSTRUCTION

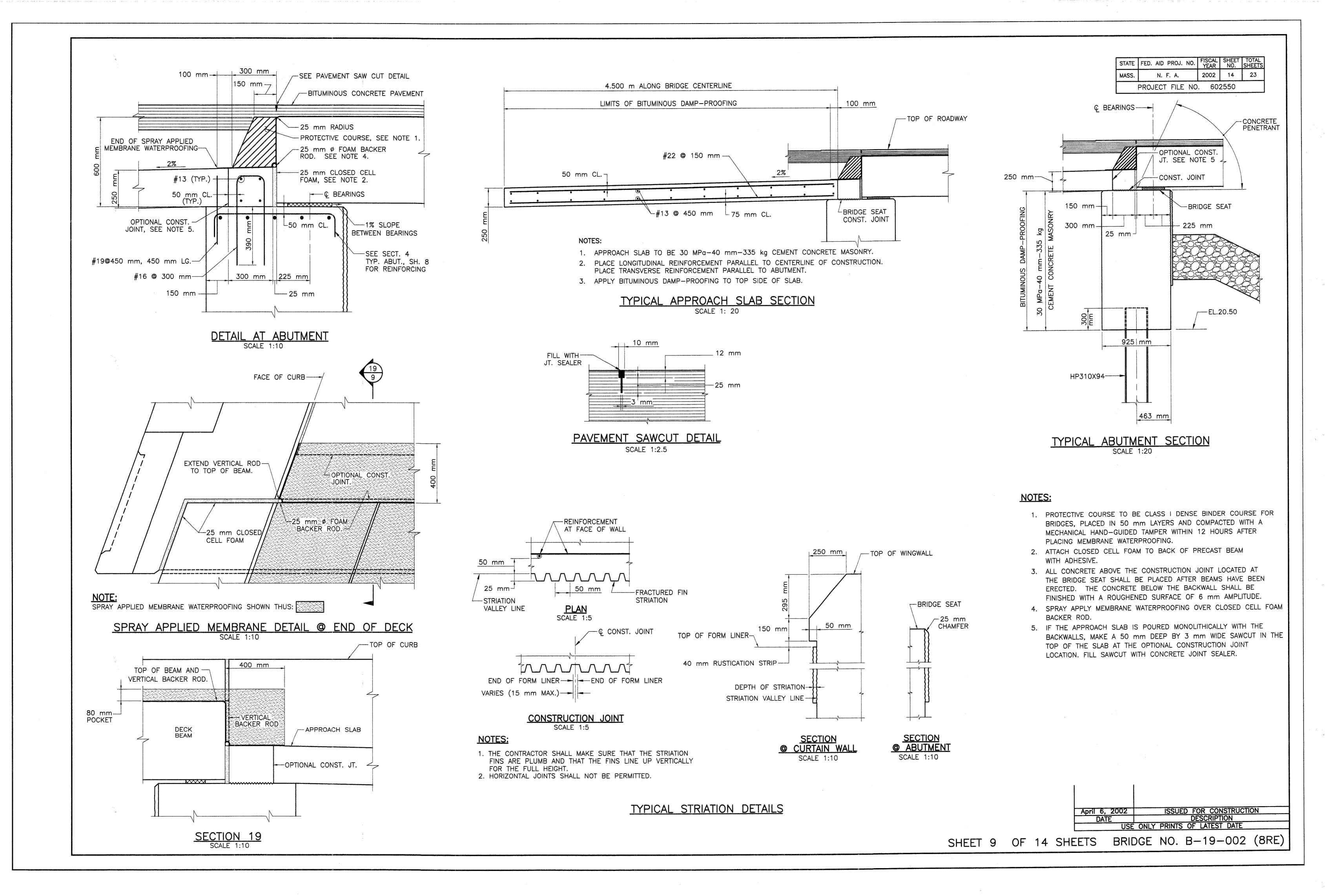
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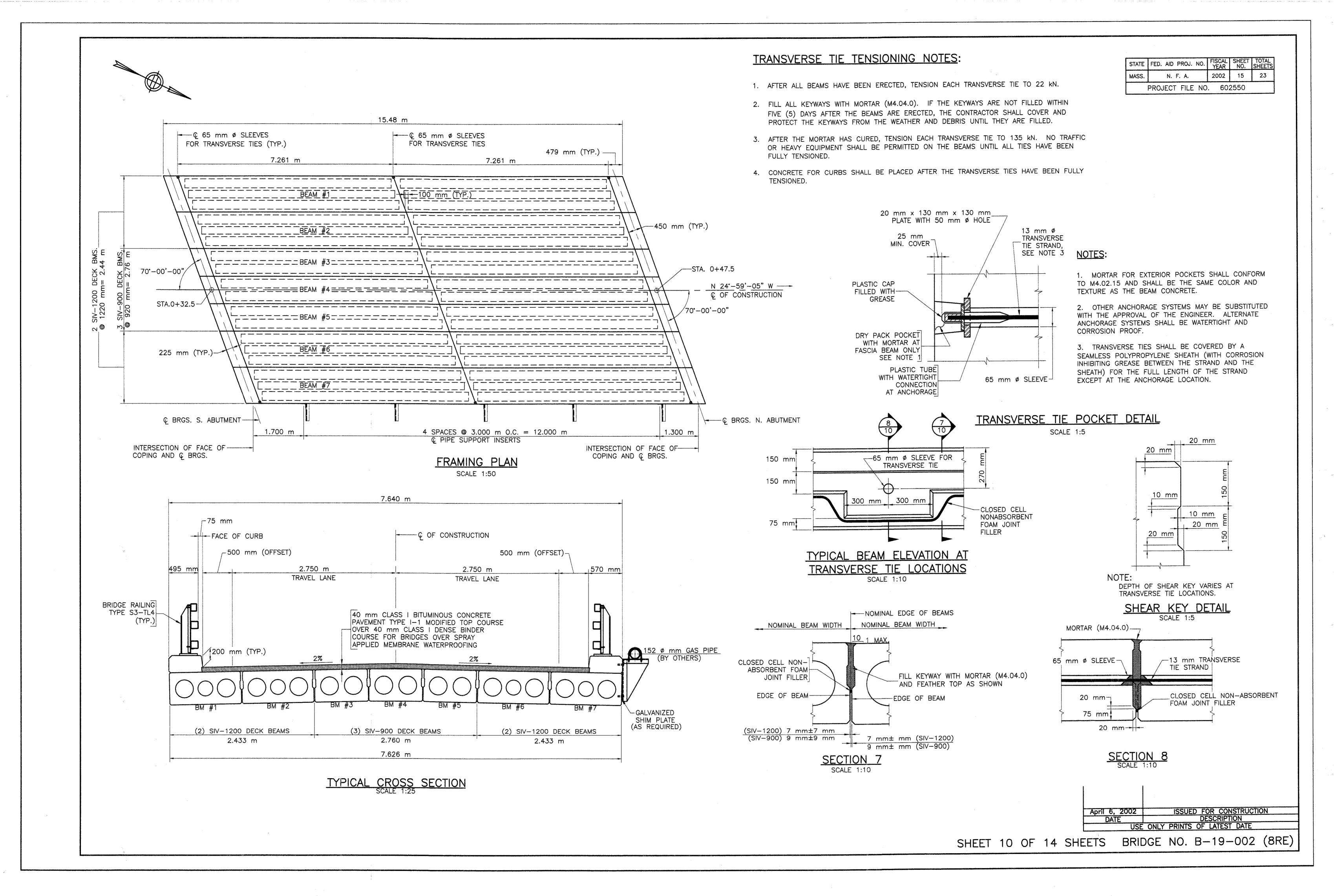
USE ONLY PRINTS OF LATEST DATE

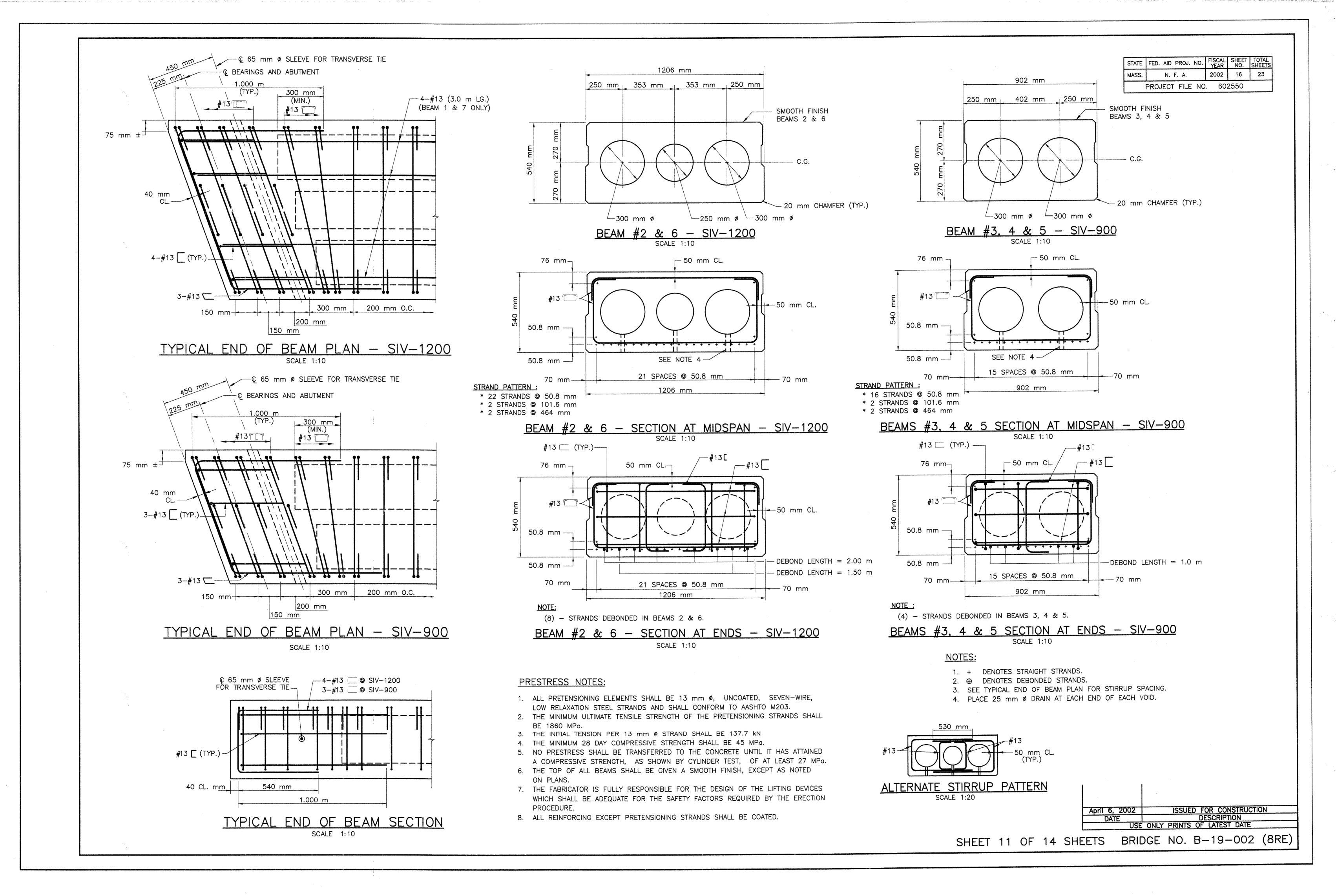




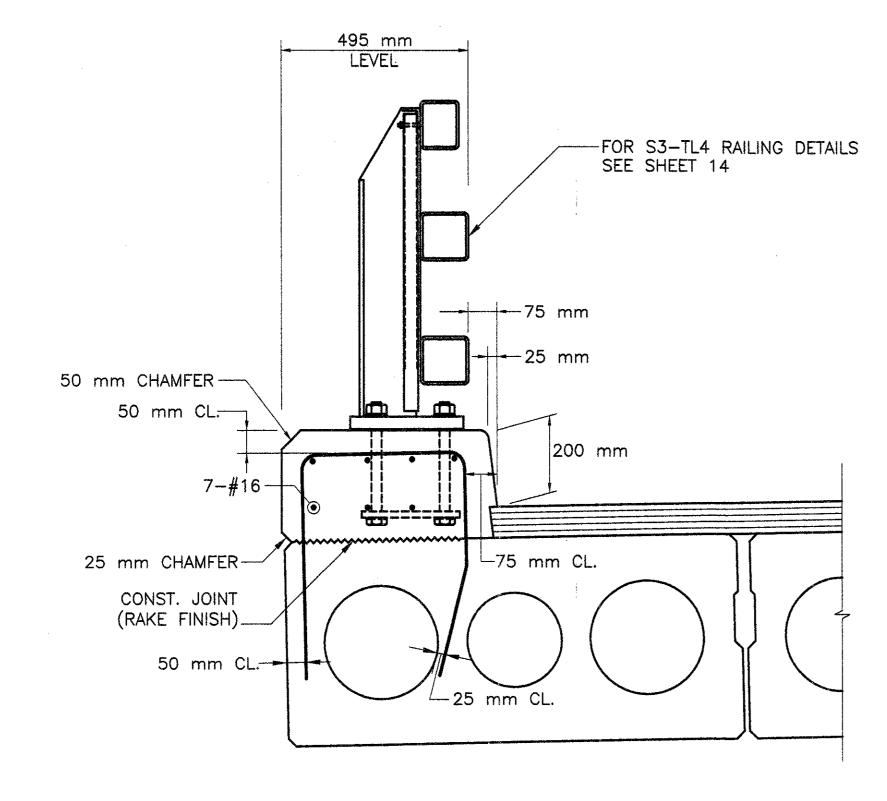




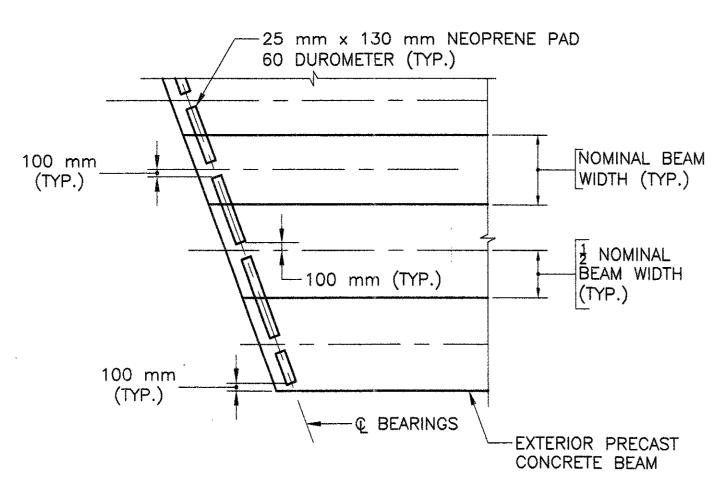




17 2002 PROJECT FILE NO. 602550



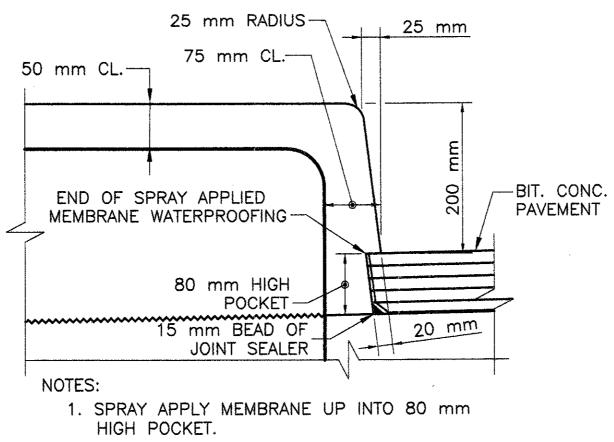


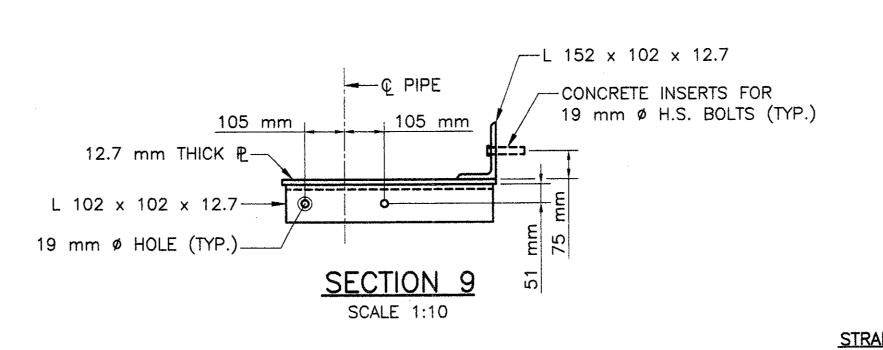


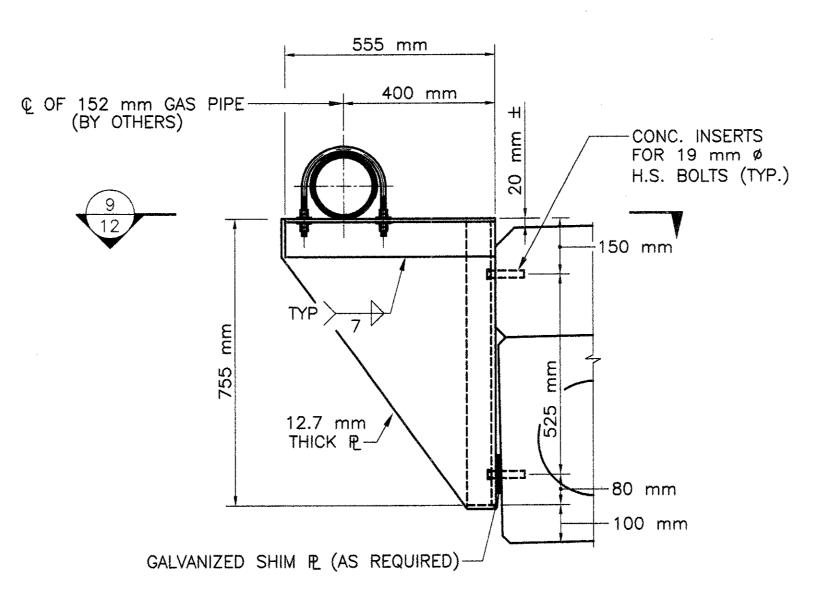
ELASTOMERIC NOTES:

- 1. ALL ELASTOMERIC BEARINGS SHALL BE
- 60 DUROMETER HARDNESS. 2. AVERAGE BEARING PRESSURE 4,130 kPa. (BASED ON FACTORED LOADS) BEARINGS ARE SUBJECT TO SHEAR DEFORMATION.

LAYOUT OF BEARINGS SCALE 1:50





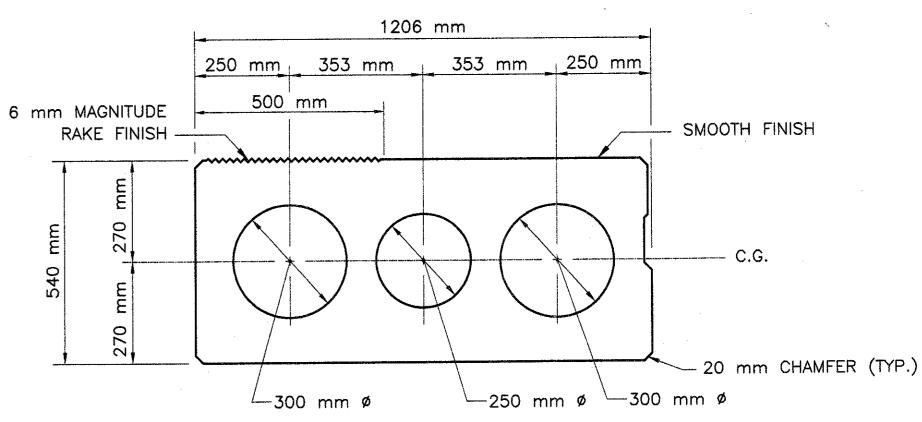


NOTES:

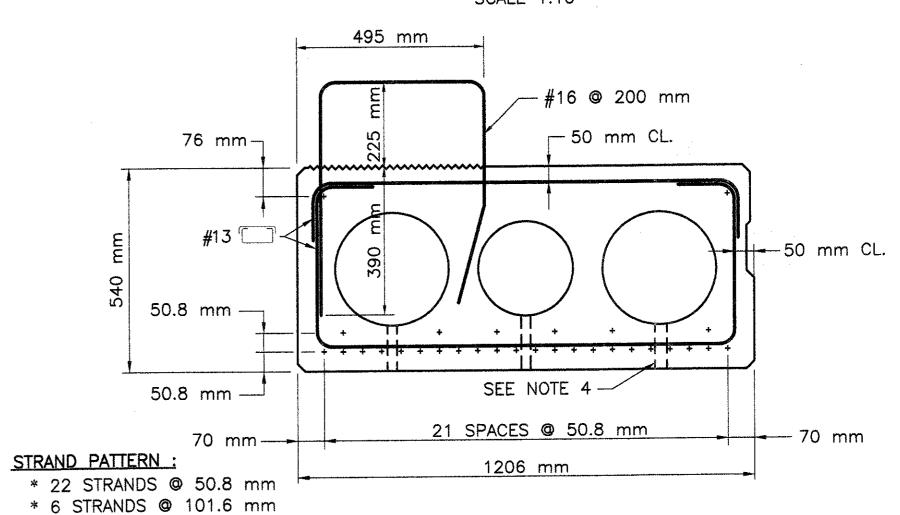
- 1. ALL STEEL SHALL CONFORM TO AASHTO M270 GR.250. 2. ALL STEEL AND FASTENERS SHALL BE GALVANIZED IN
- ACCORDANCE WITH AASHTO M111 AND M232. 3. INSERTS FOR 19 mm Ø H.S. SHALL BE CAST-IN-PLACE. THE 19 mm Ø H.S. BOLT INSERTS SHALL PROVIDE A MINIMUM ULTIMATE TENSILE CAPACITY 5 KN AND A MINIMIM ULTIMATE SHEAR CAPACITY OF OF 18.4 kN IN 20 MPa CONCRETE.

UTILITY SUPPORT DETAILS

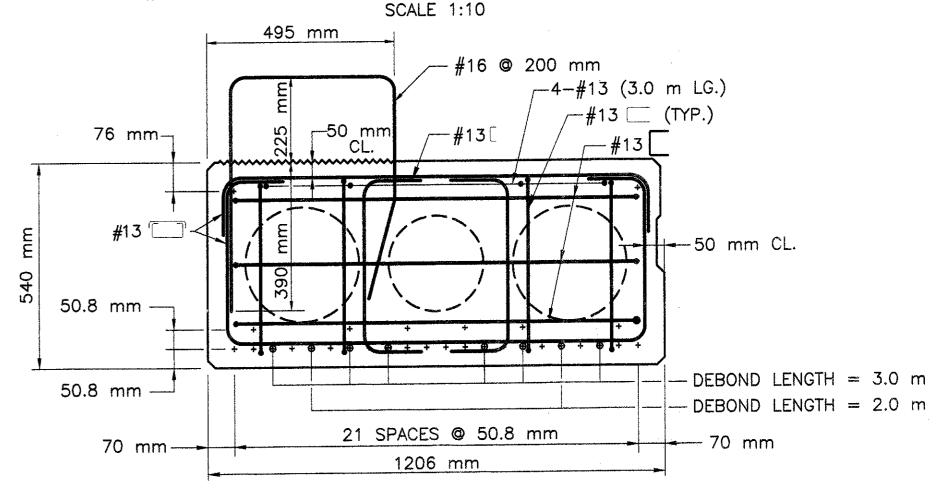
SCALE 1:10



BEAM #1 & 7 - SIV-1200



BEAM #1 & 7 - SECTION AT MIDSPAN - SIV-1200



NOTE:

(8) - STRANDS DEBONDED IN BEAMS 1 & 7.

BEAM #1 & 7 - SECTION AT ENDS - SIV-1200 SCALE 1:10

NOTES:

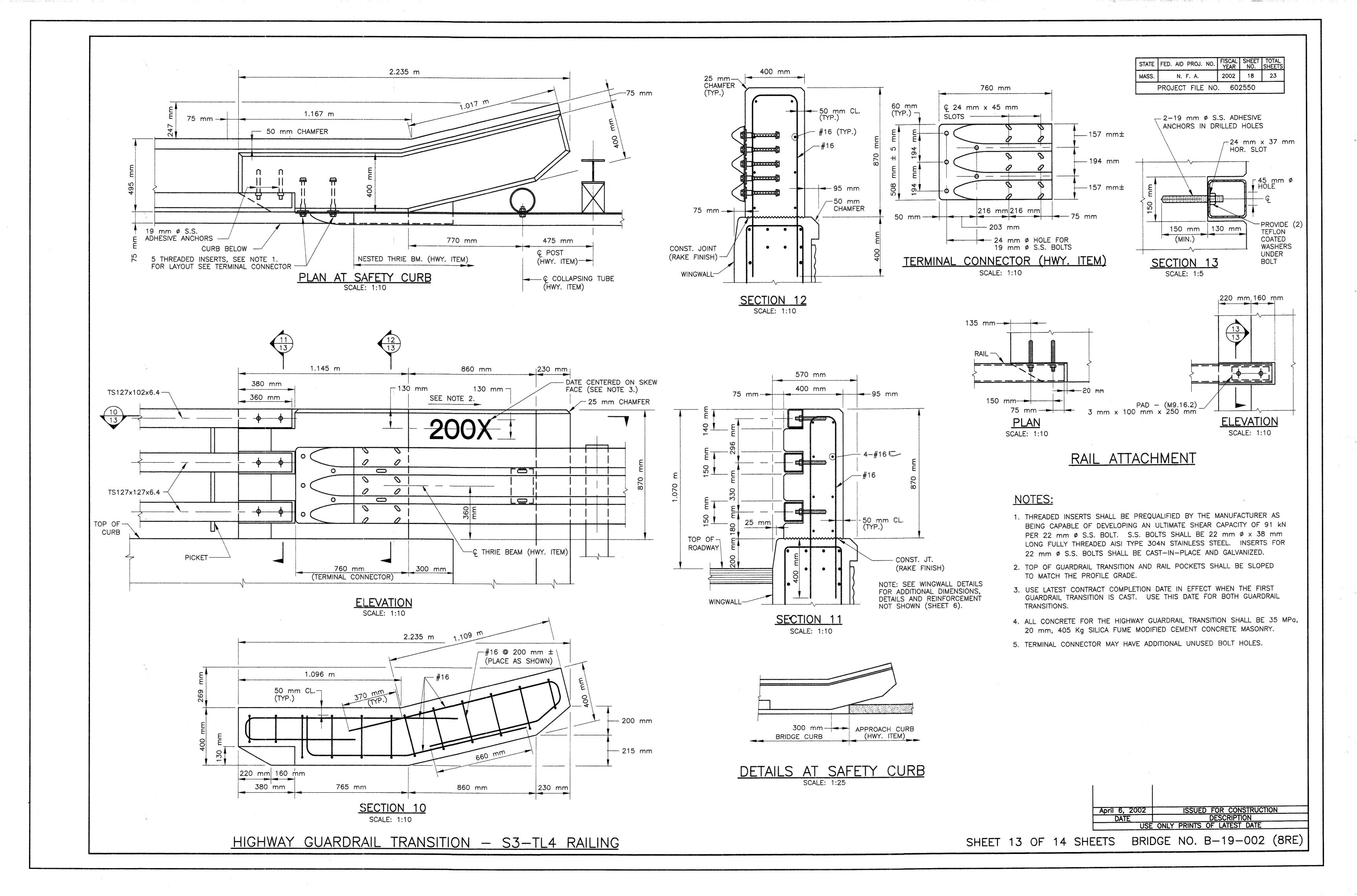
1. + DENOTES STRAIGHT STRANDS.

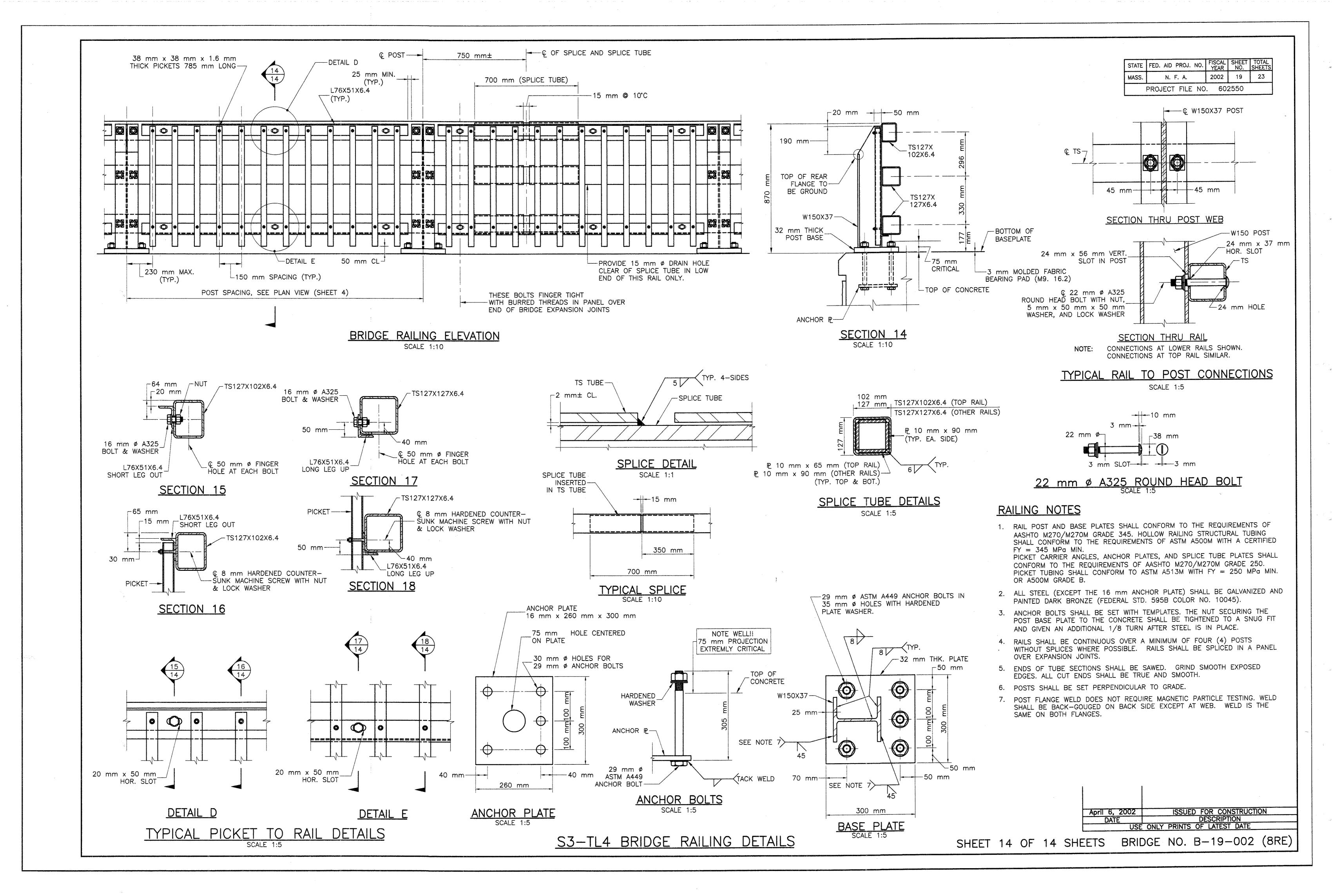
* 2 STRANDS @ 464 mm

- ⊕ DENOTES DEBONDED STRANDS.
- 3. SEE TYPICAL END OF BEAM PLAN FOR STIRRUP SPACING.
- 4. PLACE 25 mm Ø DRAIN AT EACH END OF EACH VOID.

ISSUED FOR CONSTRUCTION April 6, 2002 DESCRIPTION
USE ONLY PRINTS OF LATEST DATE

SHEET 12 OF 14 SHEETS BRIDGE NO. B-19-002 (8RE)







SOUTH ABUTMENT, SOUTHWEST CORNER

* NOTE:

4" BITUMINOUS CONCRETE PAV'T

 $1 \frac{1}{2}$ TOP COAT,

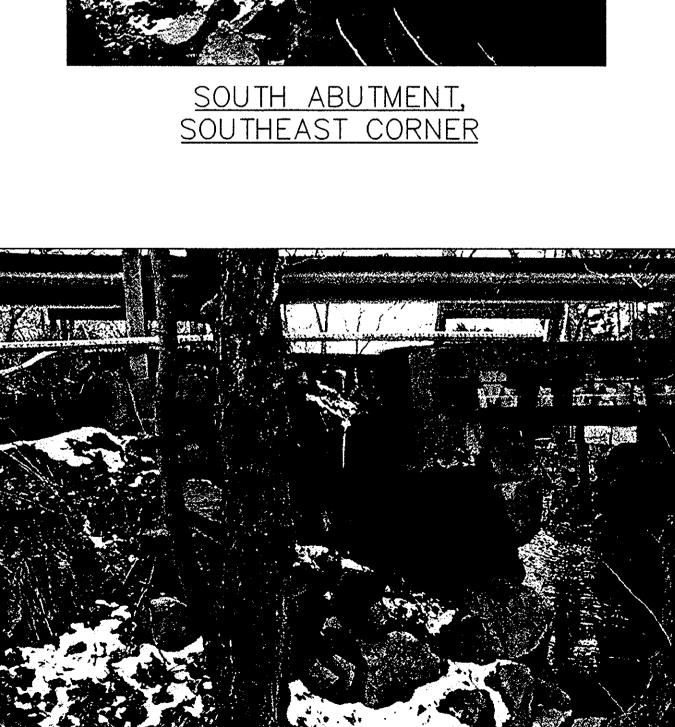
STONE

12" GRAVEL BASE

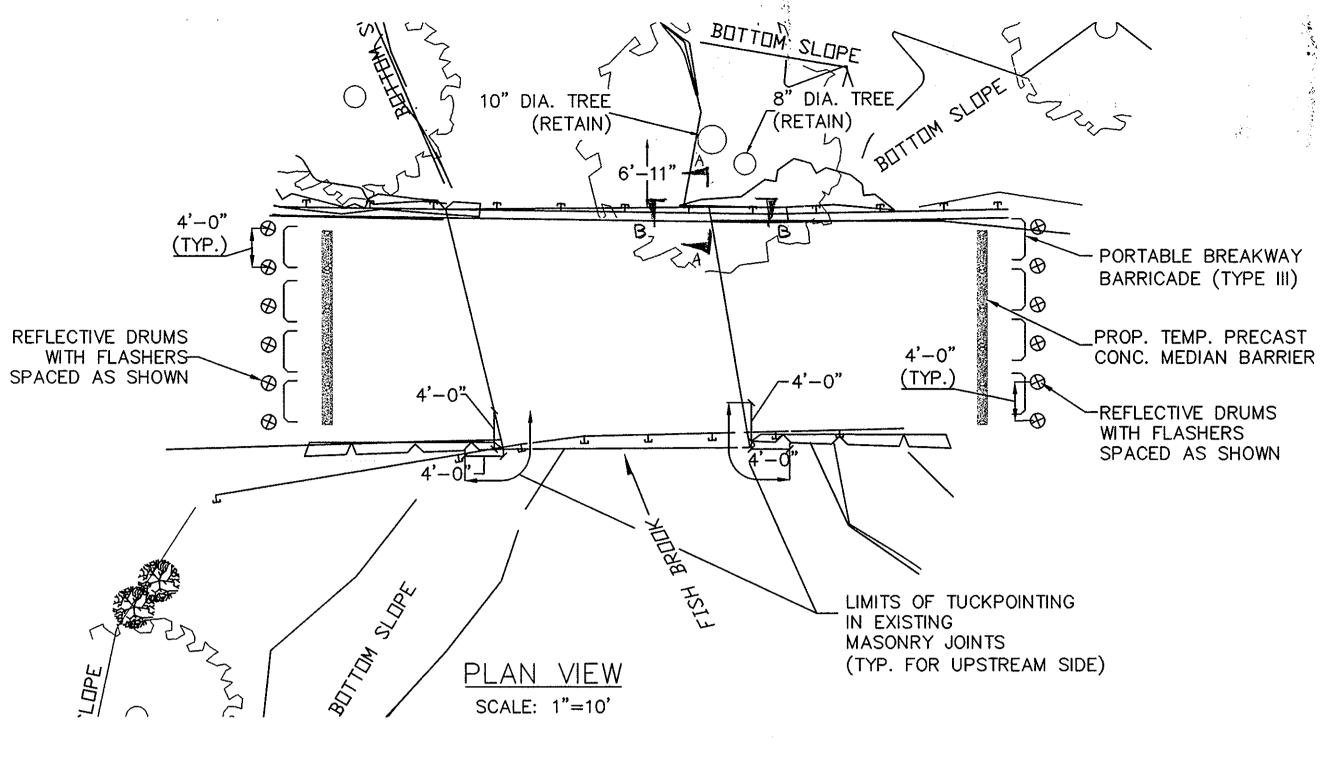
2 1/2" BINDER COURSE

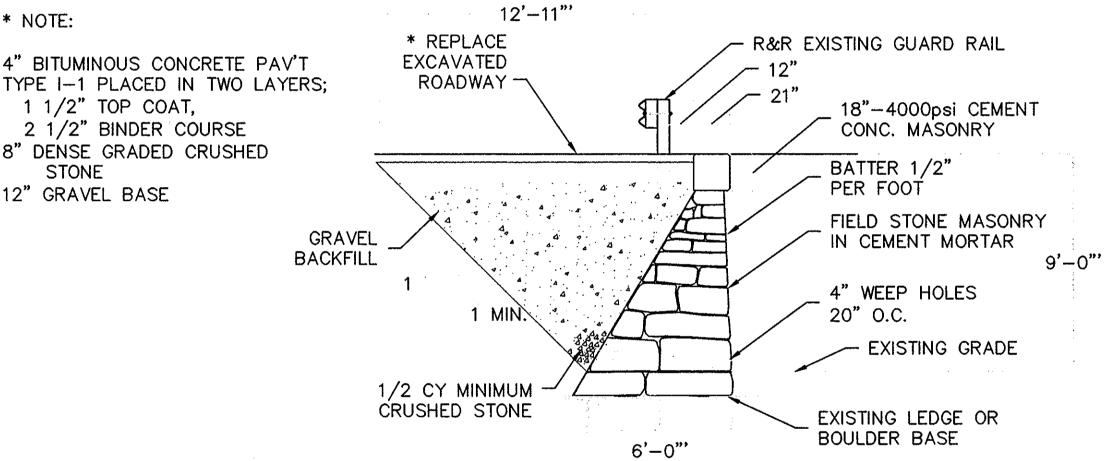
8" DENSE GRADED CRUSHED



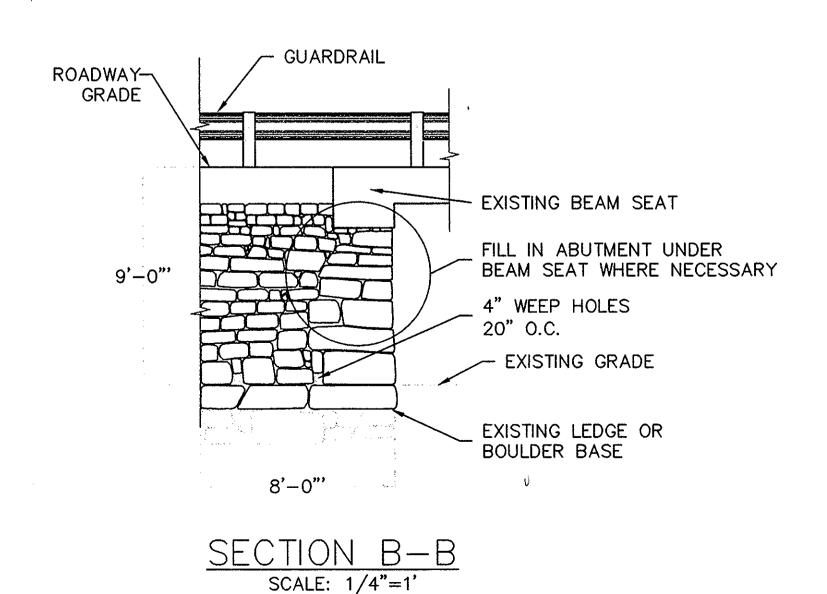


ELEVATION OF SOUTH ABUTMENT, EAST END



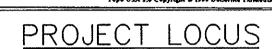


SECTION A-A SCALE: 1/4"=1'



GENERAL NOTES:

- 1. Install and/or relocate temporary drums, and other traffic maintenance devices as required for closed roadway condition.
- 2. All construction drums and other traffic maintenance devices shall conform with the 1988 MUTCD as amended.
- 3. Remove all temporary signs, drums and other traffic maintenance devices on completion of the work and upon approval of the town.
- 4. Roadway will be closed while wingwall is being repaired.
- 5. All distances may be adjusted to fit field conditions, as directed by the Engineer.



MATERIALS

690.40 General.

The stone shall consist of those in the present wall and its foundation and such new stones as may be required.

904. Concrete

Concrete shall be 4000 psi, 3/4 inch, 610 cement concrete masonry.

CONSTRUCTION METHODS

All fallen stones from the present walls to be rebuilt, shall be used to rebuild the new wall in addition to furnishing such new stones as may be necessary to provide rebuilt walls of uniform appearances and cross sectional dimensions throughout their length. The open spaces about the base of the wall shall be filled with the materials excavated from the trench and all surplus excavation shall be used as directed on the slopes of the new embankment.

B. Concrete Method of concrete placement shall be the responsibility of the contractor.

COMPENSATION

690.131 Basis of Payment.

106.25 Tuckpointing Existing Masonry Joints

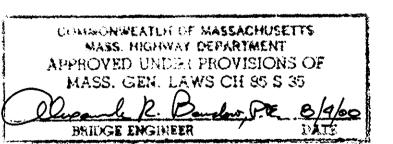
Stone Masonry Walls, Removed and Rebuilt will be paid for at the contract unit price per lump sum for the wall rebuilt, complete in place.

LINEAR FOOT

Excavation at the new location will be included under the lump sum Item 685.

<u>Payment Items:</u>

		, a	EINE/III 1001
	151.	Gravel Borrow	CUBIC YARD
	156.1	Crushed Stone for Bridge Foundation (Optional)	TON
	402.	Dense Graded Crushed Stone	CUBIC YARD
	460.	Bituminous Concrete Pavement (Type I—1)	CUBIC YARD
	482.3	Saw Cutting Bituminous Concrete	LINEAR FOOT
	685.	Field Stone Masonry in Cement Mortar	CUBIC YARD
	852.	Safety Signing for Construction Operation	SQUARE FOOT
	853.	Portable Barricade (Type III)	EACH
	853.2	Temporary Precast Concrete Median Barrier	LINEAR FOOT
	859.1	Reflectorized Drum with Flasher (Type A)	DRUM - DAY
	859.2	Reflectorized Drum with Light (Type C)	DRUM - DAY
**	904.	4000 psi. 3/4" 610 Cement Concrete Masonry	CUBIC YARD





MASSACHUSETTS HIC	HWAY	DEPAR	TMENT	BOXFORD B-19-002 (2R2) MILL STREET / FISH BROOK						
COVER SHEE	T ANI) LOC	US							
BAYSIDE ENGINEERING, INC.	DES BY	DR BY	CHK BY	SCALE: AS NOTED	SHEET 1 OF 1					
5 MIDDLESEX AVENUE SOMERVILLE, MA 02145	FXM	WF	FXM		SILLITOIT					

ATTACHMENT C: LOCKWOOD LANE OVER FISH BROOK (B-19-003)

- CULVERT INSPECTION REPORT PERFORMED BY MASSDOT ON JANUARY 11, 2019
- EXISTING BRIDGE PLANS, DATED: OCTOBER 1986
- BRIDGE REPAIR DRAWINGS, DATED: MAY 2020

WASSACHUSETTS DEPARTMENT OF TRANSPORTATIONPAGE _ 1 OF _ 13

2-DIST B.I.N. 04 2R3

STRUCTURES INSPECTION FIELD REPORT **CULVERT INSPECTION**

BR. DEPT. NO. B-19-003

BOXFORD								TRUCTURE NO. B19003-2R3-MUN-NBI						00	000.193			41-STATUS A:OPEN 106-YR REBUILT			90-ROUTINE INSP. JAN 11, 20		019			
07-FACILITY CA			30C) LN	ı				ME	MORIAL	NAME	E/L	OCAL NAI	ME					88 88	106-`	7R REE		YR R	EHAB'D 00	(NOI) 100	N 106)
06-FEATURES WATER F									1	ban l			ASS		DI	ST.	BRIDGE	ΞIN	SPECTI	ON E	NGINEI	ER	J. Did	eo		
43-STRUCTURI 919 : Iron			umi	nun	ı Cı	ulver	t			OWNER wn Age			I-MAINTAI own Age		TE	EAM	1 LEADE	R	J. Rosat	one,	P.E.	PROJ	MGR	Green II	nternat	tional
107-DECK TYP N: Not ap		ical	ble							ATHER lear		TE	EMP. (air) -1°C	;			1 MEMB (. KA									
TYPE OF	CU	LVE	RT:									Ī	BARR	ELS	: ((In N	Meters)									_
SHAPE: BOX								SIZI		.00)W	x2.00l	4		NU	MBER): 	2								
MATERIAL: ALUMINUM									DEPTI	н Оғ	· C	:0\	VER		(To the	neare	st tentl	n of a i	meter)	0.6		s 0.6				
COATING: NONE										CURB	RE	/E	AL	_				(In	millime	eters)	N		N			
							Dive	This		6				ve 1	This	,	Div	ve Rep	ort):	6	16	2 (Th	is Rep	ort):	6	
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2. Floor	N		_	— -		ankment						Deformation 6 6					M-P	ŀ								
Walls Headwall	7		- NA	—I 🗕	9.Wea 0.Railii	aring Surfa	ace	N N	6	M-P M-P	15.Sc 16.Se			_	_	6 7	S-P	-11	None ()					explair ite ()		re()
5. Wingwall	N		M-			ewalks		N	N	IVI-P	17.	lue	ement	1	\rightarrow	N	-	ŀ	ivone ()	() IV	111101 (. ,		16 ()
6. Pipe	6		M-		2.Utiliti			N	N	-	18.				_	N	-		None ()					explair ite ()		re ()
ITEM 61	CI	HAN	NEL This	& C	ΗΑΝ	INEL F	PROT	EC	TIO	N Dive This	6	1	STREAM	FLOV	v v	ELC	OCITY:			Al	PPRO	АСН	COI	NDITIC)N	
			. Rpt.	DEF						Rpt. Rpt.	DEF	_	Tidal () High (X) Moderate () Low ()										DEF			
1.Channel Scour		6	6	S-P	5. ا	Utilities				N N	-		ITF	M 61	(Di	ve i	Report	.)-	6	a.	Appr. pa	vemen	t condit	ion	5	M-P
2Embankment Er	osior	7	7	-	6.1	Rip-Rap/S	Slope Pro	tect	ion	6 6	М-Р	,			•		•			b.	Appr. Ro	oadway	Settler	ment	7	-
3Debris		7	6	M-P	7.	Aggradati	on			7 7	-		ITE	M 61	(Th	This Report): 6								-		
4.Vegetation		7	7	-									93b- U/W INS	SP DA	TE	::	03/10	0/2	2016	d.					N	
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			_	Ad	tual	Postin	g			N N	N	ı	N				Signs				E	V	_	E		W
Not Applica	ble	X		Re	com	nmende	ed Pos	tin	q [N N	N		N				(Y=Yes		=No, equired)	ŀ			\exists		JL	
				W	aived	Date:	00/0	0/0	0000		MT Da			0/000	0		Legibi Visibil		1							
ITEM 36	T	RAF	FIC	C SA	FET	'Y			A	CCES	SIBI	L	ITY	(Y	/N/F	-):						TOT	AL I	HOUR	s	60
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A. Bridge Rai					0	6 N	M-	<u> </u>	⊞	adder			N	N N	0	the	r:			N		PLA	NS	(Y/I	V):	N
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C. Approach Guardrail D. Approach Guardrail Ends O N - Waders -								T	T								•	:.R.)	(171	N).	N					
																		TAP								
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CITY/TOWN	B.I.N.	BR. DEPT. NO.	8STRUCTURE NO.	INSPECTION DATE
BOXFORD	2R3	B-19-003	B19003-2R3-MUN-NBI	JAN 11, 2019

REMARKS, PHOTOS & SKETCHES

BRIDGE ORIENTATION

Bridge No. B-19-003 (2R3) carries Lockwood Lane over Fish Brook in the town of Boxford. In this report, the approaches are labeled west and east. There are two barrels; Barrel 1 is the west barrel and Barrel 2 is the east barrel. The Fish Brook flows from north (upstream) to south (downstream).

For the framing plan, see **Sketch 1**; for the approaches, see **Photo 1** and **Photo 2**.

GENERAL REMARKS

The box culvert consists of two (2) corregated aluminum barrels, (2) reinforced concrete headwalls, and four (4) granite stone wingwalls. There are two (2) timber bridge railings and an asphalt wearing surface on the top of culvert.

ACCESS

During this inspection, access to Lockwood Lane was only possible via Middleton Road (west entrance) because the bridge to the east of Bridge No. B-19-003 (2R3) was closed.

To inspect the underside of the culvert, waist high waders and a depth probe were used.

CONDITION RATING GUIDE

	CODE	CONDITION	DEFECTS
	N	NOT APPLICABLE	Use if structure is not a culvert.
G	9	EXCELLENT	No deficiencies.
G	8	VERY GOOD	No noticeable or noteworthy differences which affect the condition of the culvert. Insignificant scrape marks caused by drift.
G	7	GOOD	Shrinkage cracks, light scaling, and insignificant spalling, which does not expose reinforcing steel. Insignificant damage caused by drift with not misalignment and not requiring corrective action. Some minor scouring has occurred near curtain walls, wingwalls, or pipes. Metal culverts have a smooth symmetrical curvature with superficial corrosion and no pitting.
F	6	SATISFACTORY	Deterioration or initial disintegration, minor chloride contamination, cracking with some leaching, or spalls on concrete or masonry walls and slabs. Local minor scouring at curtain walls, wingwalls, or pipes. Metal culverts have a smooth curvature, non-symmetrical shape, significant corrosion or moderate pitting.
F	5	FAIR	Moderate to major deterioration, or disintegration, extensive cracking and leaching, or spalls on concrete or masonry walls and slabs. Minor settlement or misalignment. Noticeable scouring or erosion at curtain walls, wingwalls, or pipes. Metal culverts have significant distortion and deflection in one section, significant corrosion or deep pitting.
Р	4	POOR	Large spalls, heavy scaling, wide cracks, considerable efforescence, or opened construction joints permitting loss of backfill. Considerable settlement or misalignment. Considerable scouring or erosion at curtain walls, wingwalls, or pipes. Metal culverts have significant distortion and deflection throughout, extensive corrosion or deep pitting.
Р	3	SERIOUS	Any condition described in Code 4 but which is excessive in scope. Severe movement or differential settlement of the segments, or loss of fill. Holes may exist in walls or slabs. Integral wingwalls, nearly severed from culvert. Severe scour or erosion at curtain walls, wingwalls, or pipes. Metal culverts have extreme distortion and deflection in one section, extensive corrosion, or deep pitting with scattered perforations.
С	2	CRITICAL	Advanced deterioration of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless closely monitored it may be necessary to close the bridge until corrective action is taken.
С	1	"IMMINENT" FAILURE	Bridge closed. Corrective action may put back in light service.
	0	FAILED	Bridge closed. Replacement necessary.

DEFICIENCY REPORTING GUIDE

DEFICIENCY: A defect in a structure that requires corrective action.

CATEGORIES OF DEFICIENCIES:

M= Minor Deficiency - (Examples include but are not limited to: Spalled concrete, minor to moderate corrosion to steel culverts, minor settlement or misalignment, minor scouring, minor damage to guardrail, etc.)

S= Severe/Major Deficiency - (Examples include but are not limited to: Large spalls, wide cracks, moderate to major deterioration in concrete, considerable settlement, considerable scouring or undermining, extensive corrosion and deflection in steel culverts, etc.)

C-S= Critical Deficiency - A deficiency in a structural component or element of a bridge that poses an extreme hazard or unsafe condition to the public. (Follow-up Critical Deficiency Report must be submitted

URGENCY OF REPAIR:

I = ImmediateA = ASAP- [Inspector(s) stay at the bridge until the District Maintenance crew or the responsible Agency crew(if not a State bridge) show up and corrective action is taken.]
[Action will be taken by the District Maintenance Engineer or the Responsible Agency (if not a State owned bridge) upon receipt of the Inspection Report].

P = Prioritize- [Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs made when funds and/or manpower is available].

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 JAN 11, 2019

REMARKS

ITEM 62 - CULVERT

Item 62.4 - Headwall

North Headwall

Moderate scaling at the north face of the wall (see Photo 3). Moss growing at the top of the wall.

South Headwall

Moderate scaling at the south face of the wall. Moss growing at the top of the wall.

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

Item 62.5 - Wingwall

Northwest Wingwall

Dry laid granite blocks misaligned vertically up to 5".

Northeast Wingwall

Dry laid granite blocks misaligned vertically up to 4". The bottom stone of the wingwall is displaced 7" and falling out (see Photo 4).

Southwest Wingwall

Dry laid granite blocks misaligned vertically up to 3". There is a 1'-10" deep x 11" high void at the bottom of the wingwall.

Southeast Wingwall

Dry laid granite blocks misaligned vertically up to 6".

<u>Item 62.6 - Pipe</u>

Barrel 1

There is a 5'-0" long x 2" deep deformation (dent) starting 10' from the south headwall and 2' east of the crown (**see Photo 5**). There is minor surface corrosion along the water-line for the entire length of the aluminum barrel and the bottom two bolts at the splices exhibit light rust.

Barrel 2

There is a minor dent at the crown of the barrel at the north headwall (see Photo 6). The aluminum at the floor of the barrel at the north end of the culvert is bent up 6" high, exposing the crushed stone under the culvert to potential scour (see Photo 7). There is minor surface corrosion along the water-line for the entire length of the barrel (see Photo 8).

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

Item 62.8 - Embankment

The trees growing on the riverbank are being undercut by flowing water, which is eroding the bank and exposing the roots, causing the trees to grow/tilt inward (see Photo 9). The upstream end has several trees growing on the west embankment displacing the stone riprap wall (see Photo 10).

<u>Item 62.9 - Wearing Surface</u>

The wearing surface exhibits 11 LF of longitudinal cracks and 15 LF of transverse cracks up to 1/2" wide. There is a heavy buildup of leaves and vegatation at the north and south curblines.

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REMARKS

<u>Item 62.14 - Deformation</u>

See Item 62.6 - Pipe.

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

Item 62.15 - Scour

There is moderate scour 2'-0" deep below the streambed between Barrel 1 and Barrel 2 at the north headwall.

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

<u>Item 61.3 - Debris</u>

Culvert

Barrel 2 has a buildup of branches, leaves, and brush 2'-0" high starting from the south end of the culvert and extending three-quarters the length of the barrel (**see Photo 11**).

Item 61.6 - Rip-Rap/Slope Protection

For underwater details, refer to the Routine Underwater Inspection Report dated 03/10/2016.

APPROACHES

Approaches a - Appr. pavement condition

West Approach

Moderate to heavy tranverse, longitudinal, and map cracking up to 1" wide (see Photo 12).

East Approach

Moderate to heavy tranverse and longitudinal cracking up to 1" wide.

TRAFFIC SAFETY

<u>Item 36a - Bridge Railing</u>

Both bridge rails consist of a timber beam mounted to timber posts. Both timber beams exhibit minor checking of the wood (see Photo 13).

Item 36b - Transitions

There are no transition rails.

<u>Item 36c - Approach Guardrail</u>

There are no approach guardrails.

<u>Item 36d - Approach Guardrail Ends</u>

There are no approach guardrail ends.

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REMARKS

Sketch / Photo Log

Sketch 1 : Framing Plan Photo 1 : West Approach Photo 2 : East Approach

Photo 3: Moderate Scaling to the North Headwall at the West End of the Culvert Photo 4: Bottom Stone at the Northeast Wingwall Dislodged and Falling Out

Photo 5: Barrel 1, 5'-0" Long Deformation Starting 10' away from the South Headwall and 2' East of the

Crown

Photo 6: Barrel 2, Dent at the Crown of the Barrel at the North Headwall

Photo 7: Barrel 2, Aluminum Bent up 6" High at the Floor of the Barrel at the North End of the Culvert

Photo 8: Barrel 2, Typical Surface Corrosion along the Waterline

Photo 9: Minor Erosion, Exposed Roots, and Trees Tilting Towards the Water at the Southeast

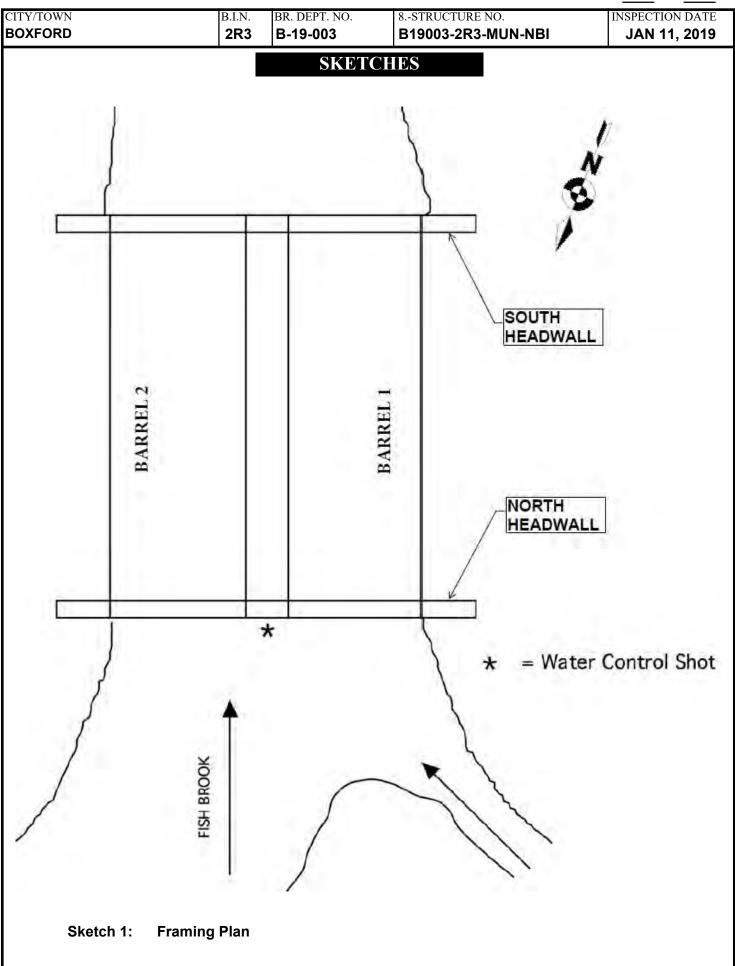
Embankment

Photo 10: Trees Tilting Towards the Water Displacing the Northwest Stone Embankment

Photo 11: Barrel 2, 2'-0" High Buildup of Branches, Leaves, and Brush Starting from the South End of the

Culvert and Extending Three-Quarters the Length of the Barrel

Photo 12: Heavy Map Cracking in the Westbound (North) Lane at the West Approach
Photo 13: Bridge Railing, Minor Checking to the Upper Portion of the Timber Beams



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Photo 1: West Approach



Photo 2: East Approach



Photo 3: Moderate Scaling to the North Headwall at the West End of the Culvert



Photo 4: Bottom Stone at the Northeast Wingwall Dislodged and Falling Out

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BOXFORD 2R3 B-19-003 B19003-2R3-MUN-NBI JAN 11, 2019

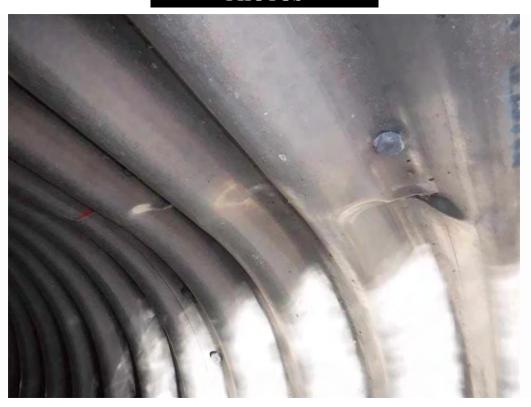


Photo 5: Barrel 1, 5'-0" Long Deformation Starting 10' away from the South Headwall and 2' East of the Crown



Photo 6: Barrel 2, Dent at the Crown of the Barrel at the North Headwall



Photo 7: Barrel 2, Aluminum Bent up 6" High at the Floor of the Barrel at the North End of the Culvert



Photo 8: Barrel 2, Typical Surface Corrosion along the Waterline



Photo 9: Minor Erosion, Exposed Roots, and Trees Tilting Towards the Water at the Southeast Embankment



Photo 10: Trees Tilting Towards the Water Displacing the Northwest Stone Embankment



Photo 11: Barrel 2, 2'-0" High Buildup of Branches, Leaves, and Brush Starting from the South End of the Culvert and Extending Three-Quarters the Length of the Barrel



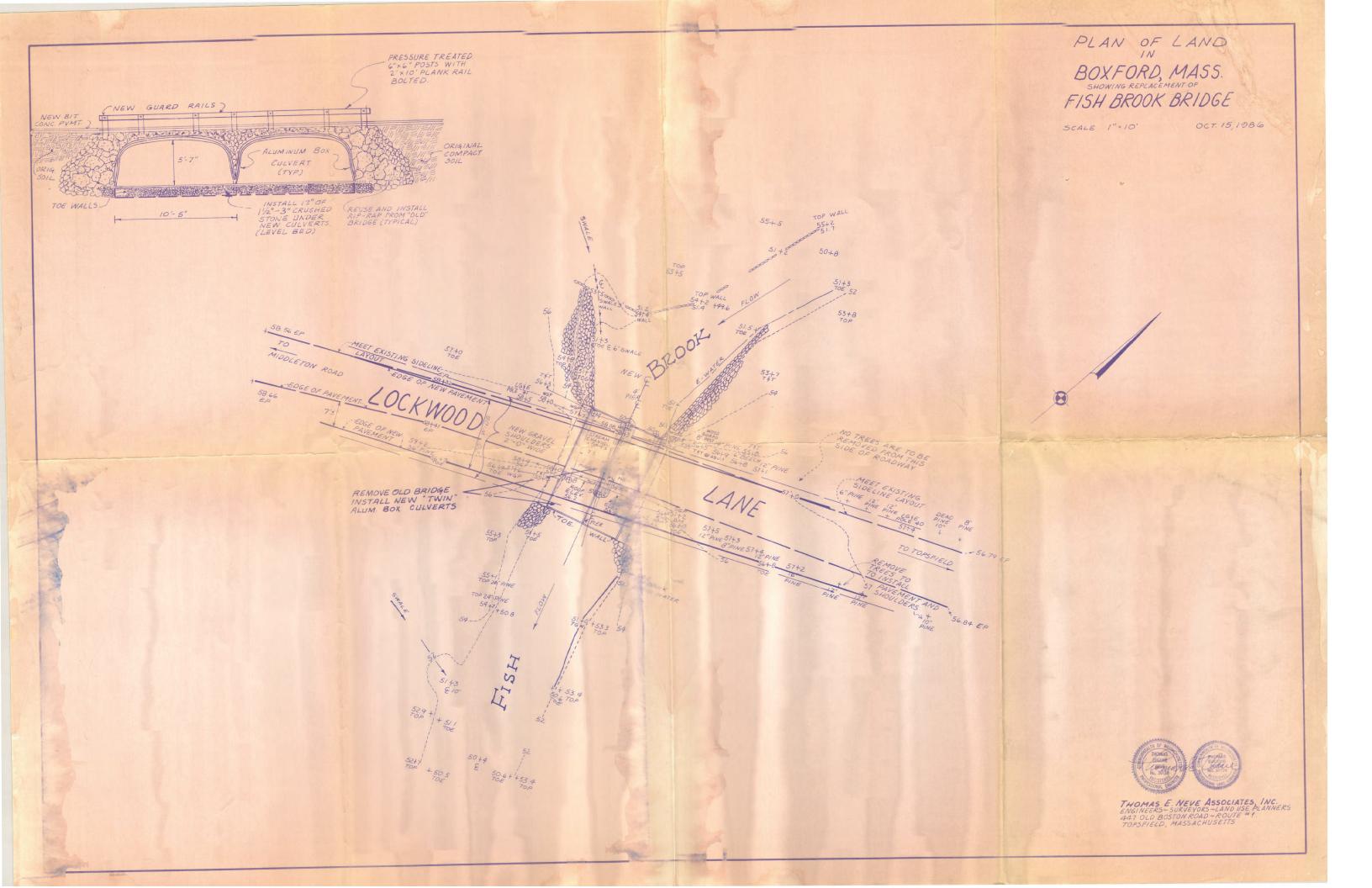
Photo 12: Heavy Map Cracking in the Westbound (North) Lane at the West Approach

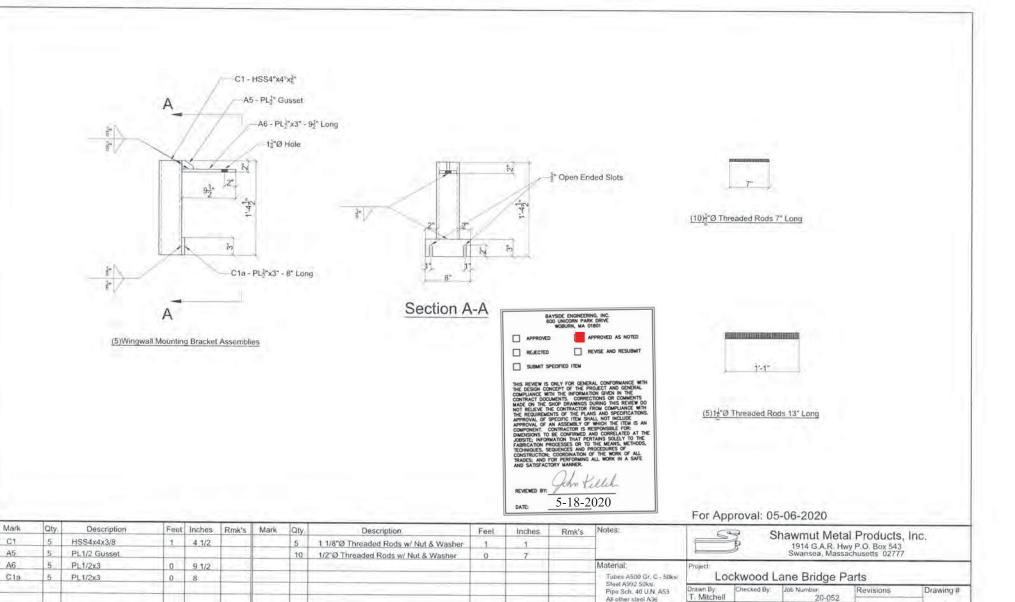
CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE
BOXFORD 2R3 B-19-003 B19003-2R3-MUN-NBI JAN 11, 2019



Photo 13: Bridge Railing, Minor Checking to the Upper Portion of the Timber Beams

State Information		Classification	Code
BDEPT#= B19003 Agenc	y Br.No.	(112) NBIS Bridge Length	Υ
Town= Boxford	L.O.	(104) Highway System	N
B.I.N= 2R3	AASHTO= 079.9	(26) Functional Class - Urban Local	19
RANK= 4218 H.I.= 88.5 % Identification FHW	'A Select List= N (6/21/2017)	(100) Defense Highway	(
(8) Structure Number	B190032R3MUNNBI	(101) Parallel Structure	1
(5) Inventory Route	151000000	(102) Direction of Traffic - 2-way traffic	2
(2) State Highway Department District	04	(103) Temporary Structure	١
(3) County Code 009 (4) Place code	07420	(105) Federal Lands Highways	(
(6) Features Intersected	WATER FISH BROOK	, ,	١
(7) Facility Carried	HWY LOCKWOOD LN	(20) Toll - On free road	
(9) Location (11) Kilometerpoint	MILE WEST I 95 0000.193	(21) Maintain - Town Agency (22) Owner - Town Agency	03 03
(12) Base Highway Network	0000.193 N		
• • •	00000000	Condition	Code
·	DEG 38 MIN 40.80 SEC	(58) Deck	
	DEG 59 MIN 24.20 SEC	(59) Superstructure	N
(98) Border Bridge State Code	Share %	(60) Substructure	N
(99) Border Bridge Structure No. #		(61) Channel & Channel Protection	6
Structure Type and Material		(62) Culverts Load Rating and Posting	€ Cod∈
(43) Structure Type Main: Iron or Aluminum	Code 919	(31) Design Load - H 20=M 18	Code
Culvert Jointless bridg	e type: Not applicable	(63) Operating Rating Method - Load Factor (LF)	1
(44) Structure Type Appr:		(64) Operating Rating	56.
Other	Code 000	(65) Inventory Rating Method - Load Factor (LF)	1
(45) Number of spans in main unit	002	(66) Inventory Rating	33.8
(46) Number of approach spans	0000	(70) Bridge Posting	5
(107) Deck Structure Type - Not applicable	Code N	(41) Structure - Open Appraisal	A Code
(108) Wearing Surface / Protective System:	and Onda N	(67) Structural Evaluation	0000
A) Type of wearing surface - Not applicable=no d		(68) Deck Geometry	4
B) Type of membrane - Not applicable=no d		(69) Underclearances, vert. and horiz.	N
C) Type of deck protection - Not applicable=no d Age and Service		(71) Waterway adequacy	8
(27) Year Built	1988	(72) Approach Roadway Alignment	8
(106) Year Reconstructed	0000	(36) Traffic Safety Features 0	
(42) Type of Service: On - Highway	0000	(113) Scour Critical Bridges Inspections	8
Under - Waterway	Code 15	(90) Inspection Date 01/07/21 (91) Frequency	24 N
(28) Lanes: On Structure 02	Jnder structure 00	(92) Critical Feature Inspection: (93) 0	CFI DAT
(29) Average Daily Traffic	000300	(A) Fracture Critical Detail N 00 MO A)	00/00/
(30) Year of ADT 2017 (109) Truck ADT	05 %	(B) Underwater Inspection Y 36 MO B)	03/15/
(19) Bypass, detour length	005 KM	(C) Other Special Inspection N 00 MO C)	00/00/
Geometric Data		(*) Other Inspection (FLOOD) N 00 MO *)	05/23/
(48) Length of maximum span	0003.0 M	(*) Closed Bridge N 00 MO *)	00/00/
(49) Structure Length (50) Curb or sidewalk: Left 00.0 M	00006.4 M Right 00.0 M	(*) UW Special Inspection N 00 MO *) (*) Damage Inspection MO *)	00/00/
(51) Bridge Roadway Width Curb to Curb	Right 00.0 M 006.3 M	(*) Damage Inspection MO *) Rating Loads	00/00/
(52) Deck Width Out to Out	009.1 M		ype HS
(32) Approach Roadway Width (w/shoulders)	006.1 M	Operating 35.0 58.0 92.0	63.0
(33) Bridge Median - No median	Code 0	Inventory 21.0 35.0 55.0 Field Posting	38.0
(34) Skew 08 DEG (35) Structure Flare		Status LEGAL Posting Date 12/20/11	
(10) Inventory Route MIN Vert Clear	99.99 M	· ·	ngle
(47) Inventory Route Total Horiz Clear	07.5 M	Actual	J
(53) Min Vert Clear Over Bridge Rdwy	99.99 M	Recommended	
(54) Min Vert Underclear ref N	00.00 M	Missing Signs N Misc.	
(55) Min Lat Underclear RT ref N	00.0 M	Bridge Name	
(56) Min Lat Underclear LT	00.0 M	N Anti-missile fence N Acrow Panel N Jointless Bri	dge
Navigation Data		Freeze/Thaw N : Not Applicable	3-
	erway Code 0	Accessibility (Needed/Used)	
	<u> </u>		
(111) Pier Protection	Code	N / N Liftbucket N / N Rigging N / N Other	r
(111) Pier Protection (39) Navigation Vertical Clearance	000.0 M	N / N Liftbucket N / N Rigging N / N Other N / N Ladder N / N Staging	r
(111) Pier Protection			





All bolts A325 U.N.

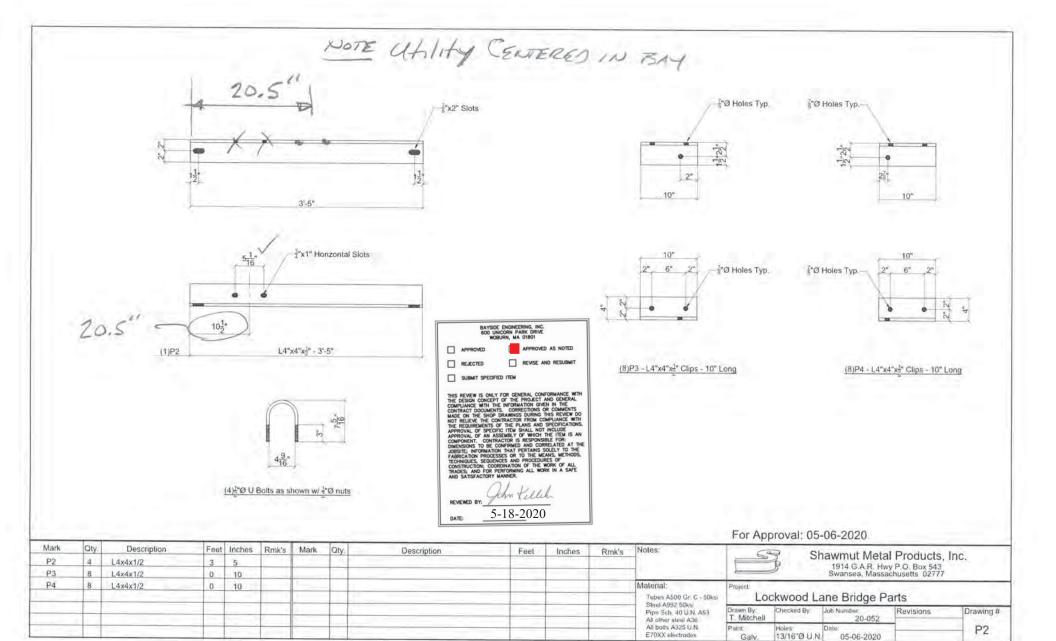
E70XX electrodes

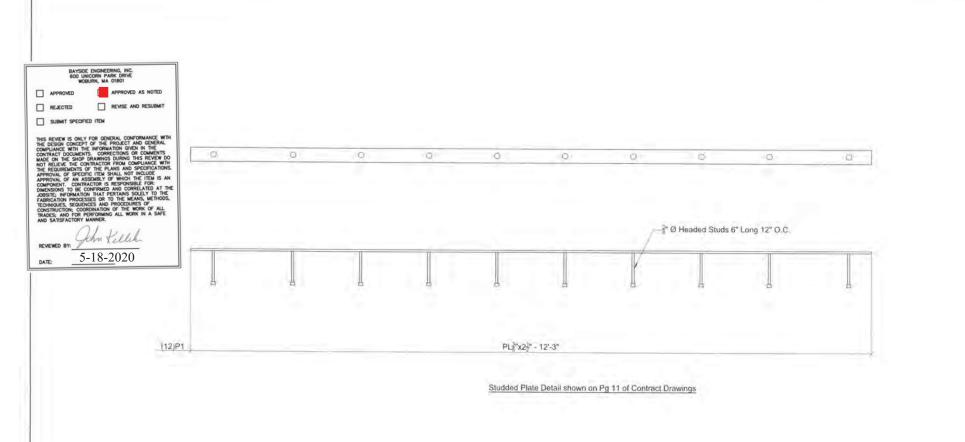
Paint.

Galv

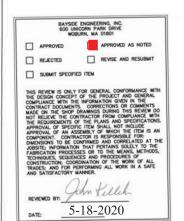
13/16"Ø U.N.

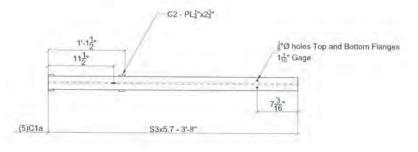
05-06-2020





Mark	Qly.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes:	E	9 9	nawmut Meta	Droducto	Inc
P1	12	PL3/8x2 1/2	12	3										1914 G.A.R. Hwy P.O. Swansea, Massachuset			mc.
												Material: Tubes A500 Gr. C - 50ksi	Project:	Lockwood Lane Bridge Parts			
												Steel A992 50ksi Pipe Sch. 40 U.N. A53 All other steel A36	Drawn By: T. Mitchell	Checked By: Job Number 20-052		Revisions	Drawing
												All bolts A325 U.N. E70XX electrodes	Paint Galv.	Holes: 13/16"Ø U.N.	Date		P3







11½" 73.*
3'-8"

C2 - PL 1"x1" - 21" Long

Wingwall Posts

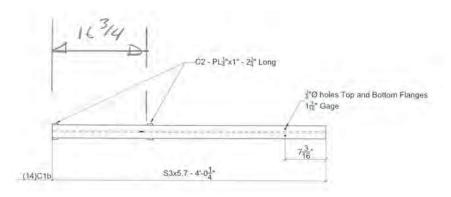
Mark	Qty.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes:	Ca	9 9	awmut Meta	Droducto	Inc
C1a	5	S3x5.7	3	8		7								1	1914 G.A.R. Hw		IIIC.
C2	20	PL1/4x1	0	2 3/4							0			Swansea, Massachusetts 02777			
				- 4	1.						100	Material:	Project:				
					Jan 198		-					Tubes A500 Gr. C - 50ksi	Loc	Lockwood Lane Bridge Parts			
_	-											Steel A992 50ksi Pipe Sch. 40 U.N. A53 All other steel A36	Drawn By. T_Mitchell	awn By. Checked By. Job Number. Revisions Dr. Mitchell 20-052		Drawing	
			_				+++					All bolts A325 U.N.	Paint:			P4	
	_											E70XX electrodes	Galv.	13/16"Ø U.N.	05-06-2020	1	



CONSTRUCTION: COORDINATION OF THE WORK OF TRADES, AND FOR PEPFORMING ALL WORK IN A AND SATISFACTORY MANNER.

John Kelleh

TE: 5-18-2020



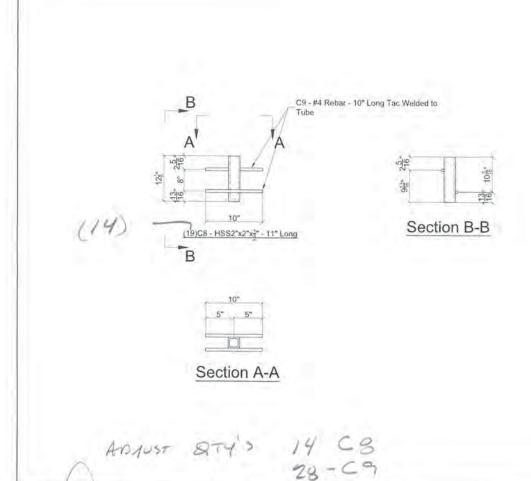
23"

2'-82" 4'-01"

or refer entire

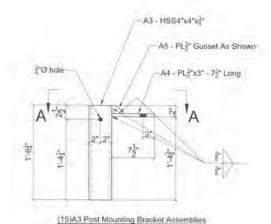
Bridge Rail Posts

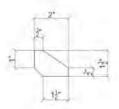
Mark	Qty.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes:	C 9			Products,	Inc
C1b	14	S3x5.7	4	0 1/4	-		i Y							1	1914 G.A.R. Hw		illo.
C2	56	PL1/4x1	0	2 3/4									1		Swansea, Massa	chusetts 02777	
										1 n		Material:	Project				
			0.00									Tubes A500 Gr, C - 50ksi	Loc	ckwood L	ane Bridge P	arts	
												Steel A992 50ksi Pipe Sch. 40 U.N. A53	Drawn By: T. Mitchell	Checked By:	Job Number:	Revisions	Drawing #
			4.5		1000							All other steel A36	_	-	20-052	_	20.
			-									All bolts A325 U.N. E70XX electrodes	Paint: Galv.	13/16"Ø U.N	Date: 05-06-2020		P5



	600 UNI	ENGINES ICORN PA URN, MA	RING, INC. ARK DRIVE 01801
	APPROVED	į	APPROVED AS NOTED
	REJECTED		REVISE AND RESUBMIT
	SUBMIT SPECIFIE	D ITEM	
NOT THE APPE APPE COME JOBS FABR TECH CONS TRACE	RELIEVE THE CON REQUIREMENTS OF ROVAL OF SPECIFI- ROVAL OF AN ASS PONENT. CONTRA NOTE: INFORMATION BICATION PROCESS MIQUES, SEQUENCE STRUCTION. COORS	TRACTOR THE PROPERTY OF THE P	S DURING THIS REVIEW DO R FROM COMPILANCE WITH LANS AND SPECIFICATIONS. SHALL NOT INCLUDE OF WHOLD THE ITEM IS AN RESPONSIBLE FOR: AT THE AND CORRELATED AT THE AND CORRELATED AT THE AND CORRELATED AT THE AND CORRELATED AT OFFICE CORRELATED OFFICE

Mark	Qty.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes:	6	a 9	nawmut Meta	Producte	Inc
C8	19	HSS2"x2"x1/4"	1	0 1/8			19	1"Ø Hex Bolt F436 GR.1 Galv.	1	3 1/4				1	1914 G.A.R. Hw		mo.
C9	38	#4 Rebar	0	10	1		1 1	1000 0000000000000000000000000000000000							Swansea, Massa	chusetts 02777	
	V					1						Material:	Project.	ect			
	1											Tubes A500 Gr C - 50ks	Lo	Lockwood Lane Bridge Parts			
							-					Steel A992 50ksi Pipe Sch. 40 U.N. A53 All other steel A36	Drawn By. T. Mitchell	Checked By:	Job Number: 20-052	Revisions	Drawing #
	-				1							All boits A325 U.N.	Paint	Holes	Date:		P6
				4						4		E70XX electrodes	Galv.	13/16"Ø U.N	05-06-2020		FO





5-18-2020

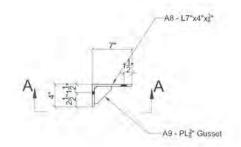
REJECTED

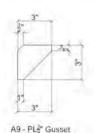
A3 - HSS4"x4"x8" A5 - PL2 Gusset As Shown A4 - PL2"x3" - 72" Long

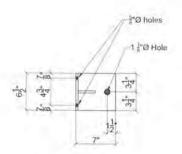
Section A-A

Aby. RTY

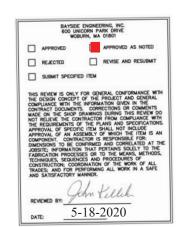
Mark	Qty.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes:	6	a Sh	awmut Meta	I Products 1	nc
A3	15	HSS4x4x3/8	1	6 3/4	1						1		- 30	1	1914 G.A.R. Hw		116.
A4	15	PL1/2x3	0	7 1/2									-	9	Swansea, Massa	chusetts 02777	
A5	15	PL1/2 Gusset							-			Material:	Project	Land I	200	above of	
			4.77.91			1						Tubes A500 Gr. C - 50ks	Loc	kwood La	ane Bridge P	arts	
												Steel A992 50ksi Piper Sch. 40 U.N. A53 All other steel A36	Drawn By: T. Mitchell	Checked By:	Job Number: 20-052	Revisions	Drawing #
												All bolts A325 U.N E70XX electrodes	Paint: Galv.	Holes: 13/16"Ø U.N.	Date: 05-06-2020		P7

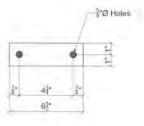






Section A-A





A10 - PLa Retainer Bracket

MADJUST QTY 14 EARH

Mark	Qty.	Description	Feet	Inches	Rmk's	Mark	Qty.	Description	Feet	Inches	Rmk's	Notes.	Ca	e sh	awmut Meta	Products I	nc
A8	15	L7x4x3/8	0	6 1/2	1	1111			11-7					1	1914 G.A.R. Hw		iii.
A9	15	PL3/8 Gusset		The state of						1			-		Swansea, Massa	chusetts 02777	
A10	15	PL3/8x2	0	6 1/2			100		-			Material:	Project				
				1.00								Tubes A500 Gr. C - 50ksi	Loc	ckwood La	ine Bridge P	arts	
	1				1							Steel A992 50ksi Pipe Sch. 40 U.N. A53	Drawn By	Checked By.	Job Number	Revisions	Drawing #
					11							All other sleet A36	T. Mitchell		20-052		*I 1 dd * 1
												All bolts A325 U.N. E70XX electrodes	Paint. Galv.	Holes: 13/16"Ø U.N.	Date: 05-06-2020		P8

TOLERANCE 1	
DESCRIPTION	TOLERANCE (INCHES U.N.O.)
OVERALL HEIGHT	-0.000, +0.250
OVERALL PLAN DIMENSIONS	-0.000, +0.125
URETHANE DISC THICKNESS	-0.000, +0.063
URETHANE DISC PLAN DIMENSIONS ≤ 20"	-0.000, +0.125
URETHANE DISC PLAN DIMENSIONS > 20"	-0.000, +0.125
URETHANE DISC FLATNESS	0.002 X NOM. DIMENSION
STEEL PLATE THICKNESS	±0.063
STEEL PLATE PLAN DIMENSIONS ≤ 30"	±0.250
STEEL PLATE PLAN DIMENSIONS > 30"	±0.250
STEEL PLATE FLATINESS IN CONTACT WITH BEARING	0.001 X NOW, DIMENSION
STEEL PLATE FLATNESS: GROUT OR CONCRETE SIDE	0.005 X NOM. DIMENSION
STEEL PLATE FLATNESS: STEEL GIRDER SIDE	0.002 X NOM. DIMENSION
STEEL PLATE FLATNESS: STEEL PLATE SIDE	0.001 X NOM. DIMENSION
STEEL PLATE SURFACE FINISH IN CONTACT WITH BEARING	125 µ" RMS
BEVEL SLOPE	±0.002 RAD
ANCHOR HOLE OR SLOT SIZE	±1/8
ANCHOR HOLE OR SLOT LOCATION	±1/8
GUIDE BAR CONTACT SURFACE: DIMENSION	-0.000, +0.125
GUIDE BAR CONTACT SURFACE: FLATNESS	0.001 X NOM. DIMENSION
GUIDE BAR CONTACT SURFACE: SURFACE FINISH	32 μ" RMS
GUIDE BAR PARALLEUSM	±0.005 RAD
DISTANCE BETWEEN GUIDE BARS	-0.000, +0.030
STAINLESS STEEL THICKNESS	-0.000, +0.063
STAINLESS STEEL PLAN DIMENSIONS	-0.000, +0.125
STAINLESS STEEL FLATNESS	0.001 X NOM. DIMENSION
STAINLESS STEEL SURFACE FINISH: SLIDING SURFACE	MIRROR #8
PTFE THICKNESS	-0.000, +0.063
PTFE PLAN DIMENSIONS	-0.000, +0.030
PTFE FLATNESS	0.001 X NOM. DIMENSION
SHEAR RESTRICTING ELEMENT DIMENSIONS	-0.000, +0.005
SHEAR RESTRICTING ELEMENT FLATNESS/OUT OF ROUND	0.001 X NOM, DIMENSION
SHEAR RESTRICTING ELEVENT SURFACE FINISH	32 μ" RMS
OTHER MACHINES PARTS THICKNESS	-0.000, +0.063
OTHER WACHINED PARTS PLAN DIMENSIONS	-0.000, +0.063
OTHER WACHINED PARTS FLATNESS	0.002 X NOM. DIMENSION
OTHER MACHINED PARTS SURFACE FINISH	63 u" RMS

GENERAL NOTES:

1. MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 6TH EDITION, SECTION 14, ASSHTO LRTD BRIDGE CONSTRUCTION SPECIFICATIONS 3RD EDITION, SECTION 18, THE MASSACHUSETTS HIGHWAY CONTRACT PLANS, AND THE PROJECT SPECIAL PROVISIONS SHALL GOVERN.

2. PTFE SHALL BE VIRGIN, UNFILLED POLYTETRAFLUOROETHYLENE ON THE SLIDING SURFACE. RESIN FOR THE PTFE SHALL SATISFY THE REQUIREMENTS OF ASTM D4894.

3. PIFE IS TO BE PURCHASED ETCHED ON ONE SIDE FOR BONDING INTO MACHINED RECESS. STEEL MATING SURFACES OF PIFE SHALL BE GRIT BLASTED AND DEGREASED PROR TO APPLICATION OF ADHESIVE. ADHESIVE SHALL BE APPLIED USING DIRECTIONS SUPPLIED BY THE ADHESIVE MANUFACTURER. CORNERS OF THE PIFE SHALL BE FILLETED TO ACCOMMODATE THE RADIUS OF THE MACHINED RECESS.

4. THE POLYETHER URETHANE DISC SHALL MEET THE PHYSICAL PROPERTIES IN TABLE 18.3.2.8-1 OF THE AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS (3RD EDITION, 2010) SUMMARIZED IN THE TABLE ON THIS SHEET. HARDNESS SHALL BE 62 ±3 DUROMETER, SHORE 'D'. INTERMEDIATE VALUES SHALL BE DETERMINED BY INTERPOLATION. EACH DISC SHALL BE MOLDED AS A SINGLE (MONOLITHIC) PIECE. THE TOP AND BOTTOM OF THE DISC SHALL BE ROUGHENED.

5. WELDING SHALL CONFORM TO THE MOST CURRENT VERSION OF ANSI/AASHTO/AWS-D1.5 BRIDGE WELDING CODE.

6. HOLES MUST BE DRILLED OR SUB-PUNCHED AND REAMED TO FULL SIZE.

7. ALL SHARP CORNERS OF STEEL MATERIALS SHALL BE REMOVED BY GRINDING OR

8. HLMR "DISC" STYLE BEARING MANUFACTURING FACILITY AND REPRESENTATIVE FOR COORDINATING PRODUCTION: THE D.S. BROWN COMPANY

300 FAST CHERRY STREET NORTH BALTIMORE, OHIO 45872

CSR - ERIC JOHNSON - (419) 257-3561

MARKING NOTES:

1. EACH BEARING SHALL BE MARKED WITH THE MANUFACTURER'S NAME, PROJECT NUMBER, THE BEARING TYPE OR MODEL NUMBER, THE BEARING NUMBER, UPSTATION, AND THE INSTALLED LOCATION. CENTERUNES SHALL BE MARKED ON BOTH SOLE $\hat{\Gamma}_c$ MASONRY $\hat{\Gamma}_c$ FOR ALIGNMENT IN THE FIELD. THE MARKING SHALL BE PERMANENT AND IN A LOCATION THAT WILL BE VISIBLE AFTER ERECTION OF THE STRUCTURE.

2. ALL BEARINGS SHALL HAVE MARKS PLACED ON THE VERTICAL EDGES OF 2. ALL BARNONS SHALL HAVE MARKS PLACED OF HE VERHOULE DOES OF THE SCE PLATE AND MASSINEY PLATES TO INDICATE THE CONTENTION OF BEARING. EACH EXPANSION BEARING SHALL HAVE MARKS PLACED ON THE SIDE OF THE GUIDE BARS OR SOLE PLATE TO NOLICATE THE CHITERINE OF MOVEMENT. THE TOP OF THE MASSINEY PLATE SHALL BE MARKED TO MIDICATE THE COLORION OF THE CENTERINE OF MOVEMENT EXTENDED. THIS MARK SHALL BE FULL LENGTH ON THE MASONRY PLATE PRIOR TO ASSEMBLY.

THESE MARKS CAN BE USED IN THE FIELD TO DETERMINE THE INITIAL OFFSET LOCATION OF THE SLIDE PLATE, IF APPLICABLE. THE MARKS SHALL BE MADE IN INDELIBLE INK AND SHALL BE VISIBLE AFTER BEARING INSTALLATION.

SAMPLING AND TESTING NOTES:

1. SAMPLING & TESTING SHALL BE IN ACCORDANCE WITH THE SPECIA PROVISIONS & AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATION, SECTION

2. THE MAXIMUM COEFFICIENT OF FRICTION SHALL BE 4% @ 68F

	600 UNICO	NGINETRING, INC. ORN PARK DRIVE N, MA 01801
APPR	OWED	APPROVED AS NOTED
☐ REJEC	CTED	REVISE AND RESUBNIT
SUBM	IT SPECIFIED I	TEM
MADE ON TH	E SHOP DRAI	FORMATION GIVEN IN THE CORRECTIONS OR COMMENTS WINGS DURING THIS REVIEW DI CTOR FROM COMPLIANCE WITH
APPROVAL C APPROVAL C COMPONENT. DIMENSIONS JOBSITE; INF FABRICATION TECHNIQUES. CONSTRUCTIO TRADES: AND	OF SPECIFIC IT F AN ASSEMB CONTRACTO TO BE CONFIS ORMATION THE PROCESSES O SEQUENCES /	E PLANS AND SPECIPICATION: UP SHALL NOT NOLLIDE EXT OF MHICH THE IEM IS AN R IS RESPONSIBLE FOR MICH AND CORNELATED AY TH MICH AND CORNELATED AY TH MICH AND CORNELATED AY TH DIFF TO THE MEANS, METHODS, NO PROCEDURES OF DION OF THE WORK OF ALL MINNS ALL WORK IN A SAFE R.
APPROVAL C APPROVAL C COMPONENT. DIMENSIONS JOBSITE; INF FABRICATION TECHNIQUES. CONSTRUCTIO TRADES: AND	SPECIFIC IT SE AN ASSEMB CONTRACTIO TO BE CONFIN ORMATION THE PROCESSES (SEQUENCES / IN COCRDINA OF FOR PERFOR CTORY MANNE	E PLANS AND SPECIFICATIONS OF SHALL NOT INCLUDE ELY OF SHALL HE PLAN IS AN R IS RESPONSIBLE FOR MED AND CORRELATED AT THA AT PERTAINS SOLELY TO THE OR TO THE MERANS, METHODS, NOT THE WORK OF ALL

PLEASE NOTE THE FOLLOWING. -SOLE PLATES DO NOT REQUIRE A BEVEL. -BEARINGS ARE TO BE GALVANIZED.



CONTRACTOR NOTES:

1. DUE TO DESIGN DIFFERENCES THE OVERALL HEIGHTS OF THE BEARING BEING SUPPLIED DIFFER FROM THE HEIGHTS SHOWN IN THE CONTRACT PLANS. THESE DIFFERENCES ARE GIVEN IN THE DATA TABLE ON THE BEARING DETAIL SHEETS. CONTRACTOR TO MODIFY PEDESTAL ELEVATIONS ACCORDINGLY

2. THE LOCATIONS OF THE ANCHOR RODS SHALL BE CROSS-REFERENCED WITH SHOP DRAWINGS TO VERIFY THAT LOCATIONS HAVE NOT CHANGED FROM ORIGINAL CONTRACT PLANS.

3. WELDING PROCEDURES SHALL BE ESTABLISHED BY THE CONTRACTOR TO RESTRICT THE MAXIMUM TEMPERATURE REACHED BY BONDED PIFE TO RESTRUCT HE MANDROMEN INSPECTATION RECORDED OF DISTRICT OF TOWNS AND THE STATE OF SUPERACES IN SOME THE STATE OF THE ST

4. THE EXPANSION BEARINGS WILL BE SHIPPED CENTERD AND IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OFFSET THE SLIDE PLATES IN THE FIELD DURING INSTALLATION, IF APPLICABLE. THE CONTRACT DRAWNINGS DID NOT INDICATE ANY INITIAL OFFSET VALUES, SO IF REQUIRED, THE OFFSETS WOULD BE PROVIDED TO THE CONTRACTOR BY THE BRIDGE DESIGNER

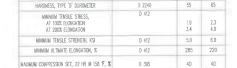
5. CONTRACTOR SHALL ENSURE NO DAMAGE OCCURS TO ANY BEARING COMPONENT DURING SURFACE PREPARATION FOR ANY SUBSEQUENT PROTECTIVE COATING OPERATIONS. THE DISC AND ALL EXPOSED STAINLESS STEEL SHALL BE PROPERLY MASKED, CARE SHALL BE EXERCISED BY THE CONTRACTOR TO PROTECT THE STAINLESS STEEL AND PTFE SLIDING SURFACES FROM DAMAGE AT ALL TIMES. THE PTFE SHALL BE PROTECTED FROM DIRECT EXPOSURE TO ULTRAVIOLET RAYS AT ALL

6. BEARINGS ARE TO BE SHIPPED AS COMPLETE UNITS, STEEL BANDED, AND SHALL BE WRAPPED TO PROTECT FROM MOISTURE AND DIRT DURING TRANSIT AND STORACE. BEARINGS SHALL BE STORED IN A CLEAN, DRY, LEYEL UPRIGHT POSITION WHILE AT JOBSITE. BEARINGS SHALL BE LIFTED FROM THEIR UNDERSIDES ONLY.

7. AT NO TIME MAY THE BEARINGS BE DISASSEMBLED WITHOUT AUTHORIZATION FROM D.S. BROWN OR WITHOUT THE PRESENCE OF A D.S.



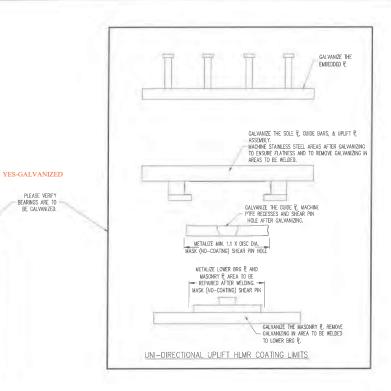




ASTM TEST METHOD REQUIREMENTS

PHYSICAL PROPERTY

5/18/20

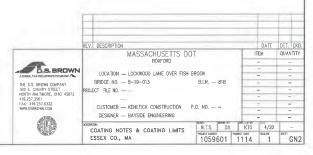


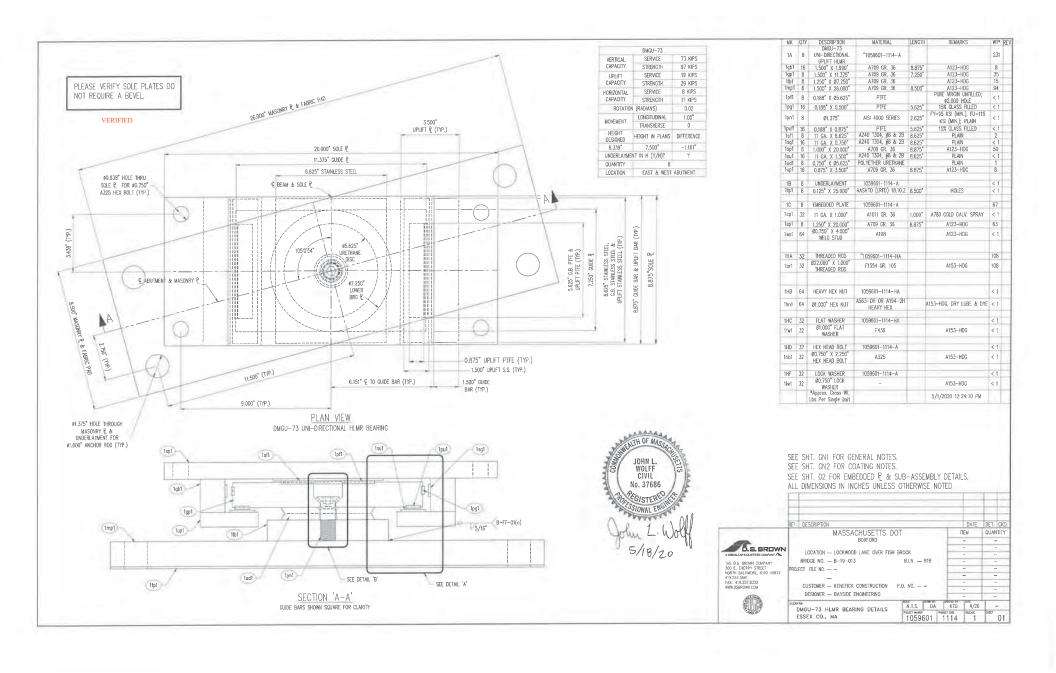
PROTECTIVE COATING NOTES:

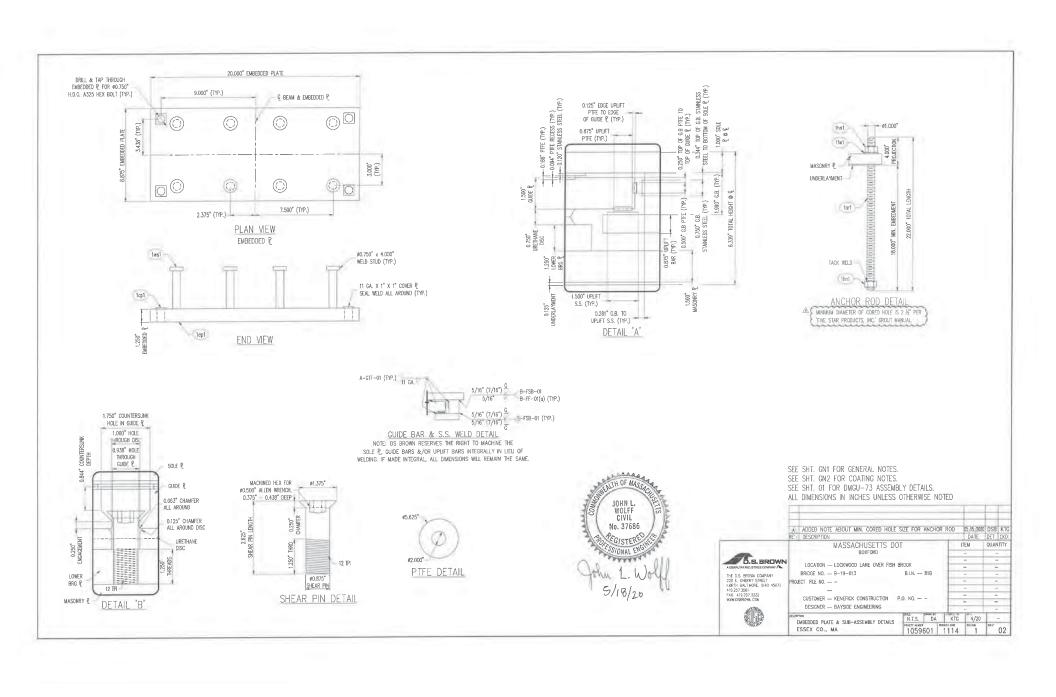
- 1. ALL EXTERNAL STEEL SPECIFIED AS GALVANIZED SHALL BE GALVANIZED IN
- 2. REPAIR ALL GALVANIZATION IN ACCORDANCE WITH ASTM A780. CALVANIZING REPAIR PAINTS CONTAINING ALUMINUM SHALL NOT BE USED ON SURFACES TO BE IN CONTACT WITH FRESH CONCRETE.
- 3. GALVANIZATION LIFTING DEVICES MAY BE WELDED TO PARTS IF NECESSARY. WHEN THEIR USE IS COMPLETE, REMOVE AND GRIND FLUSH ALL CONNECTION LOCATIONS, REPAIR GALVANIZE AREA PER ASTM A780.
- 4. ALL EXTERNAL STEEL SPECIFIED AS METALIZED SHALL BE BLAST CLEANED (SSPC-SP10) AND METALIZED IN ACCORDANCE WITH AWS C2.18 TO 6 MILS MINIMUM. METALIZING WIRE SHALL BE 99.9% PURE ZINC.

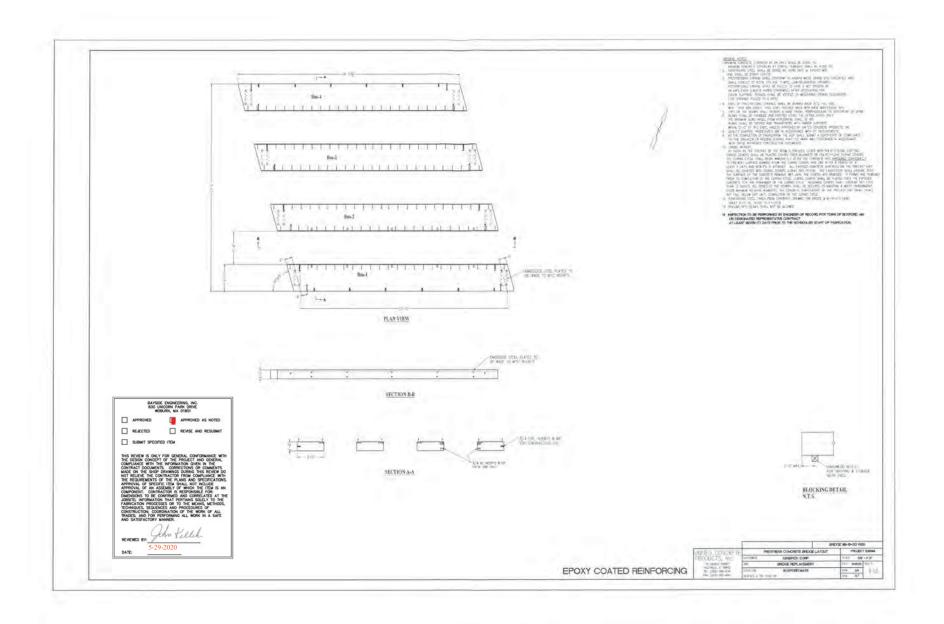


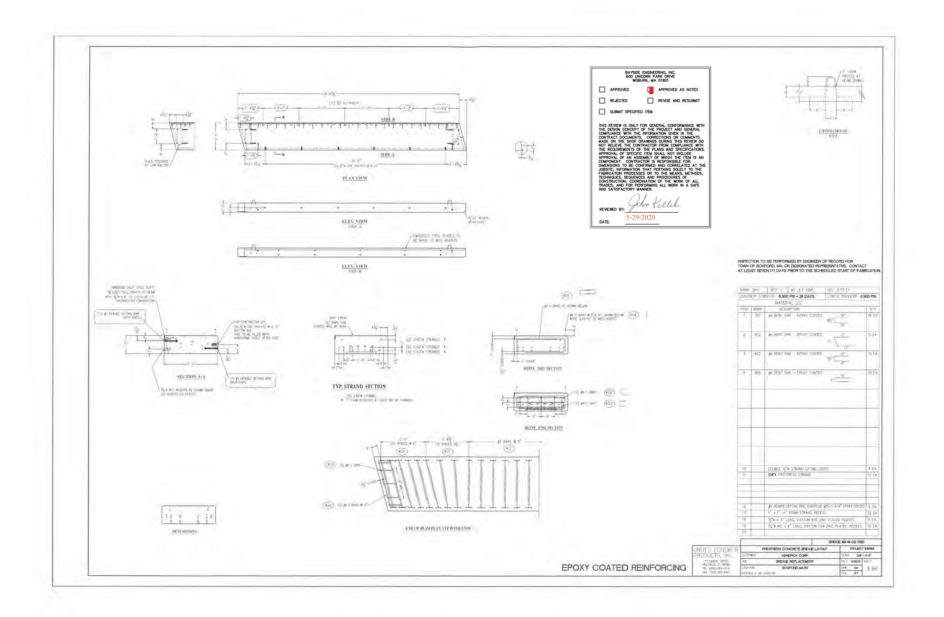
PLEASE VERIFY BEARINGS ARE TO BE GALVANIZED.

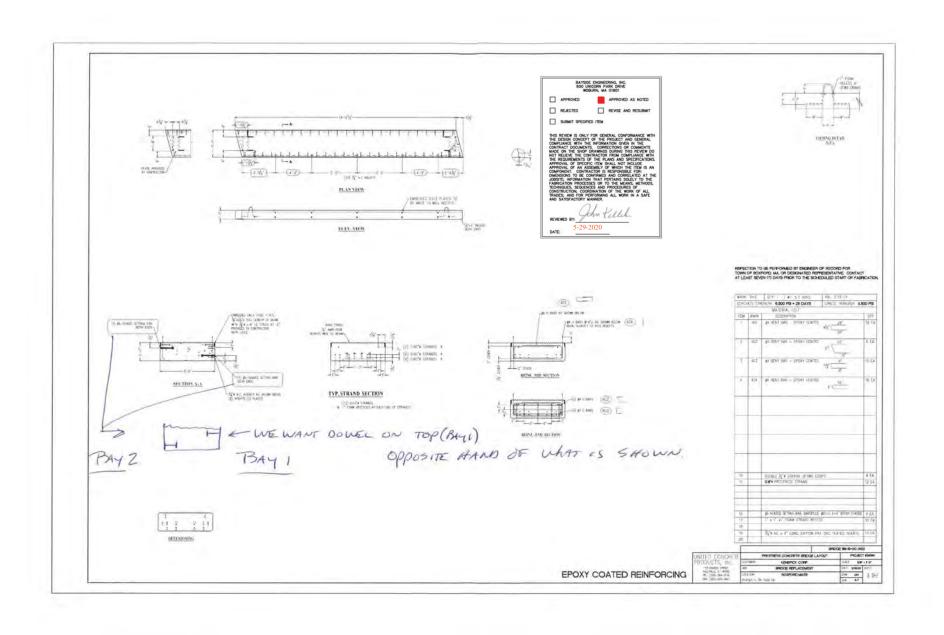


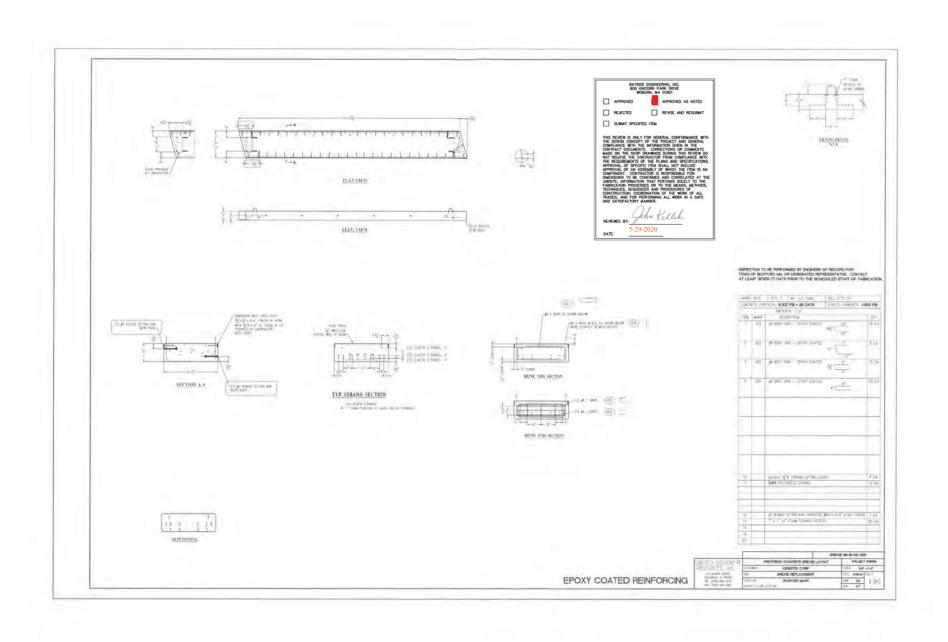


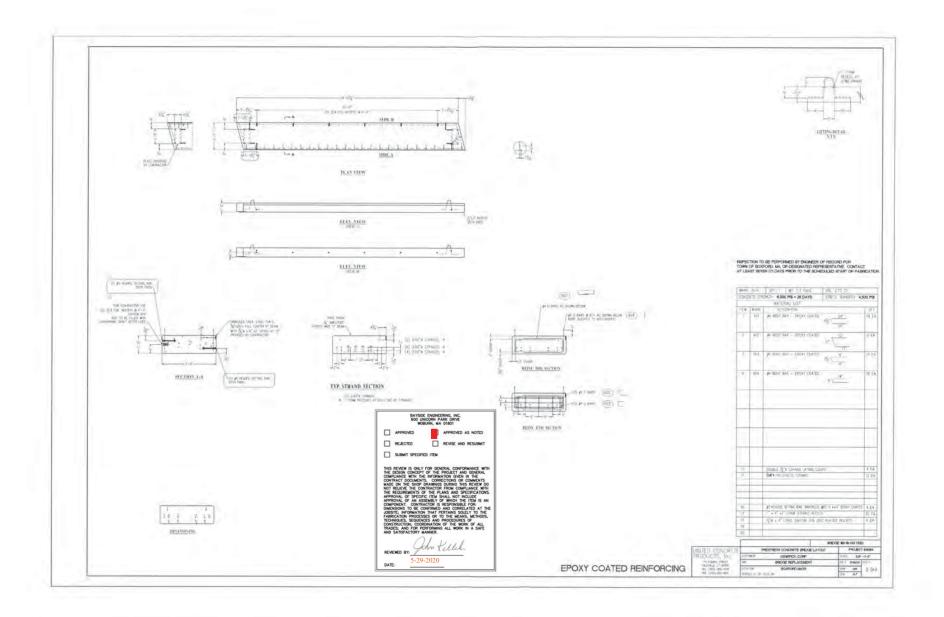


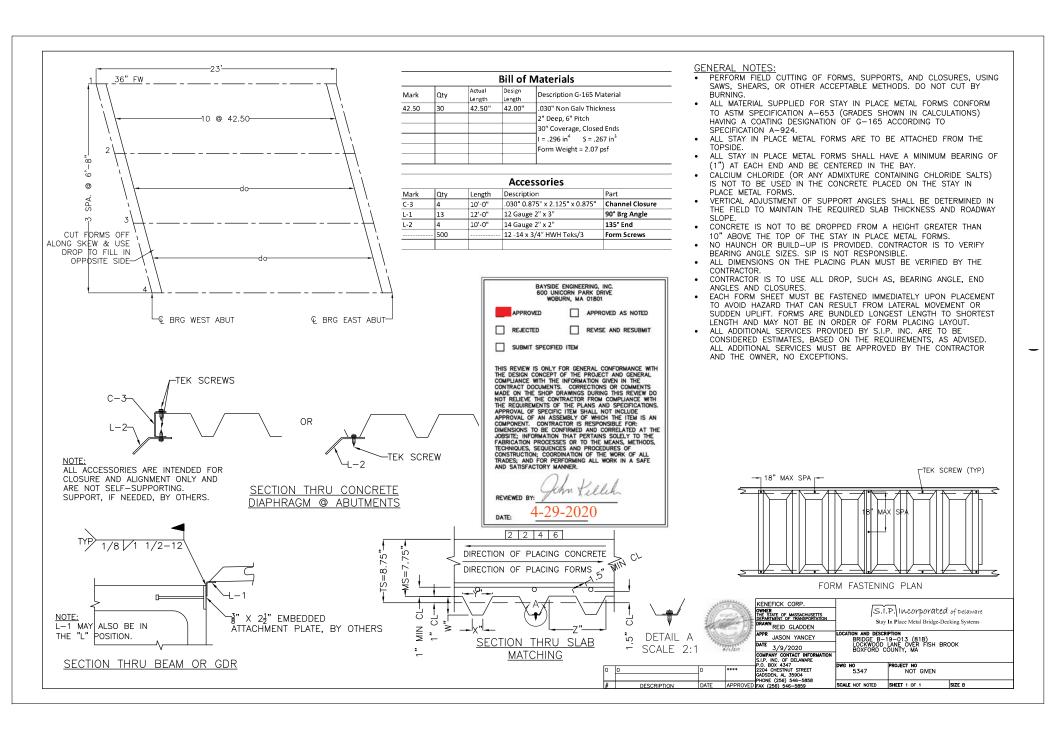












S.I.P. Incorporated of Delaware

Stay In Place Metal Bridge-Decking Systems Work Order # 5347

P.O. Box 4347 - 2204 Chestnut Street Gadsden, AL 35904 Phone: (256) 546-5858 Fax: (256) 546-5859

Calculation and Design Cover Sheet

4/7/20 1 Date Page

> **Customer Name Kenefick Corp**

> > **Project Not Given**

Bridge or Structure B-19-013 (81B)

Project Description Lockwood Lane Over Fish Brook

Boxford County, MA

PLEASE READ INFORMATION BELOW FOR TERMS OF DESIGN PACKAGE

All calculations and engineering drawings provided are for the intended party listed above. If you have received this package in error please inform the sender and delete any and all copies received.

All engineering calculations and drawings are to be approved prior to manufacturing.

S.I.P. of Delaware would like to thank you for your business and hopes you will re-use our services on your future projects.

If you have any questions please contact S.I.P. of Delaware. The engineering/ detailing department is open Monday through Thursday 7:30 AM to 4:30 PM and 7:30 AM to 1:30 PM on Fridays.

Very truly yours,

Jason Uancey

Direct (256) 312-8401

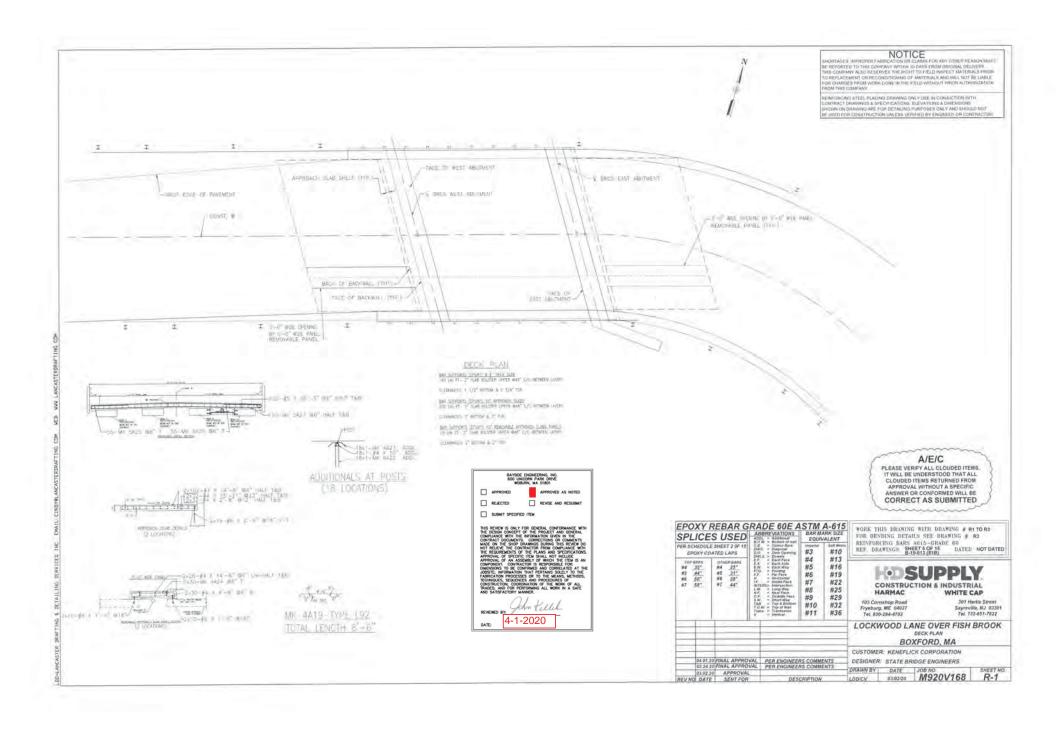


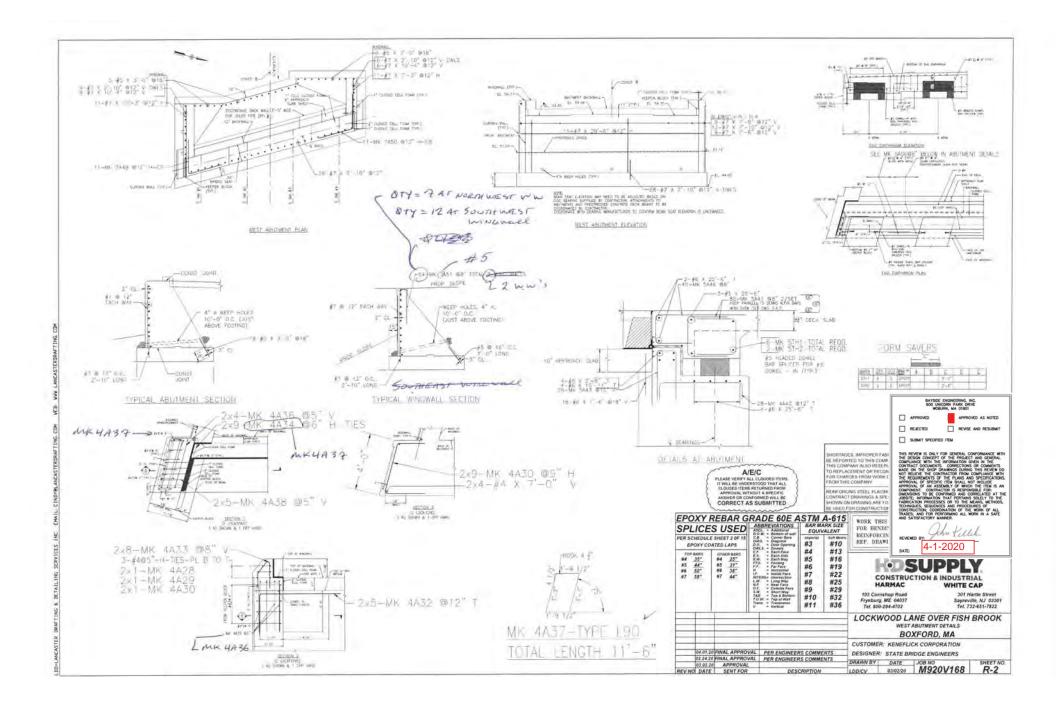
S.I.P. Incorporated of Delaware - 2204 Chestnut Street Gadsden, AL 35904

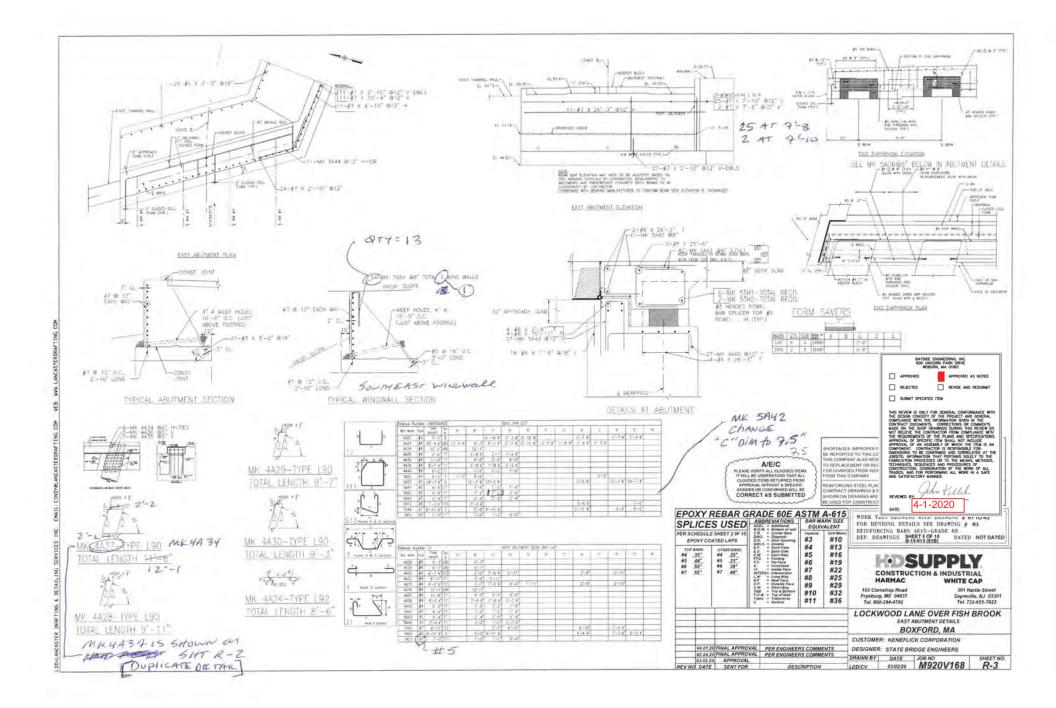
3.1.F. IIICOI	porated of Delawa	are - 220	4 Chestii	ut Street	Gausue	711, AL 33	304
Customer	Kenefick Corp		,	Sectional	Properties	3	
Work Order	5347	Profile	Galv. Th.	Design Th.	Inertia-in^4	Stress-in^3	Wt. psf
State	MA	2 x 6	.0330"	.0300"	0.296	0.267	2.07
Date	7-Apr-20			Coverage	30.00"		
Page	2		_		_/\/_	_	
Design Conditions	8		G	3165 Metal	Deck For	n	
Slab Thickness	8.19"						
With Parabolic Cro	wn						
Flute Extra Con.	1.000" Con. Wt.	150	pcf				
Additional Wt.	0 psf						
Deadload W1	116.945 psf	Minimum	Deadload	120	psf		
Construction Load	50 psf	Actual De	adload	116.945	psf		
W1 + CL = W2	166.945 psf						
Δ Allowed	.50" Max	L(12)/180	=.233"				
Center to Center	80.00"						
Flange Width (-)	36.00"						
Support Len. (-)	1.50"						
Metal Deck Form							
Form Length	` ,	gn Span L		3.5000'	Open Spa		3.6500'
Inertia Required	(5/384)(W1)L^4(172	8)/(E=29,0	00,000)(Δ /	Allowed)	0.0599	in^4	
Grade F	$f_s = 0.725$	$f_y =$	50 ksi	$f_a =$	36000	psi	
Stress Required	$W2(L^2)/8 = m$	255.635	lbf-ft	$m(12)/f_a$	0.0852	in^3	
Support Angle Ca	Iculations						
Support Angle	Gauge 12	Thickness	s of Metal	.1003''			
Grade D	$f_s = 0.725$	$f_v =$	40 ksi	f _a =	29000	psi	
Support Reaction	W2*Open Space/2	304.67	plf				
Open Space is Calo	culated as (((Center to	Center-Fl	ange Width	n))-Thickne	ss of Supp	oort Angle*2	2)/12
Bending Moment	274.112 in-lbf/ft	S =	0.02012	in^3/ft	S*f _a	583.49	in-lbf/ft
•	Therefore, 583.49	in-lbf/ft >	274.112	in-lbf/ft (A	ctual)		
Support Stress	13623.74 psi	13.6237		(-	,		
Weld Calculations							
Weld Conditions	Weld Size .1250"	Spacing	12"	Length	1.50"		
	Tw = 0.08838			Ultimate			
	$f_s = 2.65$	2251.06	lb/weld				
Distributed Load =	Support Reaction*We			304.67	lb/weld		

Please be advised that the section properties, listed above, have been calculated in accordance with the latest edition of the American Iron and Steel Institute's "Specification for the Design of Cold Formed Steel Structural Members".









 ATTACHMENT D: FULLER LANE OVER FISH BROOK (B-19-004) ROUTINE BRIDGE INSPECTION REPORT PERFORMED BY MASSDOT ON MAY 8, 2020 	7
ROUTINE BRIDGE INSPECTION REPORT PERFORMED BY MASSDOT ON MAY	7
ROUTINE BRIDGE INSPECTION REPORT PERFORMED BY MASSDOT ON MAY	7
ROUTINE BRIDGE INSPECTION REPORT PERFORMED BY MASSDOT ON MAY	

89A

STRUCTURES INSPECTION FIELD REPORT ROUTINE INSPECTION

BR. DEPT. NO. B-19-004=T-06-014

BASE																
Non-particle 1985 00000 0000 00000 00000 0000 00000 0000 0000 0000 0000 0000 0000 0000 00		8ST									90-ROUTINE INSP. DATE MAY 8, 2020					
WATER FISH BROOK					MEMORIAL NAME/LOCAL NAME								` ′			
STOP Prestressed Concrete Slab Development Tam Eader Agency A	06-FEATURES INTERSECTED				26-FUNCTIONAL	CLASS	3		DIST. B	⊥ RIDGI	E INSPECTI	ON ENGINEER	J. Did	eo		
Town Agency Text Town Agency Text	WATER FISH BRO	ОК			Urban Loc	al										
TEM 58		Conc	rete Slab		Town	Tow	/n	NER	TEAM L	LEADE	ER O. *Mou	stafa				
TIENTS		et Pa	nels		WEATHER	TEMP	P. (air)									
Dec Nearing surface 6			. 1	T K N N								(0	_			
1. Wearing surface 6	11 EN1 20	6		U	IVI 59			6				OV		7		
2. Dock Condition 6	DECK		DEF	SUP	ERSTRUCTUI	RE			DEF	F	SUBST	RUCTURE			l	DEF
S. Stay in Place Forms	1.Wearing surface	6	M-P	1.Stri	ngers			N	-		1. Abut	ments			7	
3.Floor System Bracing N -	2.Deck Condition	6	M-P	2.Flo	orbeams			N	-						-	- 9 D
S. Median	3.Stay in Place Forms	N	-	3.Flo	or System Braci	ing		N	-					_		- -
5.Median	4.Curbs	N	-	4.Gir	ders or Beams			6	M-F	P			_	-		
6. Sidewalks	5.Median	N	-	5.Tru	sses - General			N	<u> </u>					-	-	M-A
Description	6 Sidewalks	N	_	a.	Upper Chords		N		-		•				1	-
B.Railing 5		N	_	b.	Lower Chords		N		-		h. Footin	gs		_		-
9. Anti Missile Fence				c.	Web Members		N		-						-	
9. Anti Missile Fence	8.Railing		3-P	d.	Lateral Bracing		N		-		J	ment	+		-	-
1.1. Lighting Standards	9.Anti Missile Fence	N	-	e.	Sway Bracings		N		-			nem			1	-
1.Lighting Standards	10.Drainage System	N	-	f.	Portals		N		-				N	N	_	-
12.Utilities	11.Lighting Standards	N	-	a	End Posts		N		-		2. Piers	or Bents			N	
13.Deck Joints	12.Utilities	6	M-P					N	1 -			tals			-	-
14.	13.Deck Joints	Н	-	7.Coi	nn Plt's, Gussets	s & Ar	ngles	N	 		•	ns			1	-
15.	14.	N	_					N	<u> </u>		d. Stems	/Webs/Pierwalls		N		-
16.				9.Bea	arina Devices			Н	 						-	-
16.			-			s Fran	mes		 _			<u>g</u>	_			-
12. Welds	16.	N	-						1				N	N		-
CURB REVEAL (In millimeters) APPROACHES DEF a. Appr. pavement condition 6 M-P b. Appr. Roadway Settlement 7 - c. Appr. Sidewalk Settlement N - d. N - OVERHEAD SIGNS (Attached to bridge) DEF a. Condition of Welds N - b. Condition of Signs N - c. Condition N - c. Condition of Signs N - c. Condition N - c. Condition of Signs N - c. Condition N - c. Cond		N	s						-		i. Settler	nent	_		-	-
APPROACHES a. Appr. pavement condition b. Appr. Roadway Settlement c. Appr. Sidewalk Settlement d. N COLLISION DAMAGE: Please explain None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () Any Fracture Critical Member: (Y/N) Any Cracks: (Y/N) Any Cracks: (Y/N) N 3. Pile Bents a. Pile Caps N N N c. Diagonal Bracing N N N c. Diagonal		70	65			-4			-		j.				-	-
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b. Appr. Roadway Settlement 7 c. Appr. Sidewalk Settlement N c. COLLISION DAMAGE: Please explain None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X)	a. Appr. pavement condition	6	М-Р	15.				N		\sqcup		-	N		1	-
c. Appr. Sidewalk Settlement N d. N - COLLISION DAMAGE: Please explain None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () b. Condition of Welds N c. Condition of Signs N - Any Fracture Critical Member: (Y/N) N Any Cracks: (Y/N) N 1-60 (This Report): 7 93B-U/W (DIVE) Insp 00/00/00000	b. Appr. Roadway Settlement	7	_	Year	Painted		N								-	-
d. N - OVERHEAD SIGNS (Y/N) N None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain None (X) Minor () Moderate () Severe () LOAD VIBRATION: Please explain None (X) Minor () Moderate () Severe () None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe (,	_	_		<u>'</u>				1	⊣ I			_		-	-
OVERHEAD SIGNS (Attached to bridge) a. Condition of Welds b. Condition of Signs N COLLISION DAMAGE: None (X) Minor () Moderate () Severe () DEF Any Fracture Critical Member: (Y/N) Any Cracks: (Y/N) N UNDERMINING (Y/N) If YES please explain N COLLISION DAMAGE: None (X) Minor () Moderate () Severe () SCOUR: Please explain None (X) Minor () Moderate () Severe () I-60 (Dive Report): N I-60 (This Report): 7 93B-U/W (DIVE) Insp 00/00/00000	11		-				•		vere ()						
(Attached to bridge) DEF a. Condition of Welds b. Condition of Bolts c. Condition of Signs N Any Cracks: (Y/N) N Condition of Signs N Condition of Signs	d.	IN					•				UNDERM	INING (Y/N) If Y	ES ple	ase e	explair	N
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Any Cracks: (Y/N) N 93B-U/W (DIVE) Insp 00/00/0000									I-60 (Dive Report): N I-60 (This Report): 7							
X=UNKNOWN N=NOT APPLICABLE H=HIDDEN/INACCESSIBLE R=REMOVED	- Community of Origina	14	Any Cracks: (Y/N) N					93B-U/W (DIVE) Insp 00/00/0000								
	X=IINKNO)WN		N=	NOT APPLIC	ΔRL	F_b	=HII	DDEN	/INA	CCESS	IBI F		R=F	REM	OVED

CITY/	TOW	N .	—		B.I.I	N. BR. DEPT. NO. 8STRUCTURE NO.	INSPECTION DA	TE							
		RD=TOPSFIEL	LD		89										
	EM 61			,	6	ITEM 36 TRAFFIC SAFETY 36 COND DEF ACCESSIF									
	NNE			ı		A. Bridge Railing 0 5 S-P Lift Bucket	Needed U	Used N							
СНА	NNE	L PROTECTION	N			B. Transitions 0 6 M-P Ladder	N	N							
			Dive	Cur	DEF	C. Approach Guardrail 0 7 M-P Boat	Y	Y							
1.Ch	annel S	Scour	N	х	_	D. Approach Guardrail Ends 1 7 - Waders	Y	Y							
2.Em	ıbankr	ment Erosion	N	6	М-Р	WEIGHT POSTING Not Applicable Inspector 50		N							
3.Deb			N	N		H 3 3S2 Single Rigging	N	N							
	getatio		N	7	M-A	Actual Posting N N N N Staging	N	N							
		<u>n</u>			IVI-A	Recommended Posting N N N N Traffic Cont	trol N	N							
5.Util			N	N -	- -	RR Flagger	N	N							
		Slope Protection		7		Waived Date: 00/00/0000 EJDMT Date: 00/00/0000 Police	N	N							
	gradat		N	N	-	At bridge Other Advance Other: Signs In Place E W E W									
8.Fer	nder S	ystem	N	N	<u> </u>	(Y=Yes,N=No,	N	N							
			-			NR=NotRequired) Legibility/ Visibility TOTAL He	OURS 6	6							
			+	+		CLEARANCE POSTING N S PLANS	(Y/N):	Υ							
OTD!						Not X ft in ft in meter	` / _								
		LOW VELOCITY: h () Moderate () L	Low ()	X) Noi	ne ()	Posted Clearance 0 0 (V.C.R.)	, (1,1,1)								
					ort): 6	Signs In Place N S N S N S									
		. ,				(Y=Yes,N=No, NR=Not Required) Legibility/ List of field tea	ests performed:								
93b-c	J/W IN	ISP. DATE: 00)/00/	/0000	<u> </u>	Legibility/ Visibility									
RATI		N				If YES please give p	riority:								
Rating	Repo	ort (Y/N): N			ı	Recommend for Rating or Rerating (Y/N): N HIGH () MEDIUM () LOW ()									
Date:	<u> </u>	00/00/0000			I	REASON:									
	•	ion data at time of e		•	•	TEROON.									
I 58: -	· 159	9: - 160: - Da	ate : ()	ე0/00	0/0000										
						CONDITION RATING GUIDE (For Items 58, 59, 60	and 61)								
	CODE	CONDITION				DEFECTS	una 5.,								
	N	NOT APPLICABLE	+												
G	9	EXCELLENT	_	Exceller	nt condition.										
G	8	VERY GOOD	_		olem noted.										
G	7	GOOD	s	3ome m	ninor problen	ns									
F	6	SATISFACTORY	S	Structura	al elements	show some minor deterioration.									
F	5	FAIR	Α	All prima	ary structure	l elements are sound but may have minor section loss, cracking, spalling or scour.									
Р	4	POOR	Α	Advance	ed section Ir	oss, deterioration, spalling or scour.									
Р	3	SERIOUS				erioration, spalling or scour have seriously affected primary structural components. Local failures are po cks in concrete may be present.	ossible. Fatigue cracks								
		COLLICAT	A	Advance	ed deteriorat	tion of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present o									
С	2	CRITICAL	М			re support. Unless closely monitored it may be necessary to close the bridge until corrective action is tal or section loss present in critical structural components or obvious vertical or horizontal movement affec									
С	1	"IMMINENT" FAILURE				or section loss present in critical structural components or obvious vertical or nonzontal movement affect raffic but corrective action may put it back in light service.	ung structure stability.								
	0	FAILED	c	Out of se	ervice - bey	ond corrective action.									
		1				DEFICIENCY REPORTING GUIDE									
DEFI	CIENC	Y: A defect in a stru	ructure	that re	quires corre										
CATI	EGOR!	IES OF DEFICIENC	CIES	s:											
M= Minor Deficiency - Deficiencies which are minor in nature, generally do not impact the structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor pot holes, Minor corrosion of steel, Minor socuring, Clogged drainage, etc.															
S= Severe/Major Deficiency - Deficiencies which are more extensive in nature and more planning and effort to repair. Examples include but are not limited to: Moderate to major deterioration in concrete, Exposed and corroded rebars, Considerable settlement, Considerable scouring or undermining, Moderate to extensive corrosion to structural steel with measurable loss of section, etc.															
						settlement, Considerable scouring or undermining, Moderate to extensive corrosion to structural steel with measurable loss of uctural element of a bridge that poses an extreme unsafe condition due to the failure or imminent failure of the element which									
the bridge.															
С-Н=	Critica	cal Hazard Deficienc	cy -	include b	out are not lim	onent or element of a bridge that poses an extreme hazard or unsate condition to the public, but does not impair the structure ited to: Loose concrete hanging down over traffic or pedestrians, A hole in a sidewalk that may cause injuries to pedestrians, I	al integrity of the bridge. Example Missing section of bridge railing,	es.							
URGI	ENCY	OF REPAIR:													
	mediate		-		-	spection Engineer (DBIE) to report the Deficiency and to receive further instruction from him/her].									
A = AS	SAP-	[ACIIOII/Repail Siloulu	, be irina	ateu by L	JISTRICT Manner	nance Engineer or the Responsible Party (if not a State owned bridge) upon receipt of the Inspection Report].									

 CITY/TOWN
 B.I.N.
 BR. DEPT. NO.
 8.-STRUCTURE NO.
 INSPECTION DATE

 BOXFORD=TOPSFIELD
 89A
 B-19-004=T-06-014
 B19004-89A-MUN-BRI
 MAY 8, 2020

REMARKS

BRIDGE ORIENTATION

The structure is an adjacent precast box beams bridge. The approaches are East and West. The elevations are North and South. The Fish Brook flows from North to South. The superstructure is composed of 6 precast adjacent box beams that are numbered from South to North (**Sketches 1-3**). The west breastwall is concrete and the east breastwall is masonry.

ITEM 58 - DECK

<u>Item 58.1 - Wearing surface</u>

There are random longitudinal and transverse cracking, up to 1/8" wide and mostly at the westbound travel lane. These cracks continue into both approaches. At the east end of the bridge, there is a pothole, up to 2.5' long x 5" wide x 2" deep. At the west end of the bridge, there is a full width transverse cracks. (**Photo 1**).

Item 58.2 - Deck Condition

See Item 59.4

Item 58.4 - Curbs

There are no curbs on this structure. The curb reveals on page 1 were measured at the concrete rail base/top of facia beams.

Item 58.8 - Railing

See Item 36.A

Item 58.12 - Utilities

The North fascia of the bridge has a 8.5" diameter gas line located approximately 3" off the fascia. The one and only bracket near the east end is bent. (**Photo 2**).

APPROACHES

Approaches a - Appr. pavement condition

The east approach pavement has cracking that are continuing from the bridge at the center and the eastbound travel lane then, starting at the east approach guardrail end, a new wearing surface has been placed. (**Photo 1**).

On the west approach, there are up to 0.5" wide longitudinal cracks mostly on the east bound travel lane and typical full width transverse cracking. (**Photo 3**).

ITEM 59 - SUPERSTRUCTURE

Item 59.4 - Girders or Beams

- -The south fascia (south side of beam 1) has up to 0.25" wide horizontal and map cracking, delamination and spalling with heavy efflorescence. (**Photo 4**). At the top corner at the west end of the beam (beam 1), there is a spall up to 9" long x 1.5' deep x 1' high. (**Photo 5**).
- -Beam 1 has a longitudinal hairline crack at the center of the beam that extends from east end to the mid span. (**Photo 6**).
- -There is water staining at the joint between beam 1 and beam 2 near both east and west ends.
- -Beam 2 has hairline cracking at the west end. (Photo 7).

 CITY/TOWN
 B.I.N.
 BR. DEPT. NO.
 8.-STRUCTURE NO.
 INSPECTION DATE

 BOXFORD=TOPSFIELD
 89A
 B-19-004=T-06-014
 B19004-89A-MUN-BRI
 MAY 8, 2020

REMARKS

Item 59.4 - Girders or Beams (Cont'd)

- -There is water staining with efflorescence at the joint between beam 2 and beam 3 at the east end. (Photo 8).
- -Beam 5 has hairline cracking at the west end. (Photo 9).
- -At the north fascia (the north side of beam 6), there is a 1/4" wide horizontal crack near the top of the beam and typical vertical hairline cracking with minor efflorescence along the north face. (**Photo 10**).

ITEM 60 - SUBSTRUCTURE

Item 60.1 - Abutments

Item 60.1.b - Bridge Seats

The west bridge seat:

- -There is a vertical crack on the west bridge seat, 1/8" wide x full height with moderate efflorescence under the joint between beam 4 and beam 5 . (**Photo 11**).
- -There is a vertical crack on the west bridge seat, 1/8" wide x full height under beam 3. (Photo 12).

The east bridge seat:

- -There is a vertical crack on the east bridge seat, 1/8" wide x full height with moderate efflorescence under beam 1. (**Photo 13**).
- -There is a vertical cracking on the east bridge seat, up to 1/4" wide x full height with under beam 2. **(Photo 14).**
- -There are two vertical cracking on the east bridge seat, up to 1/4" wide x full height with under beam 3. (**Photo 15**).
- -There is a vertical cracking on the east bridge seat, up to 1/4" wide x full height with under beam 5. (**Photo 16.**

Item 60.1.d - Breastwalls

There is typical cracking in the mortar between the stones along the west breastwall. (Photo 17).

Item 60.1.e - Wingwalls

The southwest wingwall extension is undermined up to 1' long x 3' wide x 1' deep. (Photo 18).

The southeast masonry wingwall has cracking in mortar with voids and vegetation. (Photo 19).

<u>Item 60.1.h - Footings</u>

Footings are hidden due to water level.

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

<u>Item 61.2 - Embankment Erosion</u>

The Southwest embankment has moderate erosion. (Photo 18).

REMARKS

<u>Item 61.4 - Vegetation</u>

There is a large tree growing adjacent to the bridge at the base of the northeast embankment. (Photo 2).

TRAFFIC SAFETY

Item 36a - Bridge Railing

The last post from west south bridge railing is missing. (Photo 20).

Both the South and North bridge rail posts extend up to 4.5" above top of rail. (Photo 20).

Both bridge rail concrete bases have typical areas of cracking, delamination and random areas of spalled concrete. (Photo 21).

Item 36b - Transitions

The transition continue with the bridge railing with the same beam size and the spacing between the posts at all four transitions are excessive.

<u>Item 36c - Approach Guardrail</u>

All approach guardrails exhibit surface rust and the southwest guardrail has minor impact damage. (**Photo 22**).

Sketch / Photo Log

- Sketch 1: Plan View
- Sketch 2: Cross Section Looking West
- Sketch 3: Elevation Looking North
- Photo 1: Longitudinal and transverse cracks, mostly on westbound travel lane and a pothole at the east end. At the west end, there is a full width transverse cracking.
- Photo 2: The utility bracket at the North fascia beam near the east end is bent downwards. Also note the large tree growing adjacent to the bridge at the northeast embankment.
- Photo 3: Longitudinal cracks on east bound travel lane and typical full width transverse cracking on the west approach. (Looking east)
- Photo 4: The South fascia (south side of beam 1), up to 0.25" wide horizontal and map cracking, delamination and spalling with heavy efflorescence.
- Photo 5: Spall up to 9" long x 1.5' deep x 1' high at the top corner at the west end of the beam (beam 1).
- Photo 6: Longitudinal hairline crack at the center of the beam 1 that extends from east end to the mid span. (Looking east).
- Photo 7: Beam 2 has hairline cracking at the west end at the south corner. (Looking west).
- Photo 8: Water staining with efflorescence at the joint between beam 2 and beam 3 at the east end. (Looking east).
- Photo 9: Hairline cracking at the west end of beam 5. (Looking west).
- Photo 10: 1/4" wide horizontal crack near the top of the north fascia (the north side of beam 6) and typical vertical hairline cracking with minor efflorescence along the face.
- Photo 11: Vertical crack, up to 1/8" wide x full height with moderate efflorescence under the joint between beam 4 and beam 5 on the west bridge seat.
- Photo 12: Vertical crack on the west bridge seat, 1/8" wide x full height under beam 3.
- Photo 13: Vertical crack on the east bridge seat, 1/8" wide x full height with moderate efflorescence under beam 1.
- Photo 14: Vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 2.
- Photo 15: Two vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 3.
- Photo 16: Vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 5.
- Photo 17: Typical cracking in the mortar between the stones along the west breastwall.

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 CITY/TOWN
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REMARKS

Sketch / Photo Log (Cont'd)

Photo 18: Southwest wingwall extension is undermined up to 1' long x 3' wide x 1' deep. Also note the

moderate erosion at the embankment.

Photo 19: Southeast masonry wingwall has cracking in mortar with voids and vegetation.

Photo 20: The last post from west south bridge railing is missing.

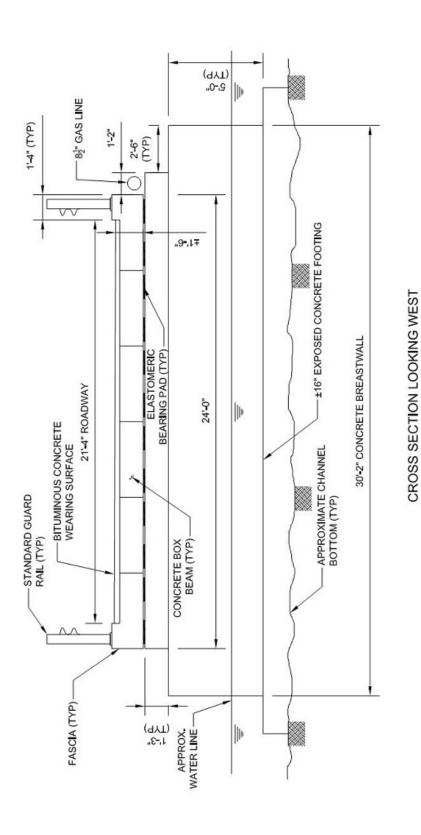
Photo 21: Both bridge railing concrete bases have typical areas of cracking, delamination and random

areas of spalled concrete. (South bridge railing shown).

Photo 22: Minor impact damage and surface rust at the southwest approach guardrail.

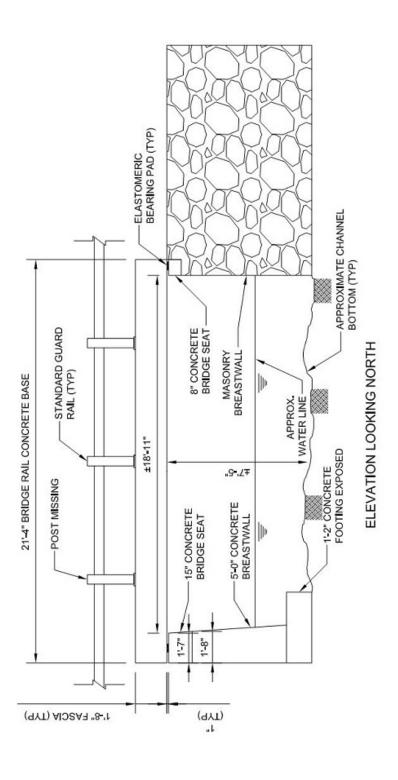
CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE **BOXFORD=TOPSFIELD** 89A B-19-004=T-06-014 B19004-89A-MUN-BRI **MAY 8, 2020 SKETCHES** FRONT FACE OF
BRIDGE SEAT
& BREASTWALL (BELOW) STANDARD GUARD RAIL (TYP) S BREASTWALL (TYP) 8½" GAS PIPE 54.-0. YAWQAOR "1-11S В BOX BEAM 6 **BOX BEAM 5 BOX BEAM 4 BOX BEAM 3 BOX BEAM 2 BOX BEAM 1** 18'-11" PLAN VIEW 18'-9" DIRECTION OF FLOW (PYT) .0-.1 RAIL CONCRETE BASE (TYP) 1'-7" TOP OF BREASTWALL 1'-6" TOP OF BRIDGE SEAT OF BRIDGE SEAT 1'-8" TOP OF BREASTWALL 1.4" BRIDGE FRONT FACE OF BRIDGE SEAT (BELOW) 1'-7" TOP FRONT FACE OF ABUTMENT (BELOW) 6 PRECAST CONCRETE BOX BEAMS -Sketch 1: **Plan View**

SKETCHES



Sketch 2: Cross Section Looking West

SKETCHES



Sketch 3: Elevation Looking North

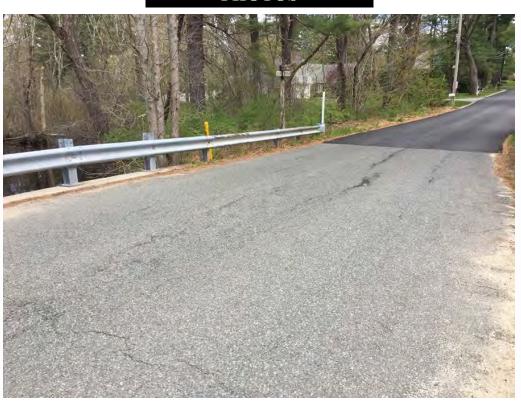


Photo 1: Longitudinal and transverse cracks, mostly on westbound travel lane and a pothole at the east end. At the west end, there is a full width transverse cracking.



Photo 2: The utility bracket at the North fascia beam near the east end is bent downwards. Also note the large tree growing adjacent to the bridge at the northeast embankment.



Photo 3: Longitudinal cracks on east bound travel lane and and typical full width transverse cracking on the west approach. (Looking east)



Photo 4: The South fascia (south side of beam 1), up to 0.25" wide horizontal and map cracking, delamination and spalling with heavy efflorescence.



Photo 5: Spall up to 9" long x 1.5' deep x 1' high at the top corner at the west end of the beam (beam 1).



Photo 6: Longitudinal hairline crack at the center of the beam 1 that extends from east end to the mid span. (Looking east).

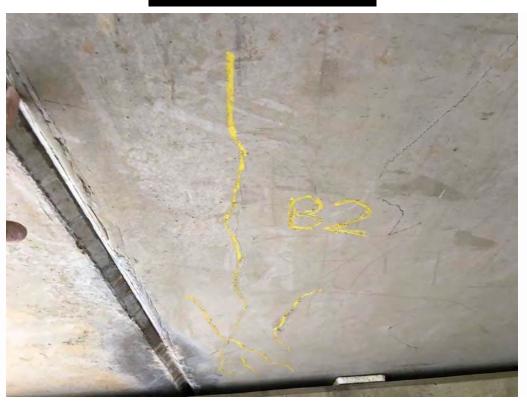


Photo 7: Beam 2 has hairline cracking at the west end at the south corner. (Looking west).



Photo 8: Water staining with efflorescence at the joint between beam 2 and beam 3 at the east end. (Looking east).



Photo 9: Hairline cracking at the west end of beam 5. (Looking west).



Photo 10: 1/4" wide horizontal crack near the top of the north fascia (the north side of beam 6) and typical vertical hairline cracking with minor efflorescence along the face.



Photo 11: Vertical crack, up to 1/8" wide x full height with moderate efflorescence under the joint between beam 4 and beam 5 on the west bridge seat.



Photo 12: Vertical crack on the west bridge seat, 1/8" wide x full height under beam 3.



Photo 13: Vertical crack on the east bridge seat, 1/8" wide x full height with moderate efflorescence under beam 1.



Photo 14: Vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 2.



Photo 15: Two vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 3.



Photo 16: Vertical crack on the east bridge seat, up to 1/4" wide x full height with under beam 5.



Photo 17: Typical cracking in the mortar between the stones along the west breastwall.



Photo 18: Southwest wingwall extension is undermined up to 1' long x 3' wide x 1' deep. Also note the moderate erosion at the embankment.



Photo 19: Southeast masonry wingwall has cracking in mortar with voids and vegetation.



Photo 20: The last post from west south bridge railing is missing.



Photo 21: Both bridge railing concrete bases have typical areas of cracking, delamination and random areas of spalled concrete. (South bridge railing shown).



Photo 22: Minor impact damage and surface rust at the southwest approach guardrail.

	ber 20, 2020 State Information	١		ClassificationC
BDEPT#= B19004=T06014		Agency Br.No.		(112) NBIS Bridge Length
Town= Boxford=Topsfield		L	O.	(104) Highway System
B.I.N= 89A		AASH	ITO= 019.3	3 (26) Functional Class - Urban Local
RANK= 0 H.I.=	NA	FHWA Select List=	N (6/21/2017	7) (100) Defense Highway
(O) O() N 1	Identification	B1900/	189AMUNBR	(101) Parallel Structure
(8) Structure Number		B19004	100000000	(400) D: (; (T f)
(5) Inventory Route	Notriot		04	
(2) State Highway Department I (3) County Code 009	(4) Place code		07420	
(6) Features Intersected	()	WATER F	ISH BROOK	, ,
(7) Facility Carried			FULLER LN	, ,
(9) Location				(21) Maintain - Town Agency
(11) Kilometerpoint			0000.370	
(12) Base Highway Network				N (37) Historical Significance undetermined
(13) LRS Inventory Route & Sub	route	00000000000	• •	Condition
(16) Latitude	Touto	42 DEG 38 MIN	05.15 SEC	C (58) Deck
(17) Longitude		70 DEG 58 MIN	29.24 SEC	(EQ) Comparative to the
(98) Border Bridge State Code		Sha		(60) Substructure
(99) Border Bridge Structure No	#	Olla	.5 /0	(61) Channel & Channel Protection
• •	· # ·ucture Type and Ma	aterial		(62) Culverts
(43) Structure Type Main:	Prestressed Conc		501	Load Rating and PostingC
Slab			applicable	(31) Design Load - Unknown
(44) Structure Type Appr:	Joinness	s bridge type. 140t a	ippiioanie	(63) Operating Rating Method - Allowable Stress (AS)
Other		Code	000	(64) Operating Rating
(45) Number of spans in main u	nit	Code	001	(00) inventory realing wethout - Allowable offess (AO)
			0000	(or) inventery realing
(46) Number of approach spans	Concrete Preca	est Danala	Code 2	(,)
(107) Deck Structure Type -(108) Wearing Surface / Protect		.st Fallels	Code 2	Appraisal
-			Code 6	(67) Structural Evaluation
A) Type of wearing surface	Unknown		Code 8	(68) Dock Goometry
B) Type of membrane -				(69) Underclearances yert and horiz
C) Type of deck protection -			Code 8	(71) Waterway adequacy
(07) \/ D :!!	Age and Service	,	4005	(72) Approach Roadway Alignment
(27) Year Built			1985	(36) Framic Safety Features 0 0 0
(106) Year Reconstructed	Highway		0000	(113) Scour Critical Bridges
(42) Type of Service: On -	Highway		Codo 15	(90) Inspection Date 05/08/20 (91) Frequency 24
Under - Waterway	00		Code 15	- (02) Critical Facture Inspection: (02) CELI
(28) Lanes: On Structure	02	Under structure		(4) 5 4 6 37 4 5 4 7
(29) Average Daily Traffic	0040 (400) T		001200	
(30) Year of ADT	2018 (109) Truck	ADI	05 %	(C) Other Cresial Inspection
(19) Bypass, detour length	■ Geometric Data		004 KM	
(48) Length of maximum span	_ Geometric Data		0005.8 M	 (*) Other Inspection () N 00 MO *) M (*) Closed Bridge N 00 MO *) M 00 MO *)
(49) Structure Length			0005.6 M	() Comment of the com
(50) Curb or sidewalk:	Left 00.	.0 M Righ		
(51) Bridge Roadway Width Cur		o w ragi	006.5 M	Pating Loads
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	(w/shouldors)			Operating 0.0 0.0 0.0 0.0
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(54) Min Vert Underclear ref (55) Min Lat Underclear RT ref (56) Min Lat Underclear LT	■ Navigation Data			Freeze/Thaw N : Not Applicable
(54) Min Vert Underclear ref (55) Min Lat Underclear RT ref (56) Min Lat Underclear LT (38) Navigation Control - No			Code 0	Freeze/Thaw N : Not Applicable Accessibility (Needed/Used)
(54) Min Vert Underclear ref (55) Min Lat Underclear RT ref (56) Min Lat Underclear LT (38) Navigation Control - No (111) Pier Protection	Navigation Data navigation control of			Freeze/Thaw N : Not Applicable Accessibility (Needed/Used) N / N Liftbucket N / N Rigging N / N Other
(54) Min Vert Underclear ref (55) Min Lat Underclear RT ref (56) Min Lat Underclear LT (38) Navigation Control - No (111) Pier Protection (39) Navigation Vertical Clearance	 Navigation Data navigation control of 		Code 0	Freeze/Thaw N : Not Applicable Accessibility (Needed/Used) N / N Liftbucket N / N Rigging N / N Other N / N Ladder N / N Staging
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ATTACHMENT E: ENDICOTT ROAD OVER FISH BROOK (B-19-005) • CULVERT INSPECTION REPORT PERFORMED BY MASSDOT ON MAY 7, 2020

WASSACHUSETTS DEPARTMENT OF TRANSPORTATIONPAGE 1 OF 15

89B 04

STRUCTURES INSPECTION FIELD REPORT **CULVERT INSPECTION**

BR. DEPT. NO. B-19-005

MEMORIAL NAME 27-YR BUILT 108-YR REBUILT 1900 0000	CITY/TOWN 8ST						TRUCTURE NO. B19005-89B-MUN-BRI									POINT 0.000		TATUS		90-ROUTINE INSP. DA					
Urban Minor Artorial				ГТ Б	RD					MEI	MEMORIAL NAME/LOCAL NAME						2			106-		I		-	N 106)
Town Agency																DIS	T. BRID	OGE	INSPECTI	ION E	NGINEI	ER J. D	ideo		
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SIAPE: RECTANGULAR		•									uiiiy		Τ.												
DEPTH OF COVER CONCRETE AND MASONRY DEPTH OF COVER CURB REVEAL (In millimeters) 0.3						LAF	₹						"		:				<u> </u>	NU	MBER				
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CITY/TOWN	B.I.N.	BR. DEPT. NO.	8STRUCTURE NO.	INSPECTION DATE
BOXFORD	89B	B-19-005	B19005-89B-MUN-BRI	MAY 7, 2020

REMARKS, PHOTOS & SKETCHES

BRIDGE ORIENTATION

The structure is a double barrel rectangular culvert composed of concrete and masonry walls with concrete and granite slabs. The approaches are North and South. The elevations are East and West. The spans were numbered from South to North. Fish Brook flows from West to East. (Sketches 1,2 and 3).

ITEM 62 - CULVERT

Item 62.1 - Roof

The roof for both the East and West entrance undersides consists of concrete slabs and then there is approximately 19'-7" of granite slabs making up the remainder of the roof. (**Sketch 1**).

The West fascia has widespread areas of large spalls with exposed reinforcing that have extended down the face from the bridge rail concrete base and the headwall. (**Photo 1**).

Span 1 near the west fascia, the underside of the concrete deck has a spall 30" long x 12" wide x 1.5" deep with exposed and rusted rebar that has 100% section loss. Also, there is an area of scaling and honeycombing up to full width x 2.5' long x 1" deep. (**Photo 2**).

Both span 1 and span 2 near the east end, the joint between the concrete slab and the first granite slab has active leakage with efflorescence. (Photos 3 and 4).

Span 2 at the edge of the west fascia, there is a spall up to 7" diameter (**Photo 5**) and the underside of the concrete deck has full length x full width area of honeycombing/scaling with moderate efflorescence (**Photo**

CONDITION RATING GUIDE

	CODE	CONDITION	DEFECTS
	N	NOT APPLICABLE	Use if structure is not a culvert.
G	9	EXCELLENT	No deficiencies.
G	8	VERY GOOD	No noticeable or noteworthy differences which affect the condition of the culvert. Insignificant scrape marks caused by drift.
G	7	GOOD	Shrinkage cracks, light scaling, and insignificant spalling, which does not expose reinforcing steel. Insignificant damage caused by drift with not misalignment and not requiring corrective action. Some minor scouring has occurred near curtain walls, wingwalls, or pipes. Metal culverts have a smooth symmetrical curvature with superficial corrosion and no pitting.
F	6	SATISFACTORY	Deterioration or initial disintegration, minor chloride contamination, cracking with some leaching, or spalls on concrete or masonry walls and slabs. Local minor scouring at curtain walls, wingwalls, or pipes. Metal culverts have a smooth curvature, non-symmetrical shape, significant corrosion or moderate pitting.
F	5	FAIR	Moderate to major deterioration, or disintegration, extensive cracking and leaching, or spalls on concrete or masonry walls and slabs. Minor settlement or misalignment. Noticeable scouring or erosion at curtain walls, wingwalls, or pipes. Metal culverts have significant distortion and deflection in one section, significant corrosion or deep pitting.
Р	4	POOR	Large spalls, heavy scaling, wide cracks, considerable efforescence, or opened construction joints permitting loss of backfill. Considerable settlement or misalignment. Considerable scouring or erosion at curtain walls, wingwalls, or pipes. Metal culverts have significant distortion and deflection throughout, extensive corrosion or deep pitting.
Р	3	SERIOUS	Any condition described in Code 4 but which is excessive in scope. Severe movement or differential settlement of the segments, or loss of fill. Holes may exist in walls or slabs. Integral wingwalls, nearly severed from culvert. Severe scour or erosion at curtain walls, wingwalls, or pipes. Metal culverts have extreme distortion and deflection in one section, extensive corrosion, or deep pitting with scattered perforations.
С	2	CRITICAL	Advanced deterioration of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless closely monitored it may be necessary to close the bridge until corrective action is taken.
С	1	"IMMINENT" FAILURE	Bridge closed. Corrective action may put back in light service.
	0	FAILED	Bridge closed. Replacement necessary.

DEFICIENCY REPORTING GUIDE

DEFICIENCY: A defect in a structure that requires corrective action.

CATEGORIES OF DEFICIENCIES:

M= Minor Deficiency - (Examples include but are not limited to: Spalled concrete, minor to moderate corrosion to steel culverts, minor settlement or misalignment, minor scouring, minor damage to guardrail, etc.)

S= Severe/Major Deficiency - (Examples include but are not limited to: Large spalls, wide cracks, moderate to major deterioration in concrete, considerable settlement, considerable scouring or undermining, extensive corrosion and deflection in steel culverts, etc.)

C-S= Critical Deficiency - A deficiency in a structural component or element of a bridge that poses an extreme hazard or unsafe condition to the public. (Follow-up Critical Deficiency Report must be submitted

separate

URGENCY OF REPAIR:

I = Immediate[Inspector(s) stay at the bridge until the District Maintenance crew or the responsible Agency crew(if not a State bridge) show up and corrective action is taken.]

A = ASAP- [Action will be taken by the District Maintenance Engineer or the Responsible Agency (if not a State owned bridge) upon receipt of the Inspection Report].

P = Prioritize- [Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs made when funds and/or manpower is available].

REMARKS

Item 62.1 - Roof (Cont'd)

6).

Item 62.3 - Walls

There is typical cracking with efflorescence in the mortar between stones at all granite walls. (Photo 7).

Both north and south concrete walls typically have minor abrasion along the water line and up to 2" high and efflorescence at the east end. (**Photo 8**).

Item 62.4 - Headwall

The West fascia headwall has large areas of heavily spalled concrete with exposed reinforcing steel that extend down from the bridge rail concrete base. (**Photo 1**).

Item 62.5 - Wingwall

All wingwalls typically have minor abrasion along the water line and up to 2" high.

Item 62.9 - Wearing Surface

There is random transverse hairline cracking mainly in the southbound travel lane.

Item 62.10 - Railing

See Item 36.A

Item 62.11 - Sidewalks

There is a 3'-11" sidewalk on the west side and there is unpaved shoulder on the east side.

The west sidewalk and the curb are continuous pour asphalt surface. The curb has sections that have deteriorated which is propagating into the sidewalk. (**Photo 9**).

Item 62.12 - Utilities

There is a 5" diameter gas pipe located along the west fascia and a 20" diameter utility pipe located along the east fascia.

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

<u>Item 61.2 - Embankment Erosion</u>

There is minor to moderate erosion behind the northeast and southeast wingwalls. (Photos 10 and 11).

TRAFFIC SAFETY

<u>Item 36a - Bridge Railing</u>

West bridge rail:

All three concrete posts have heavy spalls with exposed rebars up to full height. (Photo 12).

The top rail has split and section loss and is not attached at the center concrete post and the post has heavy spalls with exposed rebars for the full height. (**Photo 12**).

The bridge rail concrete base has large spalling with one fully exposed longitudinal rebar along the full length of the base. (**Photo 1**).

All timber bridge railings have splits and checks up to full length.

REMARKS

<u>Item 36b - Transitions</u>

The approach guardrails are not attached to the bridge railing on all four corners of the bridge.

Item 36c - Approach Guardrail

The approach guardrail at the Northeast, Northwest and Southwest approaches is a two-wire cable rail on concrete posts. Cable wire is mostly missing or ineffective. Concrete posts are heavily deteriorated and ineffective. (Photos 13, 14 and 15).

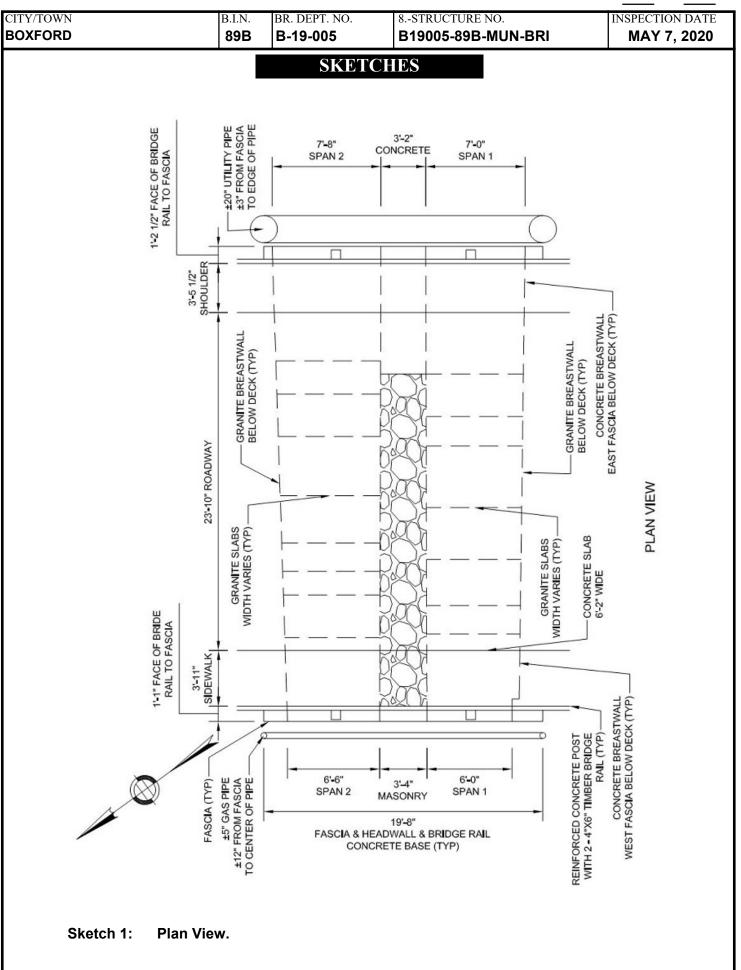
The southeast approach guardrail is a steel rail on steel posts that is too low (resting on the ground) and has major impact damage and is ineffective. It also exhibits 100% section loss at the posts connections. (Photo 16).

<u>Item 36d - Approach Guardrail Ends</u>

All guardrail ends are either missing or completely ineffective.

Sketch / Photo Log

- Sketch 1: Plan View.
- Sketch 2: Cross section at span 2, looking North.
- Sketch 3: Elevation, looking east.
- Photo 1: West fascia, areas of heavy and large spalls with exposed and rusted rebars.
- Photo 2: Span 1, near the west fascia, area of scaling, honeycombing and a spall at the concrete deck with exposed and rusted rebar.
- Photo 3: Active leakage with heavy efflorescence at the joint between the concrete slab and the first granite slab in span 2, near the east end.
- Photo 4: Active leakage with heavy efflorescence at the joint between the concrete slab and the first granite slab in span 1, near the east end.
- Photo 5: Span 2, spall up to 7" diameter at the edge of the west fascia.
- Photo 6: Span 2, west end, the underside of the concrete deck has full length x full width area of honeycombing/scaling with moderate efflorescence.
- Photo 7: Typical cracking with efflorescence in the mortar between stones at all granite walls.
- Photo 8: Typical abrasion along the water line and up to 2' high at the concrete walls. Also note the efflorescence at the east end of both north and south concrete walls. (Northeast wall shown).
- Photo 9: West sidewalk, deterioration at the curb which is propagating into the sidewalk.
- Photo 10: Minor erosion behind the southeast wingwall.
- Photo 11: Minor erosion behind the northeast wingwall.
- Photo 12: West bridge rail, the top rail has split and is not attached to concrete post. Also note the concrete post has heavy spalls with exposed rebars for the full height.
- Photo 13: The southwest approach guardrail cables are missing, and the concrete posts heavily deteriorated and ineffective.
- Photo 14: The northwest approach guardrail lower cable is missing, the upper cable is loose and ineffective and the concrete posts heavily deteriorated.
- Photo 15: The northeast approach guardrail cables are missing, and the concrete posts heavily deteriorated and ineffective.
- Photo 16: The southeast approach steel guardrail has 100% section loss, major impact damage and is ineffective.



CITY/TOWN BR. DEPT. NO. B.I.N. 8.-STRUCTURE NO. INSPECTION DATE **BOXFORD** 89B B-19-005 B19005-89B-MUN-BRI **MAY 7, 2020 SKETCHES** GAS PIPE 1'-2 1/2" *Z-.Z# 3'-5 1/2" 8'-0" CONCRETE BREASTWALL -APPROX. WATER LINE CONCRETE SLAB DEPTH UKNOWN #01± CROSS SECTION @ SPAN 2 LOOKING NORTH WIDTH VARIES, ±1½" GAPS GRANITE SLABS (TYP) ±19'-7" MASONRY BREASTWALL 23'-10" ROADWAY REINFORCED CONCRETE POSTS WITH 2 - 4"X6" TIMBER BRIDGE RAILS (TYP) CONCRETE SLAB DEPTH UKNOWN -±5" ASPHALT CURB REVEAL 5'-11" CONCRETE BREASTWALL 3'-11" #5.-7" 7 ..O-.ZŦ ±20" UTILITY PIPE-Sketch 2: Cross section at span 2, looking North.

CITY/TOWN B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. INSPECTION DATE **BOXFORD MAY 7, 2020** 89B B19005-89B-MUN-BRI B-19-005 **SKETCHES** CONCRETE BREASTWALL WIDTH UKNOWN - APPROX. WATER LINE "S-'S± -MASONRY CENTER PIER 6'-0" SPAN 1 **ELEVATION LOOKING EAST** REINFORCED CONCRETE POSTS WITH 2 - 4"X6" TIMBER BRIDGE RAILS (TYP) 3"-4" MASONRY ±19'-8" 15'-10" 6'-6" SPAN 2 .0-.ZŦ CONCRETE BREASTWALL WIDTH UKNOWN ±5" GAS PIPE CONCRETE FASCIA & HEADWALL & BRIDGE RAIL BASE — Sketch 3: Elevation, looking east.



Photo 1: West fascia, areas of heavy and large spalls with exposed and rusted rebars.



Photo 2: Span 1, near the west fascia, area of scaling, honeycombing and a spall at the concrete deck with exposed and rusted rebar.



Photo 3: Active leakage with heavy efflorescence at the joint between the concrete slab and the first granite slab in span 2, near the east end.



Photo 4: Active leakage with heavy efflorescence at the joint between the concrete slab and the first granite slab in span 1, near the east end.



Photo 5: Span 2, spall up to 7" diameter at the edge of the west fascia.



Photo 6: Span 2, west end, the underside of the concrete deck has full length x full width area of honeycombing/scaling with moderate efflorescence.



Photo 7: Typical cracking with efflorescence in the mortar between stones at all granite walls.



Photo 8: Typical abrasion along the water line and up to 2' high at the concrete walls. Also note the efflorescence at the east end of both north and south concrete walls. (Northeast wall shown).

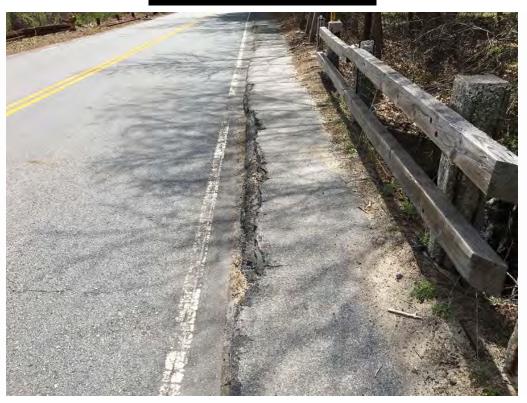


Photo 9: West sidewalk, deterioration at the curb which is propagating into the sidewalk.



Photo 10: Minor erosion behind the southeast wingwall.



Photo 11: Minor erosion behind the northeast wingwall.



Photo 12: West bridge rail, the top rail has split and is not attached to concrete post. Also note the concrete post has heavy spalls with exposed rebars for the full height.



Photo 13: The southwest approach guardrail cables are missing, and the concrete posts heavily deteriorated and ineffective.



Photo 14: The northwest approach guardrail lower cable is missing, the upper cable is loose and ineffective and the concrete posts heavily deteriorated.



Photo 15: The northeast approach guardrail cables are missing, and the concrete posts heavily deteriorated and ineffective.



Photo 16: The southeast approach steel guardrail has 100% section loss, major impact damage and is ineffective.

	State Information			Classification	Code
BDEPT#= B19005	Agen	cy Br.No.		(112) NBIS Bridge Length	١
Town= Boxford		L	.0.	(104) Highway System	١
B.I.N= 89B		AASH		(20)	16
RANK= 0 H.I.= NA	A FHV Identification	VA Select List= N	N (6/21/2017)	(100) Defense Highway	(
(8) Structure Number		B19005	89BMUNBRI	(101) Parallel Structure	1
(5) Inventory Route				(102) Direction of Traffic - 2-way traffic	2
(2) State Highway Department Dist			04	. ,	١
(o) county code	4) Place code		07420	, ,	(
(6) Features Intersected			ISH BROOK	. , ,	١
(7) Facility Carried		HVVY EN	IDICOTT RD		
(9) Location (11) Kilometerpoint			0000.000	(21) Maintain - Town Agency (22) Owner - Town Agency	03 03
(12) Base Highway Network			0000.000 N		0.0
(13) LRS Inventory Route & Subro	ite 00	0000000000	IN.	Condition	Code
(16) Latitude		2DEG 37MIN	50.42 SEC	(58) Deck	<u> </u>
(17) Longitude		ODEG 58MIN	25.69 SEC	(50) Comparation to the	N
(98) Border Bridge State Code		Shar	re %	(60) Substructure	N
(99) Border Bridge Structure No.	#			(61) Channel & Channel Protection	7
. ,	ure Type and Material			(62) Culverts Load Rating and Posting	Code
(43) Structure Type Main: M	lasonry	Code	819	Load Rating and Posting	Code
Culvert	Jointless brid	ge type: Not a	pplicable	(63) Operating Rating Method - Allowable Stress (AS)	2
(44) Structure Type Appr:				(64) Operating Rating	00.0
Other		Code	000	(65) Inventory Rating Method - Allowable Stress (AS)	2
(45) Number of spans in main unit			002	(66) Inventory Rating	00.0
(46) Number of approach spans			0000	(70) Bridge Posting	C
(107) Deck Structure Type -	Not applicable		Code N	(41) Structure - Open Appraisal	Code
(108) Wearing Surface / Protective	-			(67) Structural Evaluation	Code
A) Type of wearing surface -	Bituminous		Code 6	(68) Deck Geometry	2
B) Type of membrane -	Not applicable=no		Code N	(69) Underclearances, vert. and horiz.	N
C) Type of deck protection -	Not applicable=no of Age and Service		Code N	(71) Waterway adequacy	6
(27) Year Built	Age and Service		1850	(72) Approach Roadway Alignment	7
(106) Year Reconstructed			1900	(**)	0 0 0
(42) Type of Service: On -	Highway		1300	(113) Scour Critical Bridges Inspections	6
Under - Waterway	gy		Code 15	(90) Inspection Date 05/07/20 (91) Frequency	24 N
(28) Lanes: On Structure	02	Under structure	00	(92) Critical Feature Inspection: (93)	CFI DAT
(29) Average Daily Traffic			007200	(A) Fracture Critical Detail N 00 MO A)	00/00/
	18 (109) Truck ADT		05 %	(B) Underwater Inspection N 00 MO B)	00/00/
(19) Bypass, detour length			004 KM	(C) Other Special Inspection N 00 MO C)	00/00/
	Geometric Data			(*) Other Inspection () N 00 MO *)	00/00/
(48) Length of maximum span			0002.4 M	(*) Closed Bridge N 00 MO *)	00/00/
(49) Structure Length		D: 1	00005.1 M	(*) UW Special Inspection N 00 MO *)	00/00/
(50) Curb or sidewalk:	Left 01.2 M	Righ		(*) Damage Inspection MO *) Rating Loads	00/00/
(51) Bridge Roadway Width Curb to	o Curb		007.3 M		Type HS
(52) Deck Width Out to Out	shouldors)		010.2M	Operating 0.0 0.0 0.0	0.0
(32) Approach Roadway Width (w/s	,	Cod	008.3 M e 0	Inventory 0.0 0.0 0.0	0.0
(33) Bridge Median - No media (34) Skew 00 DEG	(35) Structure Fla			Field Posting	
(10) Inventory Route MIN Vert Clea	` ,	· Cu	N 99.99 M	Status Posting Date 00/00/00 2 Axle 3 Axle 5 Axle S	
(47) Inventory Route Total Horiz Cl			08.3 M	2 Axie 3 Axie 5 Axie 5	Single
(53) Min Vert Clear Over Bridge Ro			99.99 M	Recommended	
(54) Min Vert Underclear ref	N		00.00 M	Missing Signs N	
(55) Min Lat Underclear RT ref	N		00.00 M	Misc.	
(56) Min Lat Underclear LT			00.0 M	Bridge Name	ei al ar a
	Navigation Data			N Anti-missile fence N Acrow Panel N Jointless Br	iage
(38) Navigation Control - No na	vigation control on wa	terway	Code 0	Freeze/Thaw N : Not Applicable Accessibility (Needed/Used)	
(111) Pier Protection			Code	N / N Liftbucket N / N Rigging N / N Other	er
(39) Navigation Vertical Clearance			000.0 M	N/N Ladder N/N Staging	
(116) Vert-lift Bridge Nav Min Vert C	Clear		M	Y / Y Boat N / N Traffic Control	··
(40) Navigation Horizontal Clearanc	e		0000.0 M	P / Y Wader N / N RR Flagperson Hours:	

ATTACHMENT F: BROOKVIEW ROAD OVER FISH BROOK (B-19-018)

- ROUTINE UNDERWATER INSPECTION REPORT PERFORMED BY MASSDOT ON SEPTEMBER 18, 2020
- SPECIAL MEMBER INSPECTION REPORT PERFORMED BY MASSDOT ON JANUARY 31, 2020
- ROUTINE & SPECIAL MEMBER INSPECTION REPORT PERFORMED BY MASSDOT ON NOVEMBER 26, 2019

2-DIST **04**

B.I.N. **C68**

UNDERWATER OPERATIONS TEAM ROUTINE UNDERWATER INSPECTION REPORT

BR. DEPT. NO. **B-19-018**

CITY/TOWN LEVEL OF INSPECTION 93B-DATE INSPECTED 8-STRUCTURE NO BOXFORD B19018-C68-MUN-BRI **SEP 18, 2020** Ш 7-FACILITY CARRIED ACCESS TO BRIDGE UNDERWATER OPERATIONS ENGINEER HWY BROOKVIEW RD RANDI E. BONICA **EMBANKMENT** 06-FEATURES INTERSECTED DEPTH VISIBILITY TEAM LEADER (DIVE MASTER) Report submitted by: WILLIAM J. COLLERAN WATER FISH BROOK 1 m 0.5 m BOTTOM CONDITION TEAM MEMBERS CURRENT R. E. BONICA, G. BROZ, B. FITZGERALD, M. GRIFFIN **BOULDERS & SAND** SLIGHT ITEM 60 ITEM 61 **ITEM 62** CHANNEL & 6 6 Ν SUBSTRUCTURE **CULVERTS CHANNEL PROTECTION** DEF DEF DFF 1. Abutments 6 1. Channel Scour Ν 1. Roof N a. Pedestals Ν 2. Embankment Erosion 6 -2. Floor N 6 Ν b. Bridge Seats 3. Debris 3. Walls Ν c. Backwalls 7 Ν 4. Vegetation 4. Headwall 6 d. Breastwalls Ν Ν 5. Utilities 5. Wingwall 6 e. Wingwalls 6. Rip-Rap/Slope Protection Ν Ν 6. Pipe Ν f. Slope Paving/Rip-Rap 7 Ν 7. Aggradation 7. Protective Coating Ν g. Pointing Ν Ν 8. Fender System 8. Embankment h. Footings N Ν Ν 9. Wearing Surface a. Piles N i. Piles N Ν b. Diagonal Bracing 10. Railing 6 j. Scour Ν Ν c. Horizontal Bracing 11. Sidewalks 6 k. Settlement Ν Ν d. Wales 12. Utilities Ν Ν Ν e. Fasteners 13. Member Alignment 2. Piers or Bents Ν Ν Ν f. Ladders 14. Deformation Ν -Ν a. Pedestals Ν N b. Caps Ν ITEM 59 SUPERSTRUCTURE 16. Settlement DFF Ν Ν c. Columns 17. Ν Ν d. Stems/Webs/Pierwalls 18. Ν Ν e. Pointing Ν UNDERMINING (Y/N) Ν f. Footing **DEFICIENCY REPORTING GUIDE** N g. Piles DEFICIENCY: A defect in a structure that requires corrective action. Ν h. Scour CATEGORIES OF DEFICIENCIES: Ν i. Settlement M= Minor Deficiency- - Deficiencies which are minor in nature, generally do not impact the structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor scouring, etc. Ν S= Severe/Major DeficiencyDeficiencies which are more extensive in nature and need more planning and effort to repair. Examples include but are not limited to: Moderate to major deterioration in concrete, Exposed and corrording rebars, Deteriorated timber piles, Considerable settlement, Considerable scouring or undermining, etc. N 3. Pile Bents Ν A deficiency in a structural element of a bridge that poses an extreme unsafe condition due to the failure or imminent failure of the element which will affect the structural integrity of C-S= Critical Structural Deficiencya. Pile Caps Ν the bridge. A deficiency in a component or element of a bridge that poses an extreme hazard or unsafe condition to the public, but does not impair the structural integrity of the bridge. Examples include but are not limited to: Any part of piles or fender system which are projecting outward and may become a safety hazard for the navigational traffic, etc. C-H= Critical Hazard Deficiencyb. Piles N Ν c. Diagonal Bracing URGENCY OF REPAIR: N d. Horizontal Bracing [Inspector(s) immediately contact District Bridge Inspection Engineer (DBIE) to report the Deficiency and to receive further instruction from him/her.] I=Immediate-Ν e. Fasteners [Action/Repair should be initiated by District Maintenance Engineer or the responsible party (if not a State owned bridge) upon receipt of the Inspection Report.] A=ASAP-IShall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs UNDERMINING (Y/N) P=Prioritize-

REMARKS

GENERAL REMARKS

This bridge is a single span structure with dry laid fieldstone abutments and concrete bridge seats.

Orientation: Abutments are labeled left and right, looking downstream.

Sta 10+00 is at the downstream end of the superstructure. Sta 10+25 is the upstream end of the superstructure.

The undermining of the right abutment, the settled stone in the right abutment and the large void in the left abutment and many smaller voids throughout the structure reported in inspection reports prior to 12/23/19 have been repaired with a concrete tremie repair.

The undermining void on the right side was from Sta 10+10 to Sta 10+22 with a maximum height of 1.6' and a maximum penetration of 3.3'.

The settled stone in the right abutment was from Sta 10+05 to Sta 10+12 and had displaced 0.8' toward the channel and there was 0.3' settlement down from the bridge seat. There was a 1/8" crack in the bridge seat above the settled stone.

The large void in the left abutment was from Sta 10+06 to Sta 10+15 with a height of 3.5' and a penetration of 4.9'. This void was directly below the bridge seat. There was a 1/8" crack in the bridge seat above the void area.

The repair was done by members of the Mass DOT Underwater Operations Team. The repair was done between 12/13/19 and 12/23/19. The repair consisted of laying out grout bags to be used as a form to repair undermining and filling undermined area with concrete. Forms were then constructed in front of the void areas and filled with tremie concrete. Extensive voids behind and between stones in both abutments due to erosion of material were filled with concrete. Eighteen cubic yards of concrete were pumped into grout bags, undermined areas and into voids in the breastwalls.

ITEM 60 - SUBSTRUCTURE

<u>Item 60.1 - Abutments</u> <u>Item 60.1.d - Breastwalls</u>

Left Breastwall

Breastwall is dry laid fieldstone.

Prior to the repair of December 2019 there was a large void in the breastwall from Sta 10+06 to Sta 10+15. The void was 3.5' high with 4.9' penetration. The bridge seat above the void was unsupported and had a 1/8" vertical crack. The void area continued behind stones in the breastwall to the downstream end of the breastwall (Sta 10+01). There were several other voids. As part of the repair forms were put in front of the breastwall from Sta 10+01 (downstream end of breastwall) to Sta 10+17 and at the downstream wingwall. The entire area was filled with concrete. Breastwall is now in equilibrium with the bridge seat supported for the full length of the abutment.

Upstream of the repaired area stones are in good condition with no large voids.

Right Breastwall

Breastwall is dry laid fieldstone.

Prior to the repair of December 2019 there was a large stone from Sta 10+05 to Sta 10+12 that had displaced 0.8' toward the channel and there was 0.3' settlement down from the bridge seat. There was a 1/8" crack in the bridge seat above the settled stone. There was a large void below the stone and the stone was only supported by point loading at the ends. There was a void at the downstream end of the breastwall measuring 3.0' long, 1.5' high and 3.5' penetration. There were several other voids. As part of the repair forms were put in front of the breastwall from Sta 9+97 (downstream end of breastwall) to Sta 10+21 and at

REMARKS

Item 60.1.d - Breastwalls (Cont'd)

the downstream wingwall. The entire area was filled with concrete. Breastwall is now in equilibrium with the bridge seat supported for the full length of the abutment.

Upstream of the repaired area stones are in good condition with no large voids.

Item 60.1.e - Wingwalls

Wingwalls are dry laid fieldstone.

Downstream Right Wingwall

Prior to the repair of December 2019 there was a void at the downstream end of the breastwall/wingwall measuring 3.0' long along the breastwall, 1.5' high and 3.5' penetration into the wingwall. There were several other voids. As part of the repair forms were put in front of the breastwall and the downstream wingwall. The entire area was filled with concrete. Breastwall and wingwall are now in equilibrium.

The downstream left and upstream right and left wingwalls are in good condition with no large voids.

Item 60.1.j - Scour

The undermining of the right abutment reported in inspection reports prior to 12/23/19 has been filled in with a concrete tremie repair. The undermining void was from Sta 10+10 to Sta 10+22 with a maximum height of 1.6' and a maximum penetration of 3.3'. The repair consisted of using grout bags filled with concrete and sand bags to build a form in front of the undermined area. The undermined area was then filled with tremie concrete. Extensive voids in both abutments were filled with concrete.

Item 60.1.k - Settlement

Left Breastwall

Breastwall is dry laid fieldstone.

Prior to the repair of December 2019 there was a large void in the breastwall from Sta 10+06 to Sta 10+15. The void was 3.5' high with 4.9' penetration. The bridge seat above the void was unsupported and had a 1/8" vertical crack. The void area continued behind stones in the breastwall to the downstream end of the breastwall (Sta 10+01). There were several other voids. As part of the repair forms were put in front of the breastwall from Sta 10+01 (downstream end of breastwall) to Sta 10+17 and at the downstream wingwall. The entire area was filled with concrete. Breastwall is now in equilibrium with the bridge seat supported for the full length of the abutment.

Upstream of the repaired area stones are in good condition with no large voids.

Right Breastwall

Breastwall is dry laid fieldstone.

Prior to the repair of December 2019 there was a large stone from Sta 10+05 to Sta 10+12 that had displaced 0.8' toward the channel and there was 0.3' settlement down from the bridge seat. There was a 1/8" crack in the bridge seat above the settled stone. There was a large void below the stone and the stone was only supported by point loading at the ends. There was a void at the downstream end of the breastwall measuring 3.0' long, 1.5' high and 3.5' penetration. There were several other voids. As part of the repair forms were put in front of the breastwall from Sta 9+97 (downstream end of breastwall) to Sta 10+21 and at the downstream wingwall. The entire area was filled with concrete. Breastwall is now in equilibrium with the bridge seat supported for the full length of the abutment.

Upstream of the repaired area stones are in good condition with no large voids.

REMARKS

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

The undermining of the right abutment reported in inspection reports prior to 12/23/19 has been filled in with a concrete tremie repair. The undermining void was from Sta 10+10 to Sta 10+22 with a maximum height of 1.6' and a maximum penetration of 3.3'. The repair consisted of using grout bags filled with concrete and sand bags to build a form in front of the undermined area. The undermined area was then filled with tremie concrete. Extensive voids in both abutments were filled with concrete.

<u>Item 61.2 - Embankment Erosion</u>

There is minor embankment erosion at all embankments.

Item 61.3 - Debris

There is some tree debris in the channel at the upstream end of the bridge.

There is a large tree with a diameter of 1.5' across the channel approximately 100' downstream of the bridge.

Sketch / Chart Log

Sketch 1: PLAN VIEW SHOWING UNDERMINING, VOIDS AND DISPLACED STONE PRIOR TO 2019

REPAIR

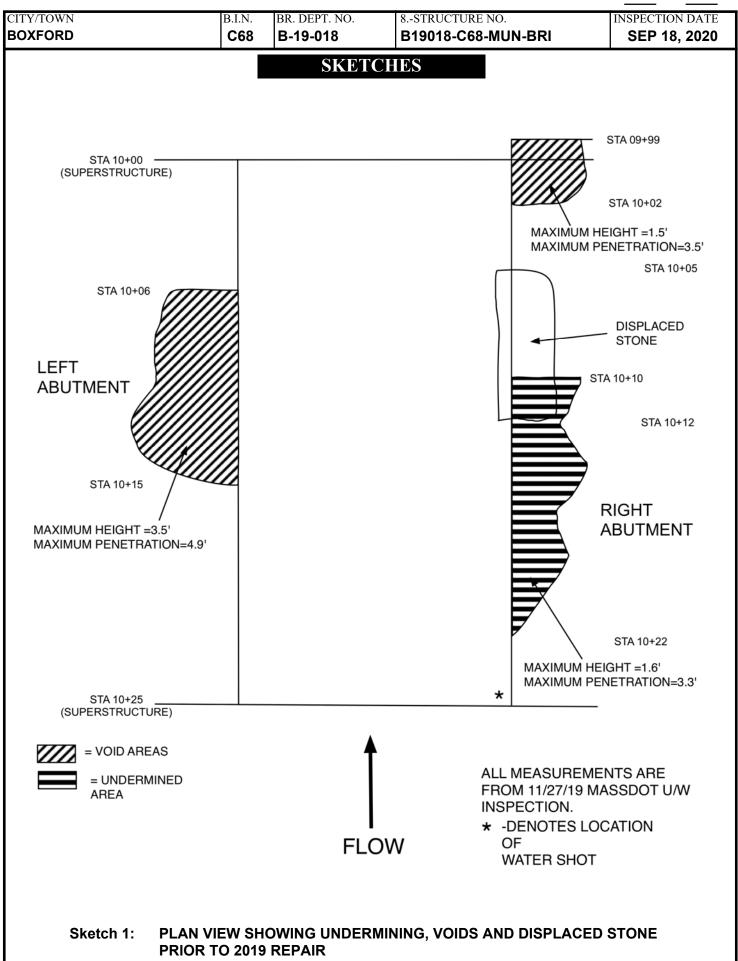
Sketch 2: PLAN VIEW SHOWING GROUT BAG PLACEMENT FOR 2019 UNDERMINING REPAIR

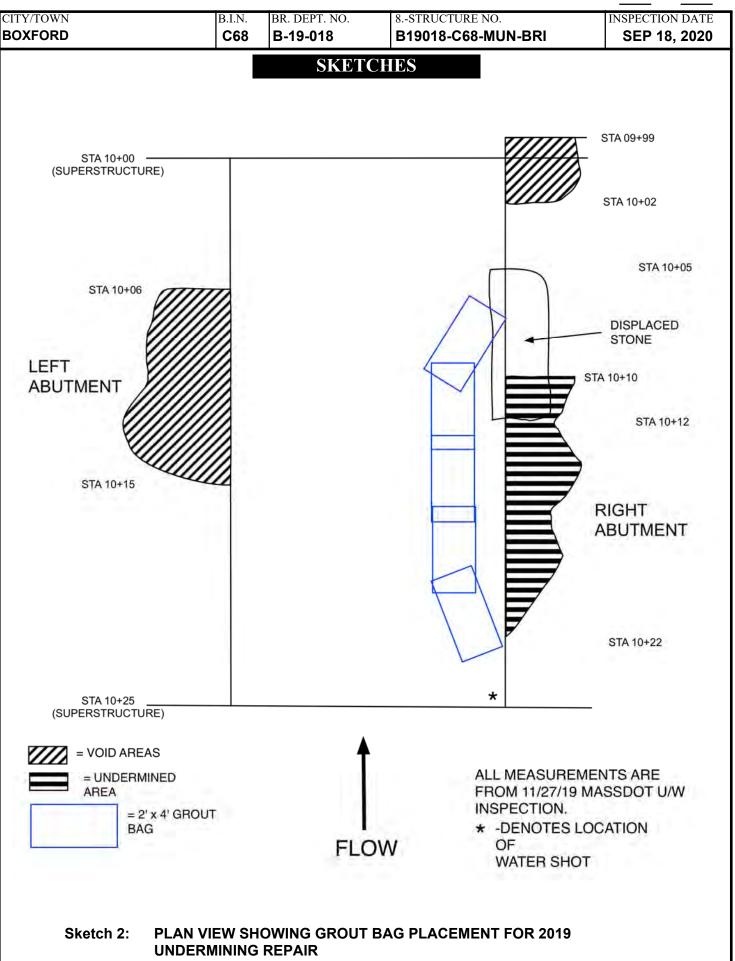
Sketch 3: ELEVATION VIEW AT STA 10+08 PRIOR TO 2019 REPAIR Sketch 4: ELEVATION VIEW AT STA 10+08 AFTER 2019 REPAIR

Sketch 5: ELEVATION VIEW AT STA 10+14 PRIOR TO 2019 REPAIR Sketch 6: ELEVATION VIEW AT STA 10+14 AFTER 2019 REPAIR

Sketch 6: ELEVATION VIEW AT STA 10+14 AFTER 2019 REPAIR Chart 1: UNDERMINING & VOID CHART PRIOR TO 2019 REPAIR

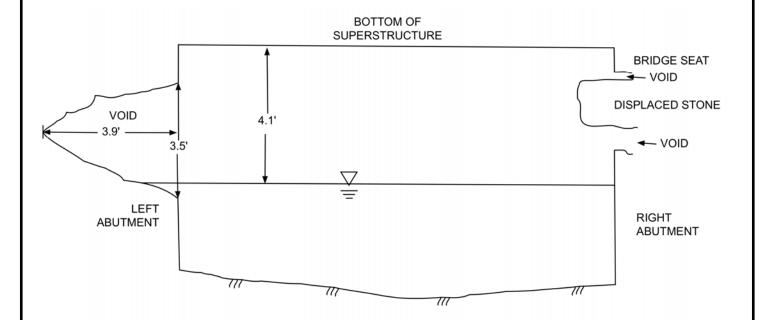
Chart 2: SCOUR MONITORING CHART





CITY/TOWN
BOXFORD
B.I.N. BR. DEPT. NO. 8.-STRUCTURE NO. B19018-C68-MUN-BRI SEP 18, 2020

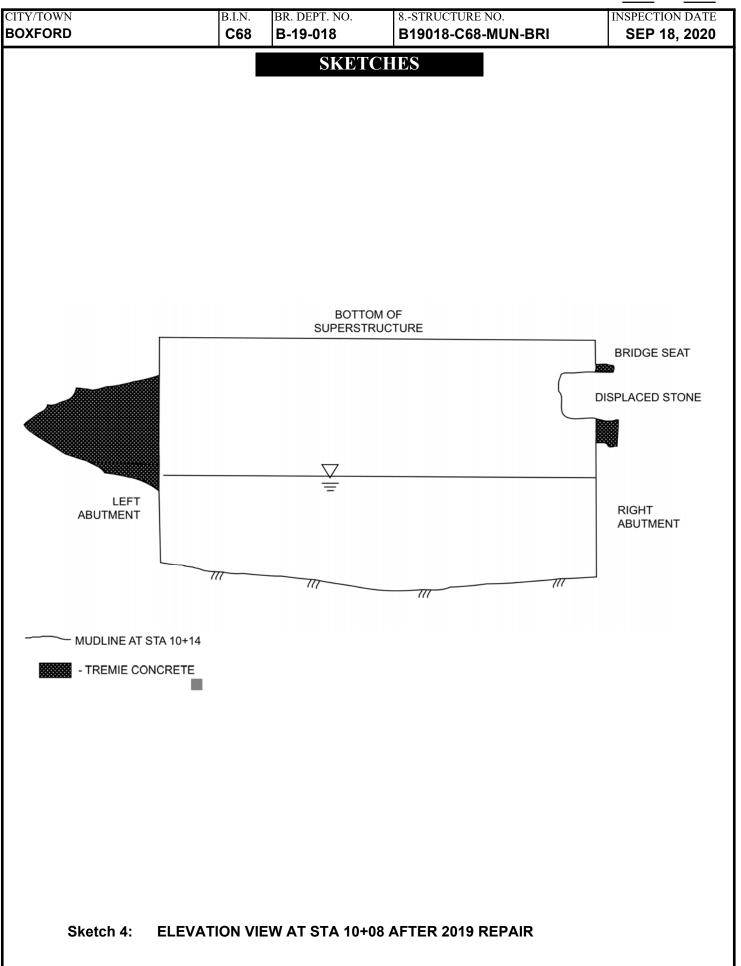
SKETCHES



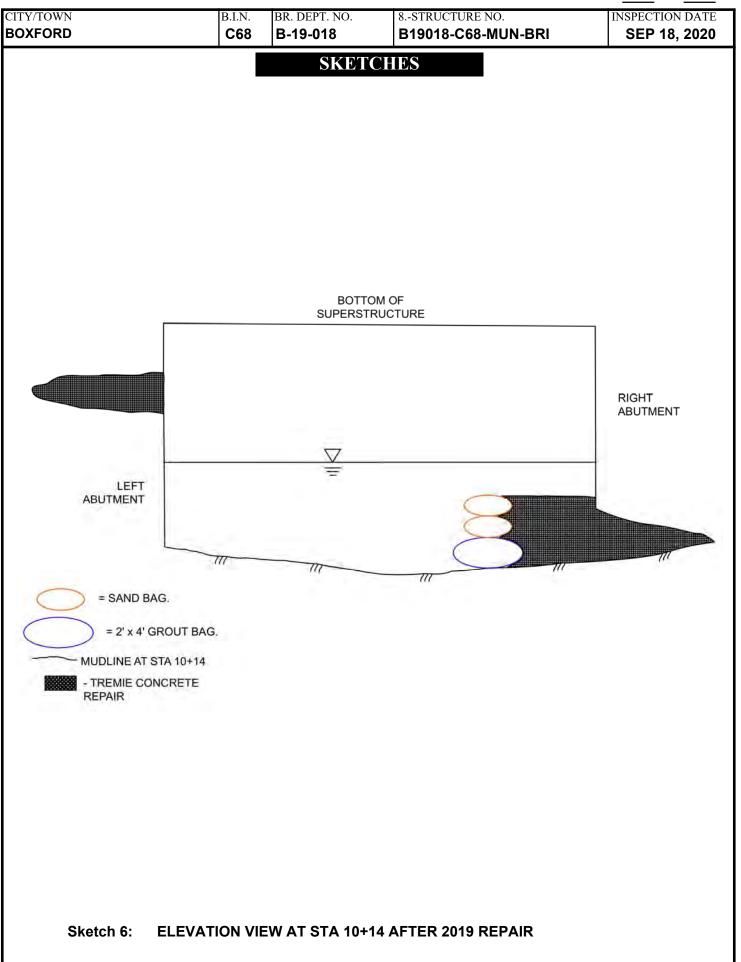
MUDLINE AT STA 10+14

NOTE: ALL SOUNDINGS AND FIELD MEASUREMENTS
TAKEN DURING U/W INSPECTION DATED 11/27/19

Sketch 3: ELEVATION VIEW AT STA 10+08 PRIOR TO 2019 REPAIR



CITY/TOWN BR. DEPT. NO. INSPECTION DATE B.I.N. 8.-STRUCTURE NO. **BOXFORD C68** B-19-018 B19018-C68-MUN-BRI **SEP 18, 2020 SKETCHES BOTTOM OF** SUPERSTRUCTURE VOID 1.3' RIGHT 4.1' ABUTMENT - 3.8' -**LEFT ABUTMENT** 1.6'UNDERMINING - 3.3'*—* - MUDLINE AT STA 10+14 NOTE: ALL SOUNDINGS AND FIELD MEASUREMENTS TAKEN DURING U/W INSPECTION DATED 11/27/19 Sketch 5: **ELEVATION VIEW AT STA 10+14 PRIOR TO 2019 REPAIR**



CITY/TOWN	B.I.N.	BR. DEPT. NO.	8STRUCTURE NO.	INSPECTION DATE
BOXFORD	C68	B-19-018	B19018-C68-MUN-BRI	SEP 18, 2020

CHARTS

RIGHT ABUTMENT UNDERMINING CHART

	11,	/27/19
	HEIGHT	PENETRATION
STA 10+10	0.6'	3.0'
STA 10+12	0.7'	2.1'
STA 10+14	1.6'	3.3'
STA 10+16	1.2'	2.0'
STA 10+18	0.9'	2.5'
STA 10+20	0.4'	1.5'
STA 10+22	0.0'	0.0'

LEFT ABUTMENT VOID CHART

	11/27/19						
	HEIGHT	PENETRATION					
STA 10+06	2.0'	2.9'					
STA 10+08	3.5'	3.9'					
STA 10+10	3.3'	3.8'					
STA 10+12	3.0'	4.9'					
STA 10+14	1.3'	3.8'					
STA 10+15	0.0'	0.0'					

NOTES:

1. STA 10+00 IS AT THE DOWNSTREAM END OF THE SUPERSTRUCTURE.

Chart 1: UNDERMINING & VOID CHART PRIOR TO 2019 REPAIR

CITY/TOWN	B.I.N.	BR. DEPT. NO.	8STRUCTURE NO.	INSPECTION DATE
BOXFORD	C68	B-19-018	B19018-C68-MUN-BRI	SEP 18, 2020

CHARTS

	11/27/19	12/23/19	9/18/20	
STA 10+00				
LEFT ABUTMENT	1.3'	1.2'	1.7'	
C/L SPAN	2.6'	2.6'	3.8'	
RIGHT ABUTMENT	3.5'	3.0'	3.2'	
STA 10+14				
LEFT ABUTMENT	2.5'	1.5'/3.2'	1.5'/3.2'	
C/L SPAN	3.3'	3.3'	3.3'	
RIGHT ABUTMENT	3.0'	1.6'/3.2'	1.7'/3.5'	1 (1
STA 10+25				
LEFT ABUTMENT	2.0'	2.3'	2.2'	
C/L SPAN	1.9'	2.0'	2.0'	
RIGHT ABUTMENT	1.0'	1.1'	0.9'	
Υ	4.1'	4.3'	5.3'	
CORRECTION FACTOR		+0.2'	+1.2'	

NOTES:

- 1. WATER CONTROL SHOT (Y) = WATERLINE TO BOTTOM OF BEAM AT UPSTREAM END OF RIGHT ABUTMENT.
- 2. FOR COMPARISON ALL SOUNDINGS ARE ADJUSTED TO 11/2019 WATER LEVEL.
- 3. STA 10+00 IS AT THE DOWNSTREAM END OF THE SUPERSTRUCTURE.
- 4. WHERE TWO SOUNDINGS ARE SHOWN FIRST NUMBER IS ON TOP OF REPAIR/SECOND NUMBER IS OFF REPAIR.

Chart 2: SCOUR MONITORING CHART

	nber 20, 2020 State Informatio	n			Classification			Code
BDEPT#= B19018		Agency Br.No.		(112) NBIS Bridge Length				N
Town= Boxford			.0.	(104) Highway System				N
B.I.N= C68		AASH		(20)	Urban Local			19
RANK= 0 H.I.=	NA ldentification	FHWA Select List= N	N (6/21/2017)	(100) Defense Highway				(
(8) Structure Number		B19018	C68MUNBRI	(101) Parallel Structure				١
(5) Inventory Route				(102) Direction of Traffic -		ay traffic		2
(2) State Highway Department			04	. ,				٨
(3) County Code 009	(4) Place code	WATER	07420	` '	-			N
(6) Features Intersected			ISH BROOK	. , .	r Network free road			3
(7) Facility Carried(9) Location		HWT BRO	OKVIEW RD	(-)	Town Agency			03
(11) Kilometerpoint			0000.000	• •	Town Agency			03
(12) Base Highway Network			0000.000 N					00
(13) LRS Inventory Route & Si	ubroute			(07) Thistorical digrillication	Condition			Code
(16) Latitude		42 DEG 39 MIN	53.40 SEC	(58) Deck				5
(17) Longitude		71 DEG 01 MIN	40.30 SEC	(EO) Superetructure				5
(98) Border Bridge State Code		Shar		(60) Substructure				5
(99) Border Bridge Structure N				(61) Channel & Channel P	rotection			6
8	tructure Type and M	laterial		(62) Culverts	Load Pating and D	looting		Code
(43) Structure Type Main:	Prestressed Con-		501	(31) Design Load -	Load Rating and P Unknown	osuny		Code 0
Slab	Jointles	s bridge type: Not a	pplicable	(63) Operating Rating Meth				
(44) Structure Type Appr:				(64) Operating Rating	··			(
Other		Code	000	(65) Inventory Rating Meth	od -			
(45) Number of spans in main	unit		001	(66) Inventory Rating				(
(46) Number of approach spar	ns		0000	(70) Bridge Posting				C
(107) Deck Structure Type -	Concrete Preca	ast Panels	Code 2	(41) Structure -	Open			A
(108) Wearing Surface / Prote	ctive System:			(Appraisal			Code
A) Type of wearing surface	e - Bituminous		Code 6	(67) Structural Evaluation				3
B) Type of membrane -	Unknown		Code 8	(68) Deck Geometry(69) Underclearances, vert	and horiz			N
C) Type of deck protection			Code 8	(71) Waterway adequacy	. and nonz.			7
	Age and Service	e		(72) Approach Roadway A	lignment			7
(27) Year Built			0000	(36) Traffic Safety Feature	=		(0 0 0 0
(106) Year Reconstructed			0000	(113) Scour Critical Bridge	s			
(42) Type of Service: On -	Highway			(00)	Inspections			
Under - Waterway			Code 15	(90) Inspection Date	11/26/19	(91) Freq		24 N 3) CFI DAT
(28) Lanes: On Structure	02	Under structure	00	(92) Critical Feature Inspect (A) Fracture Critical Deta		00 M	(9. O A)	00/00/0
(29) Average Daily Traffic	0040 (400) T	. ADT	000350	(B) Underwater Inspectio			O A) O B)	09/18/2
(30) Year of ADT	2019 (109) Truc	KADI	05 %	(C) Other Special Inspec	-		O C)	01/31/2
(19) Bypass, detour length	Geometric Data	a	008 KM	(*) Other Inspection ()	N N		O *)	00/00/0
(48) Length of maximum span			0004.1 M	(*) Closed Bridge	N		O *)	00/00/0
(49) Structure Length			00004.1 M	(*) UW Special Inspection			O *)	00/00/0
(50) Curb or sidewalk:	Left 00	0.0 M Righ	t 00.0 M	(*) Damage Inspection		M	O *)	00/00/0
(51) Bridge Roadway Width Co	urb to Curb		000.0 M	Report Date 00/00/00	Rating Loads		Tumo 202	Time IIC
(52) Deck Width Out to Out			000.0 M	Report Date 00/00/00 Operating	H20 0.0	Type 3 0.0	Type 3S2 0.0	Type HS 0.0
(32) Approach Roadway Width	(w/shoulders)		000.0 M	Inventory	0.0	0.0	0.0	0.0
(33) Bridge Median -		Cod	е		Field Posting			
(34) Skew 00 DE	G (35) Structu	re Flared	N	Status		Posting Da	ate 00/00/	/00
(10) Inventory Route MIN Vert	Clear		00.00 M	2 Ax	le 3 Axle	5 Axl	е	Single
(47) Inventory Route Total Hor	iz Clear		00.0 M	Actual				
(53) Min Vert Clear Over Bridg	e Rdwy		00.00 M	Recommended				
(54) Min Vert Underclear ref			00.00 M	Missing Signs N	Misc.			
,	f		00.0 M	Bridge Name				
(55) Min Lat Underclear RT re			00.0 M	N Anti-missile fence	N Acrow Panel	N	Jointless	Bridge
	Ne to the Both			Freeze/Thaw				=
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT	Navigation Data		0-4 ^					
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N	Navigation Datanavigation control		Code 0		Accessibility (Neede	d/Used)		
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection	o navigation control		Code 1	N / N Liftbucket	Accessibility (Neede N / N Rigging	d/Used) _	Ot	ther
(55) Min Lat Underclear RT ret (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection (39) Navigation Vertical Cleara	lo navigation control		Code 1 000.0 M	N / N Liftbucket N / N Ladder	N / N Rigging N / N Staging		Oi	ther
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection	lo navigation control nce /ert Clear		Code 1	N / N Liftbucket	N / N Rigging	ntrol		ther

2-DIST B.I.N. **C68**

STRUCTURES INSPECTION FIELD REPORT

SPECIAL MEMBER INSPECTION

BR. DEPT. NO. **B-19-018**

CITY	Y/TOWN		8	-STRI	UCTURE NO.		1	1-Kilo. PC	INT	90-ROI	ITINE IN	JSP DAT	E 03*_S	PEC 1	IEMR I	NSP. DATE
	XFORD		0.		B19018-C68-MUN-BRI						Nov 26, 2019			Jan 31, 202		
								000.0		· ·						
	ACILITY CARRIED VY BROOKVIEW R	RD		N	MEMORIAL NAME/LOCAL NA					7R BUILT 106-YR RE 1000			T *YR REHAB'D (NON 106) 0000			N 106)
06-FEATURES INTERSECTED 26-				26-FUNCTIONAL C			DIST. BR	RIDGE IN	SPECTI	ON ENC	SINEER	J. Did	eo			
W	ATER FISH BROOK	•		Į	Urban Loca	al										
_	TRUCTURE TYPE 1: Prestressed Cor	acroto	Clab			21-MAINTA Town Ag		TEAM L	EADER A	A. Labib						
		icrete	Siab													
	DECK TYPE Concrete Precast I	Panels			weather Clear	TEMP. (air) -5°C		K. CA		S						
WI	EIGHT POSTING		plicable		(At k	oridge		Advan	се		ANIC	()//	NI).	
Ac	tual Posting N		N Single	P	Signs In F (Y=Yes,N=		N	S	$\neg \parallel \vdash$	N	S		ANS	(Y/	IN). I	1
	ecommended Posting N	N	N N		NR=Not R Legibility/	. /			 			า	C.R.)	(Y/	N): I	N
		EJDMT Da		 0/00	Vicibility	′						$ _{TA}$	PE#:			
	ATING	205		0,00								If YES ;	loaso d	uivo ni	iority	
	ting Report (Y/N):	Data:			Recomme	end for Rat	ting or	Reratir	ng (Y/N)): N	1 1	HIGH () LOV	V ()
INA		Date:			REAS	ON:										
1 58	Inspection data at tim 3: - 159: - 160: - 1		ing rating Date :00	0/00/	/0000											
SP	ECIAL MEMBER(S):															
	MEMBER	CRACK (Y/N):	WELD'S CONDITION (0-9)	L	OCATION OF CORR	ROSION, SECT GE, STRESS C	CONCEN	SS (%), CF TRATION,	RACKS, ETC.	COND PREVIOUS (0-9)	PRESENT	INV. RAT FROM R			IS Defi	ciencies
Α	ltem 60.1.d - Breastwalls	N	(6.6)	Se	e remarks	in comi	ment	s sec	tion.	2	5		t Rat			М-Р
В	Item 60.1.j - Scour	N		Se	e remarks	in comi	ment	s sec	tion.	3	6	No	t Rat	ed		-
С	Item 61.1 - Channel Scour	N	N	Se	e Item 60.1	.j - Sco	ur			3	6	No	t Rat	ed		-
D																
Е																
Lis	st of field tests performed:		1									I-:	58 I	-59	I-60	I-62
							(Overa	ıll Previ	ous Co	ndition	1)		5	5	2	-
						((Overa	II Curre	nt Con	dition)		į	5	5	5	-
DE	EFICIENCY: A defect in a stru	ucture that re	equires correc	tive ac	ction.											
	ATEGORIES OF DEFICIENCE		ur in not	venille.	not impost the administration of	intogrit: -f '' ' '	dao '	ould e == 2001	ronsiss ! T	vom=!-	dude 5.1	o not lin '	to: C=: "	l nor	. Mi=	
M=	= Minor Deficiency - Deficiencies holes, Minor	wnich are mino corrosion of ste eficiencies which	or in nature, gene eel, Minor scourii h are more exter	erally do ng, Clog nsive in l	not impact the structural i gged drainage, etc. nature and need more pla	integrity of the bri	uge and co to repair ⊏	vuid easily be	repaired. E	xamples inc	aude but ar	e not limited e to maior de	ιο: Spalled	in concre	e, minor pot	d and
5=	Severe/Major Deficiency - Defic															
	S= Critical Structural Deficie H= Critical Hazard Deficienc	the b	ridge. ency in a compoi	nent or e	ement of a bridge that pos element of a bridge that po loose concrete hanging dov	oses an extreme	hazard or u	unsafe condit	tion to the pu	ublic, but do	es not imp	air the struct	ıral integrit	y of the b	ridge. Exan	nples
UF	RGENCY OF REPAIR:															
A =	= ASAP- [Action/Repair should	be initiated by	District Maintena	ince Eng	Engineer (DBIE) to report to gineer or the Responsible Responsible Party (if not a	Party (if not a Sta	ate owned	bridge) upon	receipt of th	e Inspectio		ailable].				

REMARKS

BRIDGE ORIENTATION

The one span structure carries Brookview Road over the Fish brook, which flows from west to east. For this report, the abutments are labeled South and North.

ITEM 60 - SUBSTRUCTURE

<u>Item 60.1 - Abutments</u> <u>Item 60.1.d - Breastwalls</u>

Both Breastwalls:

There are penetrable void areas between all stones.

Several stones have full height cracks, up to 1/4" wide.

The concrete cap has areas of minor honeycombing throughout.

For underwater details, refer to the Routine Underwater Inspection Report dated 12/23/2019.

North Breastwall:

The east half of the breastwall has been repaired with concrete. The repair is approximately 17' long x full height above the current water level. **See photos 1 & 2.**

The remaining length has penetrable voids between all stones. See photo 3.

At the west edge, the second stone from the top has a full height crack, up to 1" wide.

The concrete cap has a full height crack, up to 1/8" wide, above the west edge of the washout area. **See photo 4.**

South Breastwall:

There is a concrete repair along the majority of the breastwall length starting near the west end and extending into the southeast wingwall area. **See photo 5.**

Item 60.1.j - Scour

The previously noted scour area has been filled with a concrete tremie repair.

The Routine Underwater Inspection Report dated 12/23/2019 rated the element "6" with no deficiency code.

For underwater details, refer to the Routine Underwater Inspection Report dated 12/23/2019.

Photo Log

Photo 1: North breastwall, concrete repair throughout the east half.

Photo 2: North breastwall, concrete repair starting at the east edge; alternate view.

Photo 3: North breastwall, typical void areas throughout the remaining length past the repair.

Photo 4: North breastwall, west edge stone with a full height crack.

Photo 5: South breastwall, large concrete repair extending into the southeast wingwall corner.



Photo 1: North breastwall, concrete repair throughout the east half.



Photo 2: North breastwall, concrete repair starting at the east edge; alternate view.



Photo 3: North breastwall, typical void areas throughout the remaining length past the repair.



Photo 4: North breastwall, west edge stone with a full height crack.



Photo 5: South breastwall, large concrete repair extending into the southeast wingwall corner.

	nber 20, 2020 State Informatio	n			Classification			Code
BDEPT#= B19018		Agency Br.No.		(112) NBIS Bridge Length				N
Town= Boxford			.0.	(104) Highway System				N
B.I.N= C68		AASH		(20)	Urban Local			19
RANK= 0 H.I.=	NA Ldentification	FHWA Select List= N	N (6/21/2017)	(100) Defense Highway				(
(8) Structure Number		B19018	C68MUNBRI	(101) Parallel Structure				١
(5) Inventory Route				(102) Direction of Traffic -		ay traffic		2
(2) State Highway Department			04	. ,				٨
(3) County Code 009	(4) Place code	WATER	07420	` '	-			N
(6) Features Intersected			ISH BROOK	. , .	r Network free road			3
(7) Facility Carried(9) Location		HWT BRO	OKVIEW RD	(-)	Town Agency			03
(11) Kilometerpoint			0000.000	• •	Town Agency			03
(12) Base Highway Network			0000.000 N					00
(13) LRS Inventory Route & Si	ubroute			(07) Thistorical digrillication	Condition			Code
(16) Latitude		42 DEG 39 MIN	53.40 SEC	(58) Deck				5
(17) Longitude		71 DEG 01 MIN	40.30 SEC	(EO) Superetructure				5
(98) Border Bridge State Code		Shar		(60) Substructure				5
(99) Border Bridge Structure N				(61) Channel & Channel P	rotection			6
8	tructure Type and M	laterial		(62) Culverts	Load Pating and D	looting		Code
(43) Structure Type Main:	Prestressed Con-		501	(31) Design Load -	Load Rating and P Unknown	osuny		Code 0
Slab	Jointles	s bridge type: Not a	pplicable	(63) Operating Rating Meth				
(44) Structure Type Appr:				(64) Operating Rating	··			(
Other		Code	000	(65) Inventory Rating Meth	od -			
(45) Number of spans in main	unit		001	(66) Inventory Rating				(
(46) Number of approach spar	ns		0000	(70) Bridge Posting				C
(107) Deck Structure Type -	Concrete Preca	ast Panels	Code 2	(41) Structure -	Open			A
(108) Wearing Surface / Prote	ctive System:			(-) (-) (-) (-)	Appraisal			Code
A) Type of wearing surface	e - Bituminous		Code 6	(67) Structural Evaluation				3
B) Type of membrane -	Unknown		Code 8	(68) Deck Geometry(69) Underclearances, vert	and horiz			N
C) Type of deck protection			Code 8	(71) Waterway adequacy	. and nonz.			7
	Age and Service	e		(72) Approach Roadway A	lignment			7
(27) Year Built			0000	(36) Traffic Safety Feature	=		(0 0 0 0
(106) Year Reconstructed			0000	(113) Scour Critical Bridge	s			
(42) Type of Service: On -	Highway			(00)	Inspections			
Under - Waterway			Code 15	(90) Inspection Date	11/26/19	(91) Freq		24 N 3) CFI DAT
(28) Lanes: On Structure	02	Under structure	00	(92) Critical Feature Inspect (A) Fracture Critical Deta		00 M	(9. O A)	00/00/0
(29) Average Daily Traffic	0040 (400) T	. ADT	000350	(B) Underwater Inspectio			O A) O B)	09/18/2
(30) Year of ADT	2019 (109) Truc	KADI	05 %	(C) Other Special Inspec	-		O C)	01/31/2
(19) Bypass, detour length	Geometric Data	a	008 KM	(*) Other Inspection ()	N N		O *)	00/00/0
(48) Length of maximum span			0004.1 M	(*) Closed Bridge	N		O *)	00/00/0
(49) Structure Length			00004.1 M	(*) UW Special Inspection			O *)	00/00/0
(50) Curb or sidewalk:	Left 00	0.0 M Righ	t 00.0 M	(*) Damage Inspection		M	O *)	00/00/0
(51) Bridge Roadway Width Co	urb to Curb		000.0 M	Report Date 00/00/00	Rating Loads		Tumo 202	Time IIC
(52) Deck Width Out to Out			000.0 M	Report Date 00/00/00 Operating	H20 0.0	Type 3 0.0	Type 3S2 0.0	Type HS 0.0
(32) Approach Roadway Width	(w/shoulders)		000.0 M	Inventory	0.0	0.0	0.0	0.0
(33) Bridge Median -		Cod	е		Field Posting			
(34) Skew 00 DE	G (35) Structu	re Flared	N	Status		Posting Da	ate 00/00/	/00
(10) Inventory Route MIN Vert	Clear		00.00 M	2 Ax	le 3 Axle	5 Axl	е	Single
(47) Inventory Route Total Hor	iz Clear		00.0 M	Actual				
(53) Min Vert Clear Over Bridg	e Rdwy		00.00 M	Recommended				
(54) Min Vert Underclear ref			00.00 M	Missing Signs N	Misc.			
,	f		00.0 M	Bridge Name				
(55) Min Lat Underclear RT re			00.0 M	N Anti-missile fence	N Acrow Panel	N	Jointless	Bridge
	Ne to the Both			Freeze/Thaw				=
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT	Navigation Data		0-4 ^					
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N	Navigation Datanavigation control		Code 0		Accessibility (Neede	d/Used)		
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection	o navigation control		Code 1	N / N Liftbucket	Accessibility (Neede N / N Rigging	d/Used) _	Ot	ther
(55) Min Lat Underclear RT ret (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection (39) Navigation Vertical Cleara	lo navigation control		Code 1 000.0 M	N / N Liftbucket N / N Ladder	N / N Rigging N / N Staging		Oi	ther
(55) Min Lat Underclear RT re (56) Min Lat Underclear LT (38) Navigation Control - N (111) Pier Protection	lo navigation control nce /ert Clear		Code 1	N / N Liftbucket	N / N Rigging	ntrol		ther

2-DIST B.I.N.	STR	UCTU	IRE	S INSPE	ECTIO)N F	FIELI	D RE	P (ORT _	BF	R. DE	PT.	NO.
04 C68	R	OUTIN	E &	SPECIAL	L MEN	BEF	RINS	PECT	ΊΟ	N	E	3-19	-01	8
CITY/TOWN			8ST	TRUCTURE NO.			11-	Kilo. POIN	T	41-STATUS	90-R	OUTIN	NE IN	SP. DATE
BOXFORD				B19018-C	68-MUN	N-BRI		00.00	0	A:OPEN	N	OV	26,	2019
07-FACILITY CARRIED				MEMORIAL NA	ME/LOCAL N	NAME	1	27-YR BUIL	-YR BUILT 106-YR REBUI			` ′		
HWY BROOKVIEW RD								0000)	0000		(000	0
06-FEATURES INTERSECTED	,			26-FUNCTIONAL	L CLASS		DIST. BRI	DGE INSPE	ECTIO	ON ENGINEER	J. Did	eo		
WATER FISH BRO	OK			Urban Lo	cal									
43-STRUCTURE TYPE				22-OWNER Town	21-MAINT Town	AINER	TEAM LE	ADER A. L	abib					
501 : Prestressed	Conc	rete Slal	כ	Agency	Agency	,								
107-DECK TYPE		WEATHER	TEMP. (air	,	TEAM MI	EMBERS USTAI	- ^							
2 : Concrete Precast Panels				Clear	-3°	C	O. IVIO	USTAI	ГА					
ITEM 58	5		ITR	EM 59		5	7	III	EM (60		2		
DECK		DEF	SUP	ERSTRUCTU	U RE		│ DEF	SUB	STF	RUCTURE				DEF
1.Wearing Surface	7	-	1.Str	ingers		N	-	1. A	1. Abutments			Cur	2	
2.Deck Condition	5	M-P	2.Flo	orbeams		N	-		a. Pedestals			N		-
3.Stay in Place Forms	N	-	3.Flo	or System Bra	cing	N	-		b. Bridge Seats		N N	H		-
4.Curbs	N	-	4.Gir	ders or Beams	:	N	-		d. Breastwalls		N	2		C-S-I
5.Median	N	-	5.Tru	ısses - General	<u> </u>	N			e. Wingwalls			N H		-
6.Sidewalks	N	-	а.	Upper Chords	N		-		f. Slope Paving/Rip-Rap			N		-
7.Parapets	N	_	b.	Lower Chords	N		-	h. Fo		ys .	N	Н		-
8.Railing	3	S-A	c.	Web Members	N		-	i. Pil	les our		N	X 3		C-S-I
9.Anti Missile Fence	N		d.	d. Lateral Bracing			-	<i>k</i> . Se		nent	N	7		-
			e. Sway Bracings		N		-	I.			N	N		-
10.Drainage System	N	-	f. Portals			l	-		ers	or Bents	N	N	N	-
11.Lighting Standards	N	-	g.	End Posts	N	I	_	a. Pe			N	N	IN	_
12.Utilities	N	-	6.Pin	& Hangers		N	_	b. Ca		ais	N	N		-
13.Deck Joints	N	-	7.Co	nn Plt's, Gusse	ets & Angle	s N	-	c. Co			N	N		-
14.	N	-	8.Co	ver Plates		N	-	d. Ste		Webs/Pierwalls	N	N N		-
15.	N		9.Bearing Devices			N	—		otine		N	N		-

(In millimeters) **APPROACHES** DEF 6 a. Appr. pavement condition 7 b. Appr. Roadway Settlement c. Appr. Sidewalk Settlement Ν

Ν

Ν

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Ν

OVERHEAD SIGNS (Y/N) Ν (Attached to bridge) DEF a. Condition of Welds Ν Ν b. Condition of Bolts Ν c. Condition of Signs

1.Stringers	N	-					
2.Floorbeams		N	-				
3.Floor System Bracing		N	-				
4.Girders or Beams		N	-				
5.Trusses - General		N	-				
a. Upper Chords	N		-				
b. Lower Chords	N		-				
c. Web Members	N		-				
d. Lateral Bracing	N		-				
e. Sway Bracings	N		-				
f. Portals	N		-				
g. End Posts	N		-				
6.Pin & Hangers	N	-					
7.Conn Pit's, Gussets & An	N	-					
8.Cover Plates		N	-				
9.Bearing Devices	N	-					
10. Diaphragms/Cross Fran	nes	N	-				
11.Rivets & Bolts		N	-				
12. Welds		N	-				
13. Member Alignment		7	-				
14. Paint/Coating		N	-				
15. Slab		5	M-P				
Year Painted	N						
COLLISION DAMAGE: Please explain None (X) Minor () Moderate () Severe () LOAD DEFLECTION: Please explain None (X) Minor () Moderate () Severe ()							
LOAD VIBRATION: Please None (X) Minor () Model	expla	in					
Any Fracture Critical Memb	er: (`	Y/N)	N				

1. Abutments	Dive	Cur	2			
a. Pedestals	N	N		-		
b. Bridge Seats	N	Н		-		
c. Backwalls	N	Н		-		
d. Breastwalls	N	2		C-S-I		
e. Wingwalls	N	N		-		
f. Slope Paving/Rip-Rap	N	Н		-		
g. Pointing	N	N		-		
h. Footings	N	Н		-		
i. Piles	N	Х		-		
j. Scour	N	3		C-S-I		
k. Settlement	N	7		-		
I.	N	N		-		
m.	N	N		-		
2. Piers or Bents			N			
a. Pedestals	N	N		-		
b. Caps	N	N		-		
c. Columns	N	N		-		
d. Stems/Webs/Pierwalls	N	N		-		
e. Pointing	N	N		-		
f. Footing	N	N		-		
g. Piles	N	N		-		
h. Scour	N	N		-		
i. Settlement	N	N		-		
j.	N	N		-		
k.	N	N		-		
3. Pile Bents			N			
a. Pile Caps	N	N		-		
b. Piles	N	N		-		
c. Diagonal Bracing	N	N		-		
d. Horizontal Bracing	N	N		-		
e. Fasteners	N	N		-		
UNDERMINING (Y/N) If YES please explain N						
COLLISION DAMAGE: None (X) Minor () M	oderat	te () Se	vere ()		
SCOUR: Please explain						
None () Minor () M	oderat	te (X) Se	vere ()		

ort):	N

I-60 (Dive Repo

93B-U/W (DIVE) Insp

I-60 (This Report): 2 00/00/0000

Any Cracks: (Y/N)

16.

CURB REVEAL

TOTAL HOURS 1.00000000 1.00000000 1.00000000 1.00000000 1.00000000 1.00000000 1.00000000 1.00000000 1.00000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.000000000 1.0000000000	CITY/	TOWN	1			B.I.I	N.	BR. DEPT. NO. 8.	-STRU	JCTU	RE NO.		INSPECTIO	N DA	ATE
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B. Translations Dive Cur Def					L		Α. Ι	Bridge Railing		_		Lift Dualest	N		
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S.R.P. FagalStope Protection N H -	5.Util	lities		N	N	-	Red	ecommended Posting N	N	N	N		OI		
### Place Signs in Place P	6.Rip	-Rap/S	Slope Protection	N	н	-	Wai	aived Date: 00/00/0000 EJ	DMT D	ate:	00/00/0000			-	_
Signs in Place ("Y'Reg N=NO, N=N-O) NEAUM SELECTION STREAM FLOW VELOCITY: Task () High () Moderable () Low () Norre() STREAM FLOW VELOCITY: Task () High () Moderable () Low () Norre() STREAM FLOW VELOCITY: Task () High () Moderable () Low () Norre() STREAM FLOW VELOCITY: Task () High () Moderable () Low () Norre() Signs in Place ("Y'No: N=N-O) Norre Actual Field Measurement	7 Ag	oradat	ion	N	7						er Advance			-	- 1
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## Signs in Place Sig	Tidal () High	() Moderate () L	.ow () Nor	ıe ()	Pos		U	Ш_			`		
## Yes N=No,							Sig		W			TAPE#:			
RATING Rating Report (Y/N): N Date: 00/00/0000 Inspection data at time of existing rating 158: - 159: - 160: - Date: 00/00/0000 CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) CODE CONDITION N NOT APPLICABLE G 9 EXCELLENT Excellent condition. G 8 VERY GOOD No problem noted. G 7 GOOD Some minor problems. F 6 SATISFACTORY Sinctural elements alrow some minor detaincration. F 5 FAIR All primary structural elements alrow some minor detaincration. F 7 SPAIR Advanced section loss, deterioration, spalling or scour. P 4 POOR Advanced section loss, deterioration, spalling or scour. P 3 SERIOUS Insection Celebration, observation of the section of the bridge until corrective action is taken. C 2 CRITICAL Advanced defenderation of summary structural elements. Fatgue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless desery monitored it may be necessary to close the bridge until corrective action is taken. C 1 TIMMINENT" FALURE Stopes a close of serior or described primary structural elements are special but corrective action may put back in sight service. DEFICIENCY: A defect in a structure that requires corrective action may put back in sight service. DEFICIENCY: A defect in a structure that requires corrective action may put back in sight service. DEFICIENCY: A defect in a structure that requires corrective action may put back in sight service. DEFICIENCY: A defect in a structure that requires corrective action may put back in sight service. C-S-S-Certical Structural Deficiency: - Structural Deficiency: - Structural deferred solvers and restrict on the structure in the structure of the st	ITEM 61	(Dive R	eport): N ITEM 61	(This	Repor	<i>t</i>): 3	(Y=	=Yes,N=No,				List of field tes	sts performed:		
RATING Rating Report (Y/N): N Date: 00/00/0000 Inspection data at time of existing rating 158: - 159: - 160: - Date: 00/00/00000 CONDITION RATING GUIDE CODE CONDITION NOT APPLICABLE G 9 EXCELLENT Excellent condition. G 8 VERY GOOD No problem noted. G 7 GOOD Some minor problems. F 6 SATISFACTORY Structural elements show some minor deterioration. F 5 FAIR Al primary structural elements are sound but may have minor section loss, cracking, spalling or soour. P 4 POOR Advanced deterioration or securitorism, spalling or soour. P 3 SERIOUS In sale of shear cracks in concrete may be present. C 2 CRITICAL Advanced deterioration or primary structural elements. Falique cracks in shell or shear cracks in concrete may be present. C 1 "IMMINENT" FAILURE Biglior deterioration or primary structural elements. Falique cracks in shell or shear cracks in concrete may be present. C 1 "IMMINENT" FAILURE Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Biglior deterioration or section on present to extruct in critical str	026-1	1/M/ INI	SP DATE: 00	\/\\\		,		. , 11 /11					•		
Recommend for Rating or Rerating (Y/N): Y HIGH () MEDIUM (X) LOW () REASON: Has not been rated REASON: Has not been rated REASON: Has not been rated CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) DEFECTS CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) DEFECTS CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) DEFECTS CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) DEFECTS CONDITION RATING GUIDE (For Items 58, 59, 60 and 61) DEFECTS REASON: Has not been rated (G 7 GOOD Some minor problems. F 6 SATISFACTORY Structural elements show some minor deterioration. F 7 GOOD Some minor problems. F 8 FAIR All primary structural elements show some minor deterioration, spalling or scour. P 4 POOR Advanced section loss, deterioration, spalling or scour, some controller may be received in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless dosely monitored it may be necessary to dose the bridge until corrective action is steel or shear cracks in concrete may be present or scour may have removed substructure support Unless dosely monitored it may be necessary to dose the bridge until corrective action is steel or shear cracks in concrete may be present or scour may have removed substructure support Unless dosely monitored it may be necessary to dose the bridge until corrective action is steeled. C 1 "IMMINENT" FAILURE Bridge Goods to traffic but corrective action may put it back in light service. DEFICIENCY: A defect in a structure stability. Bridge stocked but are not limited to: Spalled concrete, Minor pot Market Bridge is colored to stable. The concrete may be present or source in spalling or source in stable and structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor pot Market Bridge is colored to stable. Concretely action may be repeated. Structural Bridge is colored to stable. Concretely action may be repeated. Structural Deficiency - Ordinary Structu	930-0	J/ VV 11V	SF. DATE. 00	700/	3000	<u>'</u>									
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P = Prioritize- [Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs made when funds and/or manpower is available].					-			- :	_						

2-DIST B.I.N. **C68**

STRUCTURES INSPECTION FIELD REPORT

ROUTINE & SPECIAL MEMBER INSPECTION

BR. DEPT. NO. **B-19-018**

CITY	//TOWN		8	STRUCTURE NO.	1	1-Kilo. PO	OINT	90-ROU	JTINE IN	SP. DA	ГЕ 93*	-SPEC.	МЕМВ. І	NSP. DATE
ВС	XFORD			B19018-C68-I	MUN-BRI	000.0	000	No	v 26, :	2019		Nov	26, 2	019
	ACILITY CARRIED VY BROOKVIEW R	RD		MEMORIAL NAMI	E/LOCAL NAME		27-YR I	BUILT DOO	106-YR	REBUIL 100	T *Y		AB'D (NO	N 106)
	EATURES INTERSECTED ATER FISH BROOK			26-FUNCTIONAL O		DIST. BI	RIDGE IN	ISPECTI	ON ENG	INEER	J. D	ideo		
	TRUCTURE TYPE 1 : Prestressed Cor	ncrete	Slab	22-OWNER Town Agency	21-MAINTAINER Town Agency	TEAM L	EADER -	A. Labib	1					
	DECK TYPE Concrete Precast I	Panels		weather Clear	TEMP. (air) -3°C		MEMBER OUST							
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					(Overa	all Previ	ious Co	nditio	1)		-	-	-	-
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	FICIENCY: A defect in a stru		equires correcti	ve action.										
	TEGORIES OF DEFICIENC - Minor Deficiency - Deficiencies holes, Minor		r in nature, gener	ally do not impact the structural	integrity of the bridge and o	ould easily be	e repaired. E	xamples in	clude but are	not limited	d to: Spal	led concre	te, Minor pot	
S=	Severe/Major Deficiency - De co	corrosion of ste eficiencies whic erroded rehars	eel, Minor scouring h are more extens Considerable settl	Clogged drainage, etc. ive in nature and need more place ement. Considerable scouring of	anning and effort to repair. E	xamples inc	lude but are	not limited	to: Moderate	to major o	leteriorati	on in conc	rete, Expose	d and
	S= Critical Structural Deficie	ncv _ A def	iciency in a struct											
	H= Critical Hazard Deficienc	A deficie	ency in a compone	ent or element of a bridge that p to: Loose concrete hanging do	oses an extreme hazard or	unsafe condi	ition to the p	ublic, but de	oes not impa	ir the struc	tural integ	grity of the	bridge. Exar	nples
	GENCY OF REPAIR:				W - D 5 1 11	6.00								
II.		•		ction Engineer (DBIE) to report to ce Engineer or the Responsible				-	n Report].					
P =	Prioritize- [Shall be prioritized by	/ District Mainte	nance Engineer o	r the Responsible Party (if not a	a State owned bridge) and re	epairs made	when funds	and/or man	power is ava	ilable].				

REMARKS

BRIDGE ORIENTATION

The one span structure carries Brookview Road over the Fish brook, which flows from west to east. For this report, the abutments are labeled South and North.

GENERAL REMARKS

Abutment length measured along the concrete cap = 23'-11" long

ITEM 58 - DECK

Item 58.2 - Deck Condition

See Item 59.15 - Slab.

Item 58.8 - Railing

The west rail panel is loose throughout the north half.

The east rail panel is loose near the south end with minor damage. See photo 2.

APPROACHES

Approaches a - Appr. pavement condition

The south approach has a transverse crack along the full width of the roadway near the south deck end. **See photo 1.**

ITEM 59 - SUPERSTRUCTURE

Item 59.15 - Slab

At 1.5' from the east edge of the slab, there is a delamination/incipient spalling area near the north end, 3' long x 1' wide. **See photo 3.**

ITEM 60 - SUBSTRUCTURE

<u>Item 60.1 - Abutments</u>

Item 60.1.d - Breastwalls

Both Breastwalls:

There are penetrable void areas between all stones. See photos 4 & 5.

There are several displaced stones leaning toward the stream throughout the full length. See photos 4 & 5.

Several stones have full height cracks, up to 1/4" wide.

The concrete cap has areas of minor honeycombing throughout.

North Breastwall:

At 4'-10" from the east edge of the concrete cap, there is a large washout area in the masonry section, 8'-2" long x full height x up to 4.5' deep, with stones and gravel washing into the stream. **See photo 6.**

The concrete cap has a full height crack, up to 1/8" wide, above the west edge of the washout area.

At the west edge, the second stone from the top has a full height crack, up to 1" wide. See photo 7.

South Breastwall:

REMARKS

Item 60.1.d - Breastwalls (Cont'd)

At approximately 6' from the east edge, the top stone under the concrete cap indicates severe displacement, up to 1' misalignment with the concrete cap. The concrete cap has a full height crack, up to 1/8" wide, at the mid-length point of the noted stone. **See photo 8.**

At the east edge, the bottom stone (At current water level) is undermined with no signs of support. **See photo 9.**

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

Item 60.1.f - Slope Paving/Rip-Rap

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

Item 60.1.h - Footings

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

Item 60.1.i - Scour

There is a sudden drop in the stream adjacent to the South Breastwall near mid-length suggesting a scour hole.

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

SubStructure Scour Notes

See Item 60.1.i

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

See Item 60.1.j - Scour

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

<u>Item 61.2 - Embankment Erosion</u>

All four embankment corners have areas of minor washout.

Item 61.3 - Debris

There is scattered debris throughout the channel. **See photos 10-12.**

<u>Item 61.6 - Rip-Rap/Slope Protection</u>

For underwater details, refer to the Routine Underwater Inspection Report dated 11/27/2019.

TRAFFIC SAFETY

<u>Item 36a - Bridge Railing</u>

Both bridge rails consist of w-beam panels mounted on steel posts extending into all four approach corners. See Item 58.8 - Railing.

Item 36b - Transitions

All four corners consist of w-beam panels mounted on steel posts. See Item 58.8 - Railing.

REMARKS

<u>Item 36c - Approach Guardrail</u>

All four corners consist of w-beam panels mounted on steel posts. See Item 58.8 - Railing.

<u>Item 36d - Approach Guardrail Ends</u>

All four corners consist of boxing glove type ends.

The southeast corner has minor damage. See photo 2.

The northwest corner is loose.

Photo Log

Photo 1: South approach, transverse cracking near the south deck end.

Photo 2: East rail, south end damage and loose panel.

Photo 3: Slab, delamination/incipient spalling area near the northeast corner.

Photo 4: South breastwall, typical voids and displaced stones. Photo 5: North breastwall, typical voids and displaced stones.

Photo 6: North breastwall, washout area starting at 4'-10" from the east edge of the concrete cap.

Photo 7: North breastwall, west edge stone, second layer from top, full height crack.

Photo 8: South breastwall, displaced stone starting at 5.5' from the east edge.

Photo 9: South breastwall, undermined stone at the east edge.

Photo 10: Typical debris throughout channel; view at west elevation.

Photo 11: Typical debris throughout channel; upstream.

Photo 12: Typical debris throughout channel; downstream.



Photo 1: South approach, transverse cracking near the south deck end.



Photo 2: East rail, south end damage and loose panel.



Photo 3: Slab, delamination/incipient spalling area near the northeast corner.



Photo 4: South breastwall, typical voids and displaced stones.



Photo 5: North breastwall, typical voids and displaced stones.



Photo 6: North breastwall, washout area starting at 4'-10" from the east edge of the concrete cap.



Photo 7: North breastwall, west edge stone, second layer from top, full height crack.



Photo 8: South breastwall, displaced stone starting at 5.5' from the east edge.



Photo 9: South breastwall, undermined stone at the east edge.



Photo 10: Typical debris throughout channel; view at west elevation.



Photo 11: Typical debris throughout channel; upstream.



Photo 12: Typical debris throughout channel; downstream.

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